

ANALYSIS

This ordinance amends the Los Angeles County Code, Title 22 – Planning and Zoning, to implement the Alternative Financial Services Ordinance which defines and establishes performance and development standards for alternative financial services in the unincorporated areas of the County.

Very truly yours,

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LJ:bh

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ORDINANCE NO. 2021-0011

An ordinance amending Title 22 – Planning and Zoning of the Los Angeles County Code, relating to the Alternative Financial Services Ordinance which defines and establishes performance and development standards for alternative financial services in the unincorporated areas of the County.

The Board of Supervisors of the County of Los Angeles ordains as follows:

SECTION 1. Section 22.14.010 is hereby amended to read as follows:

22.14.010 **A.**

...

Alternative Financial Services. The following terms are defined solely for Section 22.140.690 (Alternative Financial Services):

Alternative financial service. A use that charges a percentage fee to provide a loan or cash a check. This term includes, but is not limited to, deferred deposit transaction (payday) lender, check casher, and motor vehicle (auto) title lenders. This term shall not include a check cashing service or any state or federally chartered bank, credit union, mortgage lender, savings and loan association, industrial loan company, or non-profit financial institution.

Bank. This term shall have the same meaning as set forth in section 1561 of the California Financial Code.

Check casher. A business that for compensation engages, in whole or in part, in the cashing of checks, warrants, drafts, money orders, or other commercial paper serving the same purpose. This term shall not include a check cashing service or

any state or federally chartered bank, credit union, mortgage lender, savings and loan association, industrial loan company, or non-profit financial institution.

Check cashing service. A retail seller engaged primarily in the business of selling consumer goods, including consumables, to retail buyers, that cashes checks or issues money orders for a fee not exceeding two dollars (\$2) as a service to its customers that is incidental to its main business purpose.

Credit union. This term shall have the same meaning as set forth in section 14002 of the California Financial Code.

Deferred deposit transaction (payday) lender. An individual or corporation licensed by the commissioner of the California Department of Business Oversight that engages in the business of selling checks, drafts or money orders; receives money as an agent of a customer bound by contract for the purpose of paying bills, invoices or accounts of such customer; or accepts money in payment of utility bills, unless acting as an authorized agent for a utility company.

Industrial loan company. This term shall have the same meaning as set forth in section 18003 of the California Financial Code.

Mortgage lender. A bank or trust company, mortgage banker, state or federally chartered savings and loan association, service corporation, or other financial institution or governmental agency which is deemed capable of providing service or otherwise aiding in the financing of construction loans and mortgage loans.

Motor vehicle (auto) title lender. A business that grants a short-term loan to a borrower in exchange for repaying the principal amount borrowed plus interest. To

obtain the loan, the borrower offers the title to their car, motorcycle, mobile home, truck, van, or other vehicle operated on public highways and streets, as collateral should the borrower default in repaying the loan within the agreed upon time.

Savings and loan association. This term shall have the same meaning as an "eligible savings and loan association" as set forth in section 16600 of the California Government Code.

...

SECTION 2. Section 22.16.030 is hereby amended to read as follows:

22.16.030 Land Use Regulations for Zones A-1, A-2, O-S, R-R, and

W.

...

C. Use Regulations.

1. Principal Uses. Table 22.16.030-B, below, identifies the permit or review required to establish each principal use.

TABLE 22.16.030-B: PRINCIPAL USE REGULATIONS FOR AGRICULTURAL, OPEN SPACE, RESORT AND RECREATION, AND WATERSHED ZONES						
	A-1	A-2	O-S	R-R	W	Additional Regulations
...						
Service Uses						
Alternative financial services	=	=	=	=	=	Section 22.140.690
Barber shops	-	-	-	CUP ^{4,9}	-	
...

...

SECTION 3. Section 22.18.030 is hereby amended to read as follows:

22.18.030 Land Use Regulations for Zones R-A, R-1, R-2, R-3, R-4, and R-5.

...

C. Use Regulations.

1. Principal Uses. Table 22.18.030-B, below, identifies the permit or review required to establish each principal use.

TABLE 22.18.030-B: PRINCIPAL USE REGULATIONS FOR RESIDENTIAL ZONES							
	R-A	R-1	R-2	R-3	R-4	R-5	Additional Regulations
...							
Service Uses							
Alternative financial services	=	=	=	=	=	=	Section 22.140.690
Cemeteries	CEM	CEM	CEM	CEM	CEM	-	
...

...

SECTION 4. Section 22.20.030 is hereby amended to read as follows:

22.20.030 Land Use Regulations for Zones C-H, C-1, C-2, C-3, C-M, C-MJ, and C-R.

...

C. Use Regulations.

1. Principal Uses. Table 22.20.030-B, below, identifies the permit or review required to establish each principal use.

TABLE 22.20.030-B: PRINCIPAL USE REGULATIONS FOR COMMERCIAL ZONES								
	C-H	C-1	C-2	C-3	C-M	C-MJ	C-R	Additional Regulations
...								
Service Uses								
Alcohol beverage sales, for on-site consumption	-	CUP	CUP	CUP	CUP	CUP	CUP	Section 22.140.030
<u>Alternative financial services</u>	=	=	=	<u>CUP</u>	<u>CUP</u>	=	=	<u>Section 22.140.690</u>
...

...

SECTION 5. Section 22.22.030 is hereby amended to read as follows:

22.22.030 Land Use Regulations for Zones M-1, M-1.5, M-2, and

M-2.5.

...

C. Use Regulations.

1. Principal Uses. Table 22.22.030-B, below, identifies the permit or review required to establish each principal use.

TABLE 22.22.030-B: PRINCIPAL USE REGULATIONS FOR INDUSTRIAL ZONES					
	M-1	M-1.5	M-2	M-2.5	Additional Regulations
...					
Service Uses					
Alcohol beverage sales, for on-site consumption	CUP	CUP	CUP	CUP	Section 22.140.030
<u>Alternative financial services</u>	<u>SPR</u>	<u>SPR</u>	<u>SPR</u>	=	<u>Section 22.140.690</u>
...

...

SECTION 6. Section 22.24.030 is hereby amended to read as follows:

22.24.030 Land Use Regulations for Rural Zones.

...

C. Use Regulations.

1. Principal Uses. Table 22.24.030-B, below, identifies the permit or review required to establish each principal use.

TABLE 22.24.030-B: PRINCIPAL USE REGULATIONS FOR RURAL ZONES			
	C-RU	MXD-RU	Additional Regulations
...			
Service Uses			
Alcohol beverage sales, for on-site consumption	CUP	CUP	Section 22.140.030
<u>Alternative financial services</u>	=	=	<u>Section 22.140.690</u>
...

...

SECTION 7. Section 22.26.020 is hereby amended to read as follows:

22.26.020 Institutional Zone.

...

B. Land Use Regulations.

...

3. Use Regulations.

a. Principal Uses. Table 22.26.020-B, below, identifies the permit or review required to establish each use.

TABLE 22.26.020-B: LAND USE REGULATIONS FOR ZONE IT		
		Additional Regulations
...		
Service Uses		
<u>Alternative financial services</u>	-	<u>Section 22.140.690</u>
...		

...

SECTION 8. Section 22.26.030 is hereby amended to read as follows:

22.26.030 Mixed Use Development Zone.

...

B. Land Use Regulations.

...

3. Use Regulations.

a. Principal Uses.

i. Table 22.26.030-B, below, identifies the permit or review required to establish each principal use.

TABLE 22.26.030-B: PRINCIPAL USE REGULATIONS FOR ZONE MXD		
		Additional Regulations
...		
Service Uses		
Alcohol beverage sales, for on-site consumption	CUP	Section 22.140.030
<u>Alternative financial services</u>	-	<u>Section 22.140.690</u>
...		

ii. Table 22.26.030-C, below, identifies the permit or review required to establish each principal use. These uses may be established in commercial-only development projects or properties.

TABLE 22.26.030-C: PRINCIPAL LAND USE REGULATIONS FOR ZONE MXD IN COMMERCIAL-ONLY DEVELOPMENT PROJECTS OR PROPERTIES		
		Additional Regulations
...		
Service Uses		
<u>Alternative financial services</u>	=	<u>Section 22.140.690</u>
...		

...

SECTION 9. Section 22.26.050 is hereby amended to read as follows:

22.26.050 Scientific Research and Development Zone.

A. Land Use Regulations.

...

3. Use Regulations.

a. Principal Uses. Table 22.26.050-B, below, identifies the permit or review required to establish each principal use.

TABLE 22.26.050-B: PRINCIPAL USE REGULATIONS FOR ZONE SR-D		
Use Category		Additional Regulations
...		
Service Uses		
<u>Alternative financial services</u>	=	<u>Section 22.140.690</u>
...		

...

SECTION 10. Section 22.26.060 is hereby amended to read as follows:

22.26.060 Parking Restricted Zone.

...

B. Land Use Regulations.

...

3. Use Regulations.

a. Principal Uses. Table 22.26.060-B, below, identifies the permit or review required to establish each principal use.

TABLE 22.26.060-B: PRINCIPAL USE REGULATIONS FOR ZONE P-R		
		Additional Regulations
<u>Alternative financial services</u>	=	<u>Section 22.140.690</u>
...		

...

SECTION 11. Section 22.60.030 is hereby amended to read as follows:

22.60.030 Prohibited Uses.

In addition to uses listed in Section 22.22.030.E (Prohibited Uses), premises in Zone ()-IP shall not be used for the following uses listed in Table 22.60.030-A:

TABLE 22.60.030-A: USES PROHIBITED IN ZONE ()-IP	
<u>Alternative financial services</u>	<u>Ice cream shops</u>
...	

SECTION 12. Section 22.140.690 is hereby added to read as follows:

22.140.690 Alternative Financial Services

A. Purpose. This Section establishes standards for alternative financial services, which may have a detrimental effect on the health, safety, or welfare of the community, to minimize overconcentration while permitting these activities in specific Commercial and Industrial Zones.

B. Definitions. Specific terms used in this Section are defined in Section 22.14.010 of Division 2 (Definitions), under "Alternative Financial Services."

C. Applicability. This Section applies to alternative financial services in Zones C-3, C-M, M-1, M-1.5, and M-2.

D. Prohibited Areas. Alternative financial services are prohibited within any area regulated by a Specific Plan.

E. Existing Uses.

1. Alternative financial services that were lawfully existing as of (effective date of the ordinance to be inputted here), the effective date of this Section, may remain in their present condition, subject to the provisions of Chapter 22.172 (Nonconforming Uses, Buildings, and Structures).

2. Alternative financial services that were lawfully existing as of (effective date of the ordinance to be inputted here), the effective date of this Section, may be enlarged, expanded, or relocated only if the business is brought into compliance with the development and performance standards of this Section.

F. Application Requirements.

1. Ministerial Site Plan Review. A Ministerial Site Plan Review (Chapter 22.186) application is required for alternative financial services in Zones M-1, M-1.5, and M-2.

2. Conditional Use Permit.

a. Application. A Conditional Use Permit (Chapter 22.158) application is required for alternative financial services in Zones C-3 and C-M.

b. Additional Findings.

i. The requested use at the proposed location will not adversely affect the economic welfare of the nearby community.

ii. When an alternative financial service is located within 1,320 feet of another alternative financial service, a finding of public convenience or necessity shall be made based upon review and consideration of relevant factors, which shall include, but not be limited to, the following:

(1) The extent to which the requested use would duplicate services and, therefore, contribute to an over-concentration of similar uses;

(2) The extent to which the requested use will enhance the economic viability of the area;

(3) The ability of the requested use to serve a portion of the market not served by other uses in the area; and

(4) The extent to which the requested use, location, surrounding area, and/or operator has a history of law enforcement problems.

G. Additional Application Materials. In addition to any information required by this Title 22, an application for an alternative financial service shall include a site plan depicting the location of all businesses within a 1,320 foot radius of the subject property.

H. Development Standards. Alternative financial services shall comply with the following development standards:

1. Buffer. The property boundary of an alternative financial service shall be a minimum of 1,320 feet from the property boundary of all other alternative financial services.

2. Sign Requirements. Alternative financial services shall post their loan rates in multiple languages including, but not limited to, English, Spanish, Korean, Chinese, Armenian, and Tagalog, adjacent to the customer service window using a letter height of not less than one-half inch.

I. Performance Standards. Alternative financial services shall comply with the following performance standards:

1. Hours of Operation. All alternative financial service activities shall be confined to the hours between 8:00 a.m. and 8:00 p.m., daily.

2. Security.

a. Security bars and accordion folding grilles installed on the exterior of a storefront are prohibited.

b. Exterior cashier windows are prohibited.

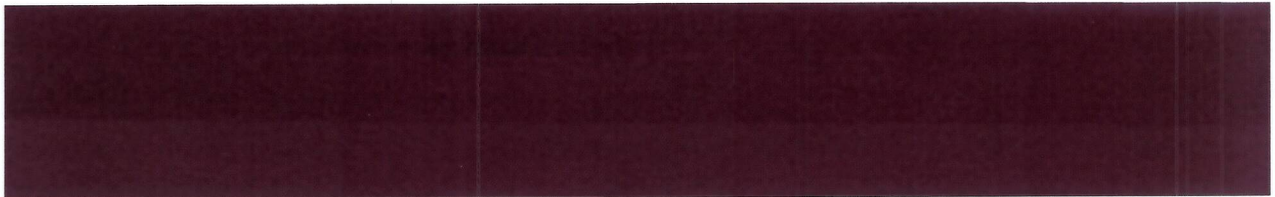
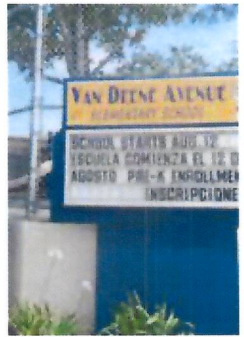
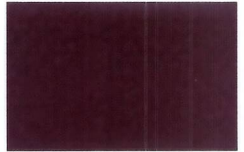
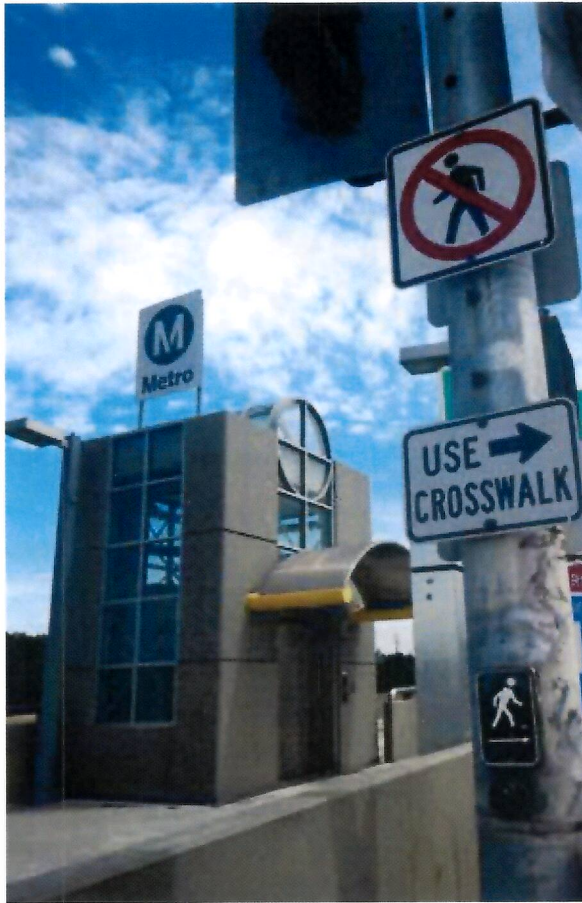
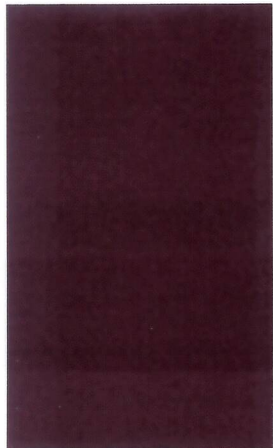
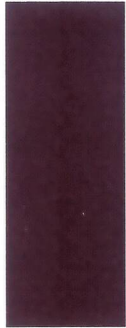
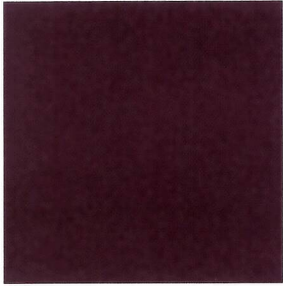
c. Building security grilles may be placed within the interior of the building if the grilles are concealed so that they are not visible from the exterior of the building when not in use during business hours.

d. Video security cameras shall be installed to monitor the customer service area and exterior areas surrounding the business. The cameras shall record video for a minimum of 30 days and be available to law enforcement officers upon request.

[2214010AFSSCCC]

WEST CARSON

TRANSIT ORIENTED DISTRICT SPECIFIC PLAN



County of Los Angeles

WEST CARSON TOD SPECIFIC PLAN



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Torrance Transit

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INTRODUCTION

INTRODUCTION

OVERVIEW

The West Carson Transit Oriented District (TOD) Specific Plan offers an incredible opportunity to create more engaging and vibrant places by leveraging the community's assets, connecting uses and activities, and attracting future investment. Los Angeles County Metropolitan Transportation Authority's Carson Station's proximity to numerous community facilities, including the Harbor-UCLA Medical Center campus, creates opportunities for improving the built environment and overall community livability, but there are also challenges that must be addressed as part of a successful planning process.

The West Carson TOD Specific Plan (Specific Plan) will guide future transit oriented development to create a distinct identity; improve connections and access for all users; and improve the safety, economic vitality, and overall quality of life for the West Carson community.

The Specific Plan will be used in conjunction with the Los Angeles County 2035 General Plan (General Plan) and Los Angeles County Code (County Code) to provide more detailed design and development criteria for individual project proposals and public improvements. The plan defines the proposed land use plan, development standards, infrastructure improvements, design guidelines, and implementation programs for any proposed project in the Specific Plan area.

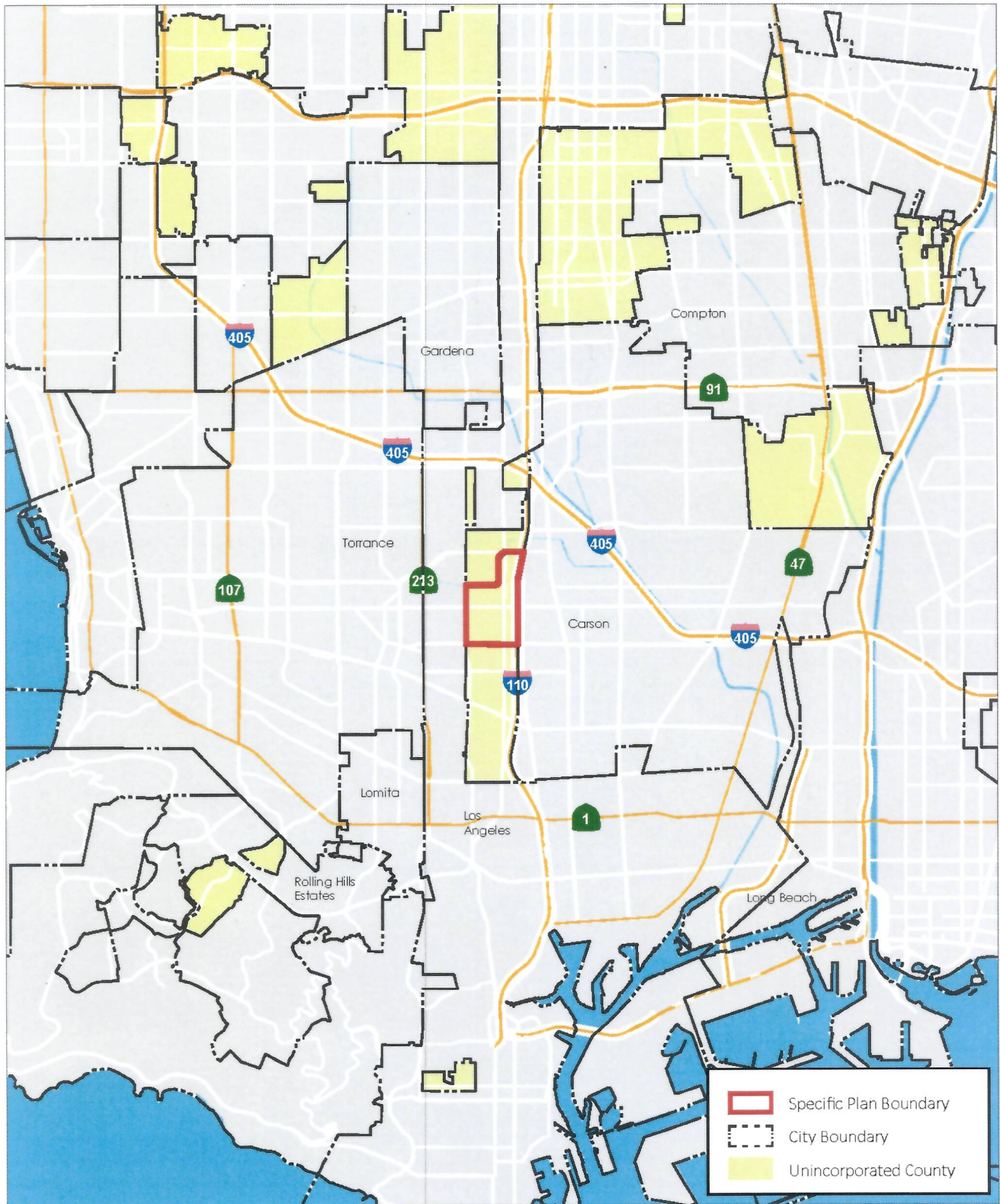
What is a Specific Plan?

A specific plan is one of several policy or regulatory tools used by local governments to guide community development. While a general plan sets forth goals, objectives, policies, and programs for the entire jurisdiction, the specific plan does so for a localized area and in greater detail. The authority for preparing a specific plan is in the California Government Code sections 65450 through 65457. The law allows, but does not require, the planning agency to prepare and adopt specific plans for the systematic execution of the general plan. According to State law, all specific plans must be consistent with the adopted general plan, and all subdivision and development activity must be consistent with the specific plan.

COUNTY FOCUS ON TRANSIT ORIENTED DEVELOPMENT

The County of Los Angeles (County) adopted a strong policy framework in its General Plan for implementing smart growth and providing healthy, livable, and equitable communities. As part of the General Plan update, the County identified several urban and suburban areas with access to major transit and commercial corridors as priority policy areas for infill development. These transit oriented district priority areas are recognized as areas well suited for higher density housing and mixed uses surrounding existing major commercial, employment, and civic activity nodes. To spur this type of development, the General Plan establishes the TOD Program, which provides guidance for the preparation of TOD specific plans (or similar mechanisms) to address the unique characteristics and needs of each community with regard to access, connectivity, pedestrian improvements, and safety.

Figure I.1 Regional Location



The West Carson TOD Specific Plan applies to a portion of the unincorporated area in the Second District known as "West Carson." Throughout the planning process several stakeholders expressed a general disagreement with the area being identified as "West Carson."

Use of the name to identify the unincorporated area within which this TOD Specific Plan is located is not intended to establish a precedent. Should the name of this unincorporated area be changed at some time in the future, this TOD Specific Plan would be updated

These plans are to be used as policy and implementation tools to:

- »» Increase walking, bicycling, and transit ridership and reduce vehicle miles traveled.
- »» Facilitate compact, mixed-use development.
- »» Increase economic activity.
- »» Facilitate the public investment of infrastructure improvements.
- »» Streamline the environmental review process for future infill development projects.

While the term "transit oriented development" is used to refer to individual development projects, buildings, or other improvements in proximity to transit stations (often as public/private partnerships), it does not accurately capture the important relationship between transit stations and their surrounding communities. "Transit oriented district" (sometimes referred to as transit oriented community) is a more appropriate term for referring to areas, neighborhoods, or communities with concentrated residential, commercial, and mixed-use development served by high-quality transit. The provision of a variety of transportation options for these districts or communities will help to reduce greenhouse gas (GHG) emissions, increase transit ridership, promote walking and biking, reduce vehicle miles traveled, and facilitate housing and employment opportunities for all residents in the region. It will also improve the performance of the street, highway, and freeway system. Given the presence of the Harbor-UCLA Medical Center, Carson Station functions as both an origin and destination station that could benefit from the type of development and improvements that transit oriented communities can provide.

PURPOSE & INTENT

The overall purpose of the Specific Plan is to provide comprehensive direction for the development and facilitate implementation of the goals and policies of the General Plan, including the vision for the TOD priority areas. The intent of the Specific Plan is to expand opportunities for compact, infill development that is compatible with and supports the intensification of Harbor-UCLA, yet is sensitive to the existing single-family neighborhoods. The Specific Plan facilitates increased housing opportunities and employment-generating uses proximate to the Carson Street (rapid bus transitway) station to take advantage of the significant local and regional transit services already provided in the area. The Specific Plan lays the foundation to create a more walkable, transit-oriented area with a mix of land uses that is accessible by all modes of transportation, including transit, walking, and bicycling.

The Specific Plan will not only establish a vision for the West Carson community, but will provide the regulatory framework—which includes policies, development standards, design standards, and recommended capital improvement projects—that will help to achieve that vision. An implementation and financing strategy is also included to support future implementation of the plan.

SPECIFIC PLAN LOCATION

The unincorporated territory of West Carson encompasses about 2.3 square miles of land between the cities of Torrance to the north, Harbor City (City of Los Angeles neighborhood) to the south, Carson to the east, and the cities of Los Angeles and Torrance to the west. The Specific Plan covers approximately 319 acres in West Carson within a half-mile radius of Metro's Carson Station, a bus rapid transit stop along a designated bus lane adjacent to Interstate 110. The project area is generally bounded by the 208th Street flood control channel to West Torrance Boulevard to the north, 223rd Street to the south, Interstate 110 to the east, and Normandie Avenue to the west as seen in Figure 1.2, Project Area.

A notable portion (22 percent) of the project area contains the Harbor-UCLA Medical Center, a major employment center that draws people from across the entire Los Angeles region. The project area is also just south of the Harbor Gateway Transit Center, which is a regional transit hub that connects the South Bay area to Downtown Los Angeles and other locations throughout the County. To the west of the project area is the community planning area known as Harbor Gateway in the City of Los Angeles, and to the east is the City of Carson.

Interstate 110, which runs along the eastern edge of the project area, travels north-south for about 32 miles, connecting Pasadena and Downtown Los Angeles in the north with San Pedro and the Port of Los Angeles in the south. Interstate 110 is a principle route for freight traveling from the port of Los Angeles to rail yards and warehouses further inland, which has resulted in a high number of industrial land uses adjacent to the highway.

The plan area's relation to its regional vicinity is displayed in Figure 1.3, Local Context.

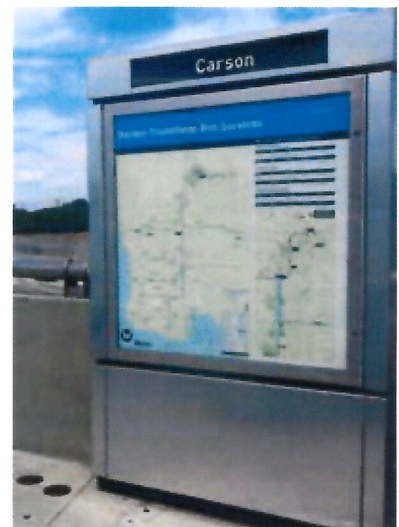
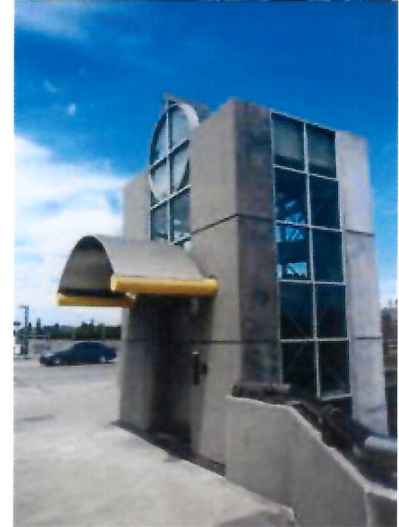
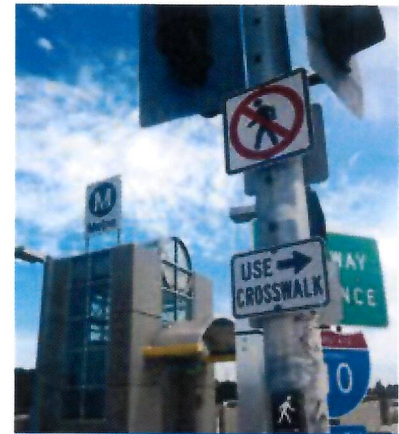
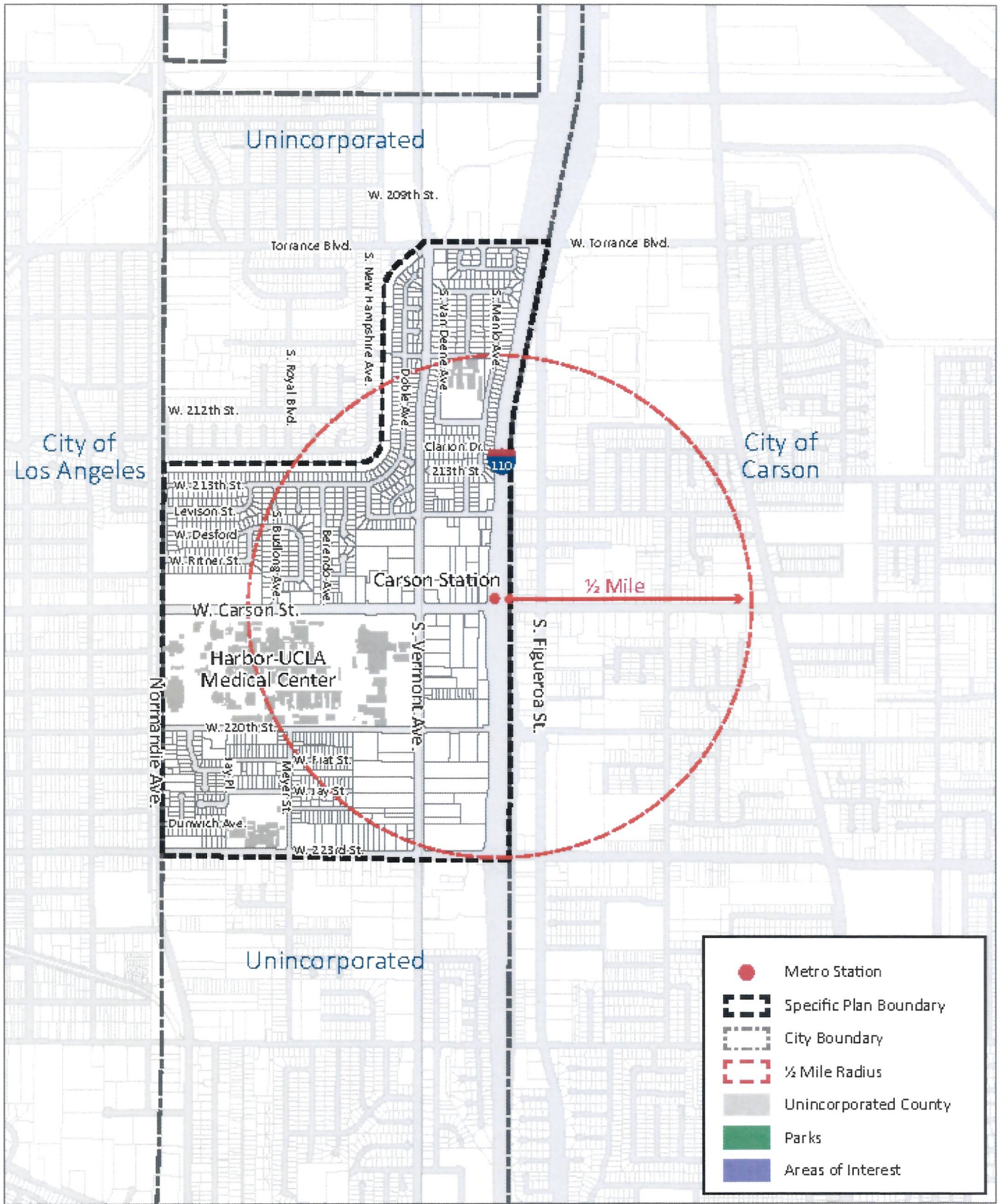


Figure 1.2 Project Area



PROJECT SETTING AND BACKGROUND

Currently, the Carson Street corridor through the West Carson community is crowded and full of activity—it is a high traffic thoroughfare serving travel needs to Interstate 110, adjacent cities, surrounding neighborhoods, and the Harbor-UCLA Medical Center.

Land Use and Urban Design Character

Central to the Specific Plan project area is the Harbor-UCLA campus, which is the activity hub of the West Carson community. Surrounding the campus along the north side of Carson Street and east side of Vermont Avenue are mainly low-density strip malls and auto-centric businesses—such as chain fast-food establishments, supermarkets, pharmacies, gas stations, and health-care-related offices. Commercial development in the project area is characterized by large community-serving retailers interspersed with smaller stores and services that vary in scale, style, orientation, lot coverage, and setback from the sidewalk and property lines. On-site parking is generally provided either at the front or side of a building and is typically underutilized. Many of the commercial structures are older, poorly maintained, and lack quality of design.

Beyond this commercial core, the project area is made up of residential development in the northern and southern portions, including two public elementary schools. Land uses along Vermont Avenue, the primary north-south arterial running through the community, consists of several multi-family housing projects and small areas of light industrial properties, many of which are used for warehousing, distribution, and storage, and small equipment-manufacturing facilities. Very few parcels are vacant in the project area, and there are no designated public parks.

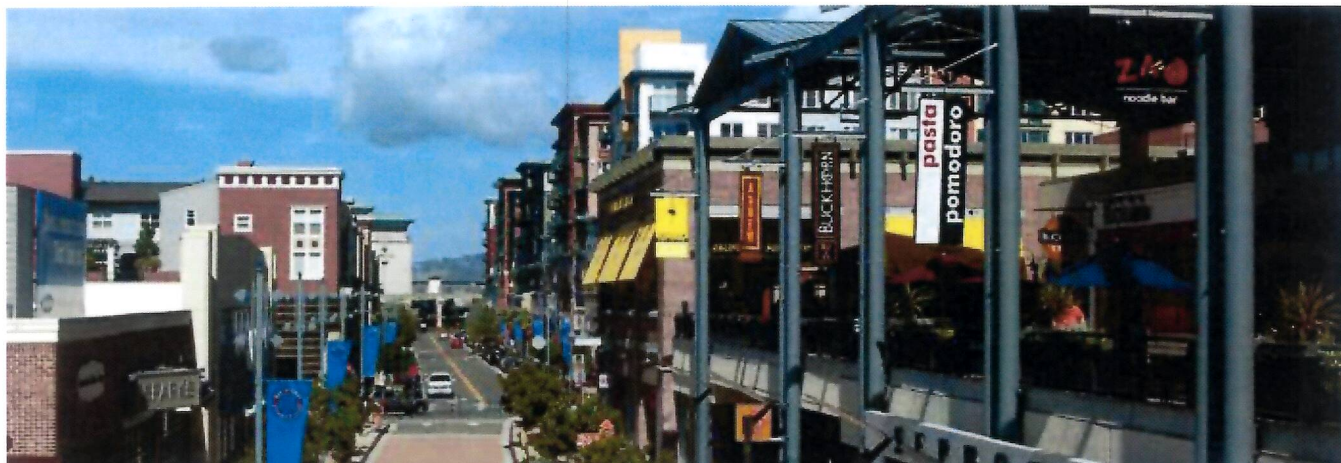
Mobility and Parking

West Carson is well served by bus transit, regional arterial roadways, and the adjacent Interstate 110 freeway. Roadways function well overall, with congestion occurring primarily along Carson Street and Vermont Avenue in proximity to Carson Street. It has main bus transit corridors with multiple bus routes from various local transit agencies and a bus rapid transit station at Interstate 110 and Carson Street. On-street parking is sufficient overall to serve the area's residential and commercial uses, but constrained along roadways surrounding the Harbor-UCLA Medical Center.

Though there is a completed sidewalk network, the overall pedestrian and bikeway network is limited and somewhat disconnected.

Figure I.3 Local Context





Example photo TOD adjacent to Emeryville, CA transit station.

The current environment is not friendly or attractive and feels unsafe to users and therefore does not promote bicycle and walking.

Infrastructure

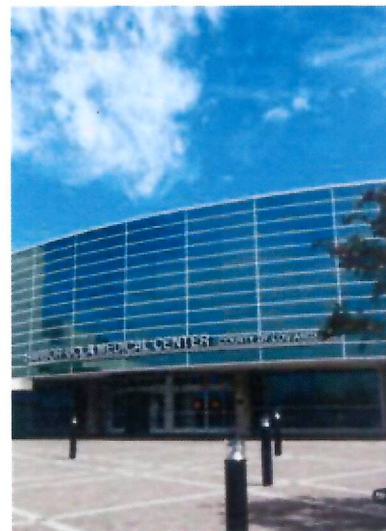
Current water supply, sanitary systems, and drainage systems in West Carson are in good condition, and capacities are sufficient to serve the existing community. Solid-waste-processing facilities serving West Carson and the Los Angeles metropolitan area as a whole lack capacity to handle expected future volumes of waste.

Economic Market Conditions

West Carson differs in several key ways from the County as a whole: It features a proportionally large Health Care sector, slightly faster employment growth, and higher rate of homeownership.

Within West Carson, in 2015, the four largest sectors by number of jobs: Health Care (19.8 percent), Construction (10.9 percent), Wholesale Trade (10.8 percent), and Accommodation and Food Services (9.8 percent) accounted for 51.3 percent of all jobs in the area. The Harbor- UCLA Medical Center is the largest employer in the Study Area, also home to the Los Angeles Biomedical Research Institute (LA BioMed), one of the largest independent, not-for-profit biomedical research institutes in the United States.

Between 2010 and 2015, the total number of jobs in West Carson grew from 14,866 to 17,014, an increase of 14.4 percent, indicating that the local economy has begun to recover from the Great Recession. In addition, this pace of





job growth has been significantly faster than the County, where the number of jobs increased 8.8 percent over the same time period. Passage of the Affordable Care Act is likely responsible for a portion of the increase in healthcare jobs, while the expansion of the medical center and new residential development are primarily responsible for new construction jobs.

Unemployment Rate

West Carson has a lower unemployment rate than the County. In August 2015, the unemployment rate in West Carson was 5.9 percent, lower than the County's rate of 7.0 percent.

Major Employment Generators

The Harbor-UCLA Medical Center campus is a key economic driver for the West Carson economy and wider South Bay region. Its 70 primary and specialty-care clinics are staffed by more than 4,000 employees, and supported by an annual operating budget of approximately \$677 million.

The Campus Master Plan proposes to add nearly 1.1 million square feet¹ of new medical space, augmenting the existing footprint of 1.1 million square feet. This major renovation represents a crucial investment in the Healthcare sector, a significant growth industry within the County.

The Biotechnology industry is also expected to grow as a result of the Campus Master Plan. LA Biomed already attracts millions of research dollars each year; vaccines and other medical discoveries originating at LA BioMed have already resulted in a number of spin-off companies—including several based in Southern California. Once the Master Plan is complete, LA BioMed's aging research facilities will be consolidated into a thriving, incubator space with 202,000 square feet of new facilities.

In addition to LA BioMed's research facilities, the County approved the creation of a new 250,000 square foot Bioscience Tech Park to be located on the Harbor-UCLA Medical Center campus. This will be a public/private partnership to develop a \$110-125 million facility that would create between 800 and 900 jobs.² Until now, however, the lack of a larger "hub" of biomedical research facilities has been a detriment to this sector's growth.

¹ County Department of Public Works. Notice of Preparation of an Environmental Impact Report and Notice of Public Scoping Meeting. June 29, 2015.

² MyLAnews.com. "LA County Lays Groundwork for Biotech Park at Harbor-UCLA Medical Center." April 28, 2015.

RELATIONSHIP TO OTHER RELEVANT PLANS AND PROGRAMS

This section describes how existing County or applicable agency plans and programs have helped set the discussion and regulatory framework for the Specific Plan area.

Los Angeles County 2035 General Plan

The General Plan identifies 11 Planning Areas and provides goals and objectives for all of the unincorporated areas in the County. West Carson is identified in the General Plan as an Opportunity Area for TOD redevelopment and infill, with significant portions of the project area designated as Industrial Flex District, Neighborhood Center, or Corridor. This Specific Plan shall be consistent with the General Plan.

Los Angeles County Code

Since the project area is in unincorporated territory, the regulations defined in this Specific Plan are in addition to the provisions of Title 22 (Planning and Zoning) and others as noted by the County Code. The provisions of the Specific Plan shall prevail and supersede the applicable provisions of the County Code.

Los Angeles County TOD Access Study

In February 2013, the Department of Regional Planning (Regional Planning) conducted a TOD Access Study as part of the General Plan in consultation with the Department of Public Works (Public Works) and Metro. The purpose of the study was to assess station access capacity and needs within nine proposed TODs throughout the County, including Carson Station. Several recommendations regarding bike facilities were made for the area surrounding Carson Station, and Regional Planning has been working in conjunction with Public Works to incorporate many of those recommended facilities, including Class IV bike lanes and a cycle track.

Harbor-UCLA Medical Center Master Plan

In 2016, the County proposed the Harbor-UCLA Campus Master Plan to enhance the unique, interactive relationship between the clinical, educational, and research components of the Harbor-UCLA Medical Center campus. The Harbor-UCLA Campus Master Plan envisions a significantly different campus from what exists today. The new campus design is intended to better engage Carson Street and the surrounding community through a new pedestrian trail along Carson Street and a landscape design that opens the campus more to the street. The master plan also presents an opportunity to address certain issues that affect the

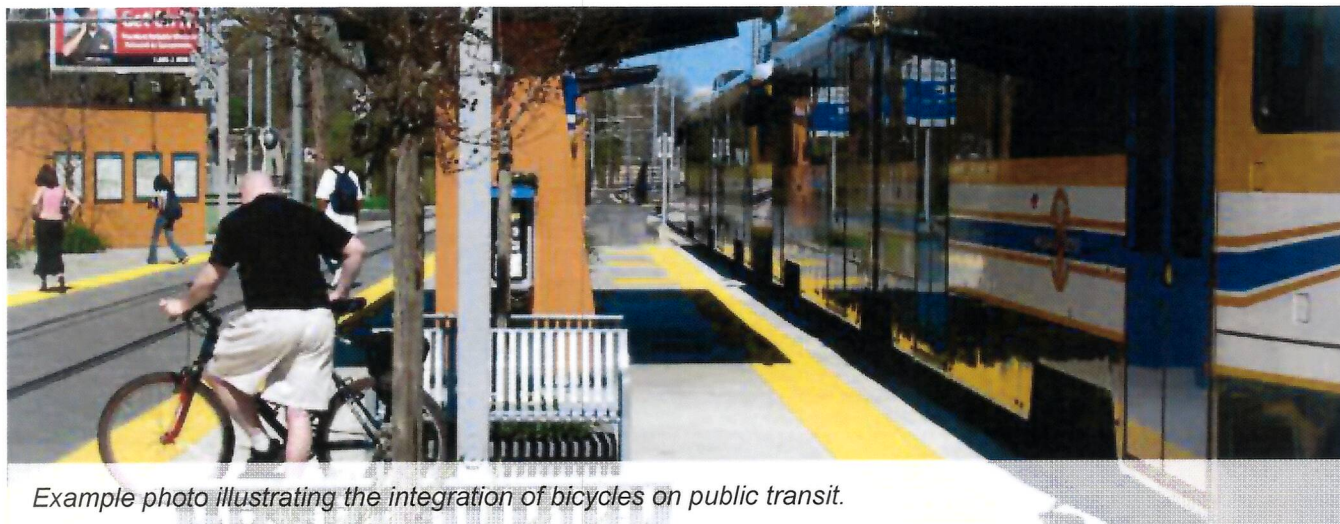
surrounding community, such as crime, safety, and sanitation concerns with the homeless population. To inform the campus planning efforts, the master plan considers the quality of the built environment of the Harbor-UCLA Medical Center and incorporates the following built-environment principles:

- »» Create a friendlier, more easily accessible place for patients.
- »» Enhance the integration of the campus with Carson Street activity.
- »» Enhance the integration of the campus with transit oriented development along Vermont Avenue and Carson Street.
- »» Improve the physical relationships between Harbor-UCLA and LA BioMed to enhance the operational efficiency of the campus.

Other Relevant Regulatory Plans and Programs

Below is a list of other regulatory agencies or documents that will need to be consulted as the Specific Plan is developed, for any type of development in the plan area or project area.

- »» **First-Last Mile Strategic Plan (Metro).** This plan contains guidelines and outlines a specific infrastructure improvement strategy, the Metro Pathway, to facilitate easy, safe, and efficient access to Metro transit facilities, including the Carson Street BRT station. It is intended to serve as a resource to agencies in the region working to update programs, land-use plans, planning guidelines, business models, entitlement processes, and other tools that take advantage of the County's significant investment in the public transportation network. Relevant strategies from the First-Last Mile plan were applied to mobility recommendations for this Specific Plan.



Example photo illustrating the integration of bicycles on public transit.

- »» **Harbor Gateway Community Plan** (City of Los Angeles). This plan contains the land use, circulation, and service systems goals, policies, and programs for incorporated Harbor Gateway community planning area of the City of Los Angeles adjacent the project area to the west, and guides the land use decisions regarding development.
- »» **Carson Master Plan of Bikeways** (City of Carson). This plan contains goals, policies, and actions; proposed bicycle infrastructure improvements and programs; funding sources; and design guidelines to comprehensively plan for bicycle transportation throughout the city of Carson, including Carson Street adjacent to the Specific Plan area. This plan was used in identifying appropriate bikeway connections from West Carson into Carson.
- »» **Carson Street Mixed-Use District Master Plan** (City of Carson). This plan contains development standards and design guidelines; public improvement objectives, concepts, and design proposals; and implementation strategies for the proposed mixed-use district on a 1.75-mile section of Carson Street in the City of Carson. The development regulations and streetscape improvements were considered in preparing the standards for this Specific Plan.

COMMUNITY ENGAGEMENT

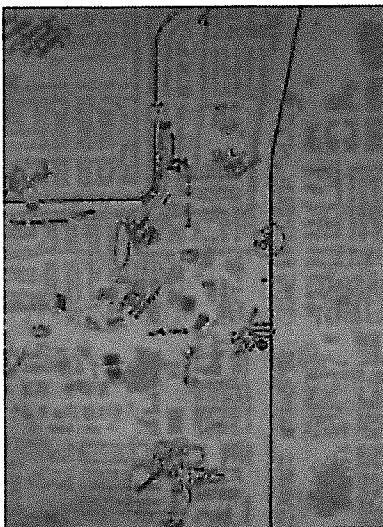
As part of the planning process for the Specific Plan, County staff from Regional Planning facilitated community outreach events that helped shape the Specific Plan.

West Carson TOD Specific Plan Task Force

The West Carson TOD Specific Plan Task Force was convened by Regional Planning for the express purpose of guiding the preparation of this plan. The task force consisted of Regional Planning staff; representatives from other County agencies, including the Departments of Public Health, Parks and Recreation, Public Works, the Community Development Commission, the Arts Commission; and other key stakeholders, including the Cities of Carson and Los Angeles and Metro. Task Force members met quarterly throughout the Specific Plan preparation process to review key project products and provide input at major milestones of the project and on draft documents.

Throughout the planning process for the Specific Plan, Regional Planning staff met with and received input from the following community groups and stakeholders:

- »» Harbor City/Harbor Gateway Chamber of Commerce
- »» South Bay Cities Council of Governments
- »» Harbor City Neighborhood Council
- »» South Bay Coalition to End Homelessness
- »» Harbor-UCLA Medical Center
- »» Van Deene Avenue Elementary School
- »» Second District Board Office
- »» Del Amo Action Committee
- »» Torrance Transit
- »» Developers: Equassure, Inc., Blackwood Real Estate, Abode Communities
- »» West Carson residents
- »» Prevention Institute
- »» LA BioMed
- »» California Department of Transportation (Caltrans)



Community Workshops

On November 17, 2015, and February 6, 2016, Regional Planning hosted two public workshops to solicit community input regarding the Specific Plan project. Attendees included residents, business owners, County staff, and property owners. Key themes from the workshops included:

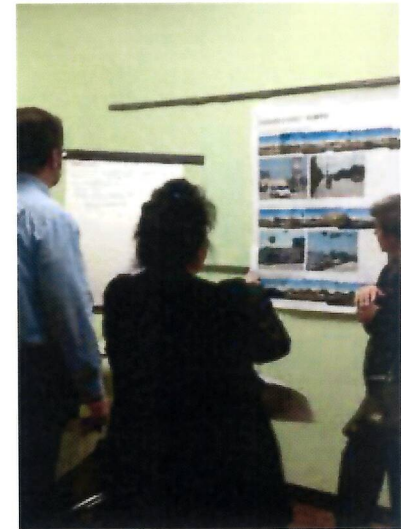
- »» A strong desire for pedestrian and bicycle improvements and street beautification.
- »» Improved access and amenities.
- »» Limited density mixed-use development along main corridors.
- »» Beautification of existing commercial development.

- »» Measures to reduce traffic speeds along Vermont Avenue.
- »» Better gateway and entryway monuments that define the community.
- »» Improved pedestrian safety and access in proximity to elementary schools.
- »» A great need for parks and community space.
- »» Open parts of the Harbor-UCLA campus to the community.
- »» Redesign Carson Metro Silver Line Station to improve safety and access.
- »» Address homeless population surrounding the Harbor-UCLA campus.

Developer Surveys

The County distributed a survey to developers in the County-South Bay-West Carson area to solicit information regarding the strength of the commercial and residential markets, potential redevelopment opportunity sites, and incentives that would better attract development in West Carson. The surveys were distributed in written format and had a small but valuable number of respondents. Respondent feedback noted the following findings:

- »» The residential market for high density housing (at up to 200 dwelling units per acre (du/ac) is very strong based on very low amount of multi-family housing stock, but would require parcel assembly.
- »» The commercial market is likely overserved and contributes to low rental rates and vacancies.
- »» The mixed-use areas identified in the Specific Plan along Carson Street present good opportunity sites.
- »» Affordable housing projects are likely to score very competitively for cap-and-trade funding and special needs/affordable housing subsidies.
- »» Developers are most interested in a streamlined entitlement process incentive.



Harbor-UCLA Medical Center Surveys

In May 2016, surveys were distributed to employees at the Harbor-UCLA-Medical Center to collect information on employee commutes, housing options, and amenity and improvement suggestions for the Specific Plan area. The survey was distributed both in written and online formats and had over 180 respondents.

The average commute for most employees to the medical center was between two and 10 miles, and the majority of those drive alone.

Factors that employees noted would encourage them to use public transit included improved safety at and around the stations, accessibility to the stations, more frequent/late service, and fewer transfers between lines. Employees also noted that a larger supply of more decent, affordable, and primarily single-family housing and improved safety in West Carson would encourage them to live closer to the hospital campus. Suggested improvements for the Specific Plan area included: a wider variety of places to eat and drink, improved landscaping and open spaces, a farmers' market, improved lighting at night, control of the transient population, a shuttle service, and pedestrian improvements.

ORGANIZATION OF THE SPECIFIC PLAN

The Specific Plan covers the following topics:

- 1. Introduction.** Covers the purpose and context for the Specific Plan, an overview of the planning process, and the plan's relationship to other relevant plans and programs.
- 2. Vision & Goals.** Outlines the vision for the community and the overarching goals and policies for achieving that vision.
- 3. Land Use and Urban Design Framework.** Identifies the land use and urban design framework, including permitted uses and regulations and development standards for each of the Specific Plan zones, including building height, density, parking, site configuration, building design, open space and landscaping requirements, and other design standards.
- 4. Mobility.** Provides a summary of the proposed mobility plan, including the vehicular, pedestrian, bicycle, transit, and parking networks.
- 5. Infrastructure.** Addresses the critical infrastructure requirements associated with future development in the Specific Plan area, including water, sewer, stormwater, solid waste, and public services.

[24]

6. **Economic Development.** Highlights opportunities for economic development in the plan area and associated community benefits.
7. **Capital Improvement Plan.** Details the capital improvement recommendations and phasing for the plan.
8. **Implementation and Administration.** Provides specific implementation and financial strategies for realizing the goals of the Specific Plan as well as describing project review and administrative procedures required for amendments and/or modifications to the plan.

HOW TO USE THE SPECIFIC PLAN/SPECIFIC PLAN AUTHORITY

Under California Law (Government Code §§ 65450 et seq.), cities and counties may adopt specific plans to develop policies, programs, and regulations to implement the jurisdiction's adopted general plan. The specific plan, therefore, serves as a bridge between the general plan and individual development projects.

Specific plans are similar to development zoning ordinances in that they establish implementation through the use of development regulations. However, unlike the County Code, the West Carson TOD Specific Plan is targeted to a specific planning area to allow for greater flexibility and specificity.

This Specific Plan was adopted by ordinance and is intended to be used by residents, business and property owners, developers, designers, County staff, and elected officials in the review of proposed development projects in the project area. The Specific Plan should be used in conjunction with the goals, policies, and regulations in the General Plan and County Code to guide users through the development review process. It is important to note that the Specific Plan only establishes land use zones, land use regulations, and development standards. Any particular development proposals would occur through private investment following the adoption of the Specific Plan.

02

VISION, GOALS, AND POLICIES

VISION, GOALS AND POLICIES

The Specific Plan is shaped by the community's vision for the area in the future. A combination of underutilized land; a community's desire for a new identity, streetscape and façade improvements, and enhanced pedestrian and bicycle connections; and the project area's proximity to a freeway, Metro transit stop, and a major employment center make West Carson an ideal candidate for reinvestment and future development opportunities. The Specific Plan will establish the foundation for a more livable, accessible, and sustainable community by:

- Changing zoning to allow for new housing and mixed-use development opportunities
- Focus on a multimodal approach to transportation
- Create a strong identity for the project area
- Allow for development flexibility to meet market demands

The Specific Plan develops a land use and development framework that could ultimately support 3,574 new residential units and 4,602,660 new square feet of commercial and employment space. This chapter will provide a detailed summary of that vision, and describe specific goals and policies to help guide this Specific Plan in achieving that vision.

VISION STATEMENT

This is a vision for how the Specific Plan area will look 20 years from now as future development decisions and public improvements are guided by the Specific Plan.

The West Carson TOD area is the heart of the unincorporated area known as West Carson. Harbor UCLA Medical Center and LA BioMed are the employment and health care hubs for the area, with the new campus expansion and improvements being well integrated with the adjacent community. The Specific Plan area is safe, attractive, and economically thriving. The area has a unique blend of commercial uses that serve the well-established neighborhoods of West Carson, as well as the employees of the area. Street improvements have made it easier and safer to access transit stations, employment, shopping areas, and schools. Improved transit service and facilities in the area have increased ridership, and reduced commuting by car. New high quality housing and mixed-use development has replaced aging and obsolete buildings along Carson Street and Vermont, adding new life and activity along these key corridors. This new housing supports local businesses, provides a range of multi-family housing types, increases property

values, and helps to meet the demand for housing in the community. Public open space in the form of pocket parks and a multi-purpose path has been developed within the Specific Plan area, helping to address the need for green spaces and outdoor recreation areas. Long-time residents and high levels of home ownership continue to be a hallmark of this community.

OVERALL GOALS & POLICIES

The overarching goals and policies for the Specific Plan are intended to provide structure to the plan, and will be used to evaluate future development proposals.

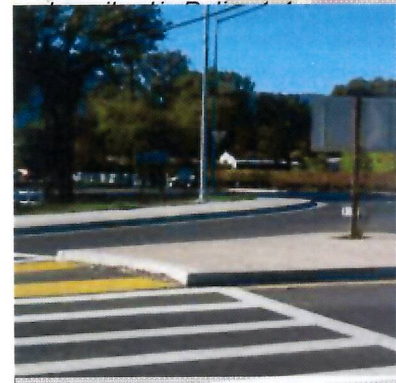
The following goals and policies were developed based on input from the community members, stakeholders, County Task Force, and County staff:

Goal 1: Create a distinct identity in the West Carson community.

- »» Policy 1.1. Promote a strong and authentic sense of place through community collaboration, compact and walkable building design, a range of housing choices and employment, and enhancement of open space opportunities.
- »» Policy 1.2. Activate the public realm through the addition of appropriately scaled mixed-use development and attractive streetscape.
- »» Policy 1.3. Locate defining gateway features at focal entry points.
- »» Policy 1.4. Address the community's park needs and deficiencies by constructing pocket parks on underutilized parcels, roadway segments, and other spaces that may be repurposed for recreational use.
- »» Policy 1.5. Preserve the character of residential neighborhoods.
- »» Policy 1.6. Require proposed development to contribute to a pedestrian-friendly walking environment and neighborhood character.
- »» Policy 1.7. Design buildings with entrances and windows oriented to the street to emphasize a community, pedestrian atmosphere. Where possible, on-site parking should not be located in front of a building.



Example photo illustrating a park with a walking path, as

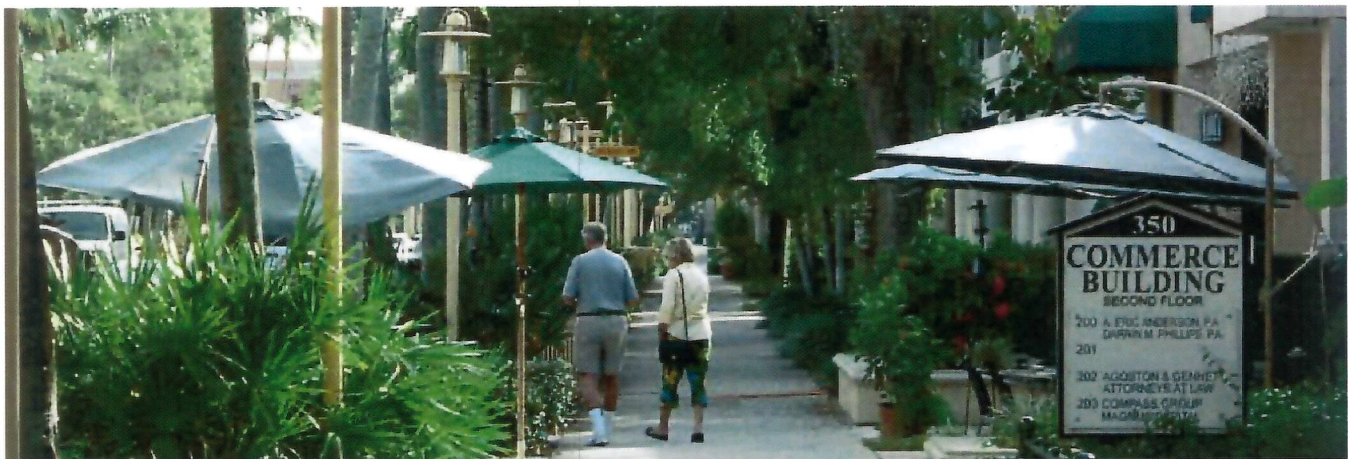


Example photo illustrating a mid-block crossing for pedestrians, as described in Policy 1.6.

- »» Policy 1.8. Activate school campuses through programming, such as farmer's markets, sports classes, etc.
- »» Policy 1.9. Incorporate new development that increases density along corridors and in proximity to transit while respecting the character of surrounding neighborhoods
- »» Policy 1.10. The expansion and renovation of the Harbor-UCLA Medical campus should integrate into and support the activity of the surrounding community.

Goal 2: Improve connections within the community and increase access to transit.

- »» Policy 2.1. Implement complete streets design that contributes to a multi-modal transportation system in collaboration with Caltrans, Metro, and the City of Carson.
- »» Policy 2.2. Implement streetscape features such as street lighting, landscaping, and wayfinding to create safer and attractive bike/pedestrian connections and linkages throughout the community.
- »» Policy 2.3. Improve and prioritize access to local bus stops and the Metro's Silver Line Station in new roadway development and design.
- »» Policy 2.4. Establish a connected pedestrian and bicycle network that links the Metro's Silver Line Station, Harbor-UCLA Medical Center, residential neighborhoods, local schools, and retail corridors.



Example photo illustrating pedestrian scaled mixed-use development, as described in Policy 1.2.



Example photo illustrating a pedestrian friendly plaza with street lighting, landscaping, and wayfinding that creates a safe and attractive connection to transit, as described in Policy 2.2.

- »» Policy 2.5. Collaborate with Harbor-UCLA Medical Center to provide enhanced bike and pedestrian facilities on campus property along Carson Street.
- »» Policy 2.6. Pursue the feasibility of constructing a multi-purpose trail along the 208th Street flood control drain.

Goal 3: Ensure the health and safety of residents, visitors, and employees.

- »» Policy 3.1. Engage with the surrounding jurisdictions, including Caltrans, to incorporate better lighting and visibility of pedestrian bridges and other public facilities within the project area.
- »» Policy 3.2. Implement traffic calming features along main corridors to improve safety and flow and discourage vehicular travel through residential neighborhoods.
- »» Policy 3.3. Create safer routes to schools and parks through increased signage, lighting, landscaping, and pedestrian connections around schools.
- »» Policy 3.4. Improve visibility and access to the Metro's Silver Line Station and local bus stops through increased lighting, signage, and crosswalks.



Example photo illustrating signage at a local bus stop as described in Policy 3.4.



Example rendering illustrating a new residential development with ground-floor retail.

- »» Policy 3.5. Incorporate open space, parks, plazas, and/ or recreational facilities as part of new developments to address the community's deficiencies.
- »» Policy 3.6. Address long-term over-sized vehicle parking violations through code revisions to improve the safety and aesthetics of the residential neighborhoods and multi- modal transportation.
- »» Policy 3.7. Work with service providers to support the transient population affiliated with the Harbor-UCLA Center.

Goal 4: Ensure economic vitality of the project area.

- Continue active engagement and partnership with the Harbor-UCLA Medical Center.
 - »» Policy 4.1. Take advantage of the nearby Harbor-UCLA Medical Center and provide workforce development and healthcare related employment resources for local residents.
 - »» Policy 4.2. Encourage the expansion and retention of high- performing industry clusters within West Carson, including healthcare, biomed, and biotech.
 - »» Policy 4.3. Leverage new County Biomed Economic Development initiatives to ensure success of private uses on the Harbor-UCLA Medical Center campus and help catalyze new investments in the Plan Area.
- Encourage employment-generating uses where possible.
 - »» Policy 4.4. Continue to allow employment-generating uses along Vermont Avenue, with the flexibility to incorporate new residential development where feasible.
 - »» Policy 4.5. Incorporate adequate buffering and other land use practices to improve the compatibility between industrial and non-industrial uses.
- Leverage existing strong demand for new residential development.
 - »» Policy 4.6. Support a broader mix of uses and development along both Carson Street and Vermont Avenue.
 - »» Policy 4.7. Preserve existing stock of affordable housing and promote diversification of housing choices for various income groups.

»» Policy 4.8. Consider development proposals with unique financing that unbundle the cost of parking from the cost of housing.

- Spur a revitalized retail sector along Carson Street.

»» Policy 4.9. Activate street frontages to encourage retail activity with aesthetic improvements to commercial development.

»» Policy 4.10. Improve access to shopping centers for pedestrians, vehicles, transit users, and bicyclists.

Goal 5: Encourage a diverse mix of land uses and transit oriented development.

»» Policy 5.1. Provide a variety of housing choices within a half-mile distance to the Metro's Silver Line Station.

»» Policy 5.2. Incentivize mixed uses in specific areas to encourage employment-generating uses near the Metro Silver Line Station.

»» Policy 5.3. Reduce parking standards and incorporate parking garages along major corridors to increase the viability of development.

Goal 6: Improve the quality of life for existing residents with improvements to the public realm.

»» Policy 6.1. Make the community a more pleasant place to walk and bike.

»» Policy 6.2. Provide secure bicycle parking adjacent to the Metro Silver Line Station and ensure new development provides bicycle parking.

»» Policy 6.3. Enhance the public realm with street trees, street furniture, bicycle lanes, improved sidewalks, lighting, and pedestrian paths.

»» Policy 6.4. Improve underutilized sites—such as parking lots and vacant property—with community gardens, farmers markets, pocket parks.

»» Policy 6.5. Increase public amenities, such as a community pool, multi-purpose path along the 208th Street drainage channel, recreation center, library, fitness studio, and others.

- »» Policy 6.6. Encourage outdoor dining and seating areas in mixed-use areas.
- »» Policy 6.7. Seek out opportunities to enhance the Caltrans park-and-ride lot, to activate the space so that it is an asset to the community.
- »» Policy 6.8. Increase the supply of different housing types affordable to the community.
- »» Policy 6.9. Streamline and expedite affordable housing developments through the project and environmental review process and coordinate with private and non-profit organizations.
- »» Policy 6.10. Ensure that there is no net loss of affordable housing units in the community.

Goal 7: Maximize the use of sustainable development practices.

- »» Policy 7.1. Encourage resource-efficient building techniques, materials, and other principles of green building design in new construction, renovation, and landscaping.
- »» Policy 7.2. Incorporate "green" building practices into the planning, design, construction, and operation of County-owned facilities.
- »» Policy 7.3. Promote tree planting in the public and private realm for shade, cooling, and aesthetic benefits.



03

LAND USE AND DESIGN GUIDELINES

LAND USE AND URBAN DESIGN FRAMEWORK/REGULATING CODE

3.1 PURPOSE AND SCOPE

This Land Use and Urban Design Framework is intended to achieve a specific pattern of development in accordance with the envisioned future of the West Carson community. The standards in this section regulate the development of buildings, streets, and public spaces with a focus on the physical, built environment and the relationship between the private and public realms. Certain standards may apply only to specific planning areas, and are indicated as such in this section.

Development standards established in this Specific Plan are tailored and summarized to each zoning designation based on its location, adjacent streets, and intended use, as shown in Figure 3.1, Land Use Framework Plan. The Regulating Code below includes development standards for each applicable planning area, as follows:

- »» Use regulations, which provide permitted and conditionally permitted uses in the Specific Plan area (see Appendix A. Glossary for use definitions). Those uses not specifically listed in this Specific Plan or Title 22 of the County Code, are subject to a determination by the Director of Regional Planning (Director) as either permitted, permitted subject to a conditional use permit, or prohibited consistent with the intent of the planning area and the Specific Plan. Decisions of the Director are appealable to the Regional Planning Commission (RPC).
- »» Built form standards, which address specific aspects of site development, including building mass and placement adjacent to streets and other buildings.
- »» Building frontage standards, which identify permitted frontage types per applicable street.
- »» Open space standards, which address the required amount of private and public open space for residential and non-residential developments.
- »» Parking standards, which address the placement and number of required vehicle and bicycle parking spaces for residential and non-residential developments.
- »» Landscape standards, which address the placement and type of vegetation, for residential and non-residential developments.

- »» Signage standards, which address placement, type, and design for different signs.
- »» Other design standards, which address other streetscape elements such as lighting, furnishings, public art, outdoor dining, etc.

3.2 LAND USE APPLICABILITY

The provisions of this **Regulating Code** are applicable to all lots in the Specific Plan area and shall supersede the provisions in the County Code. The County shall administer the provisions of this Regulating Code in conjunction with the State of California Government Code and the General Plan and County Code. Where this Regulating Code differs from provisions in the County Code, the provisions herein shall prevail. If the Specific Plan is silent on an issue, the provisions of the County Code shall prevail.

Design standards and guidelines in Section 3.6, Urban Design, of this Specific Plan shall be used in concert with the development standards in the Regulating Code. No construction, modification, addition, placement, or installation of any building or structure shall occur, nor shall any new use commence on any lot, on or after the effective date of this Specific Plan, except in conformity with the provisions of this Specific Plan.

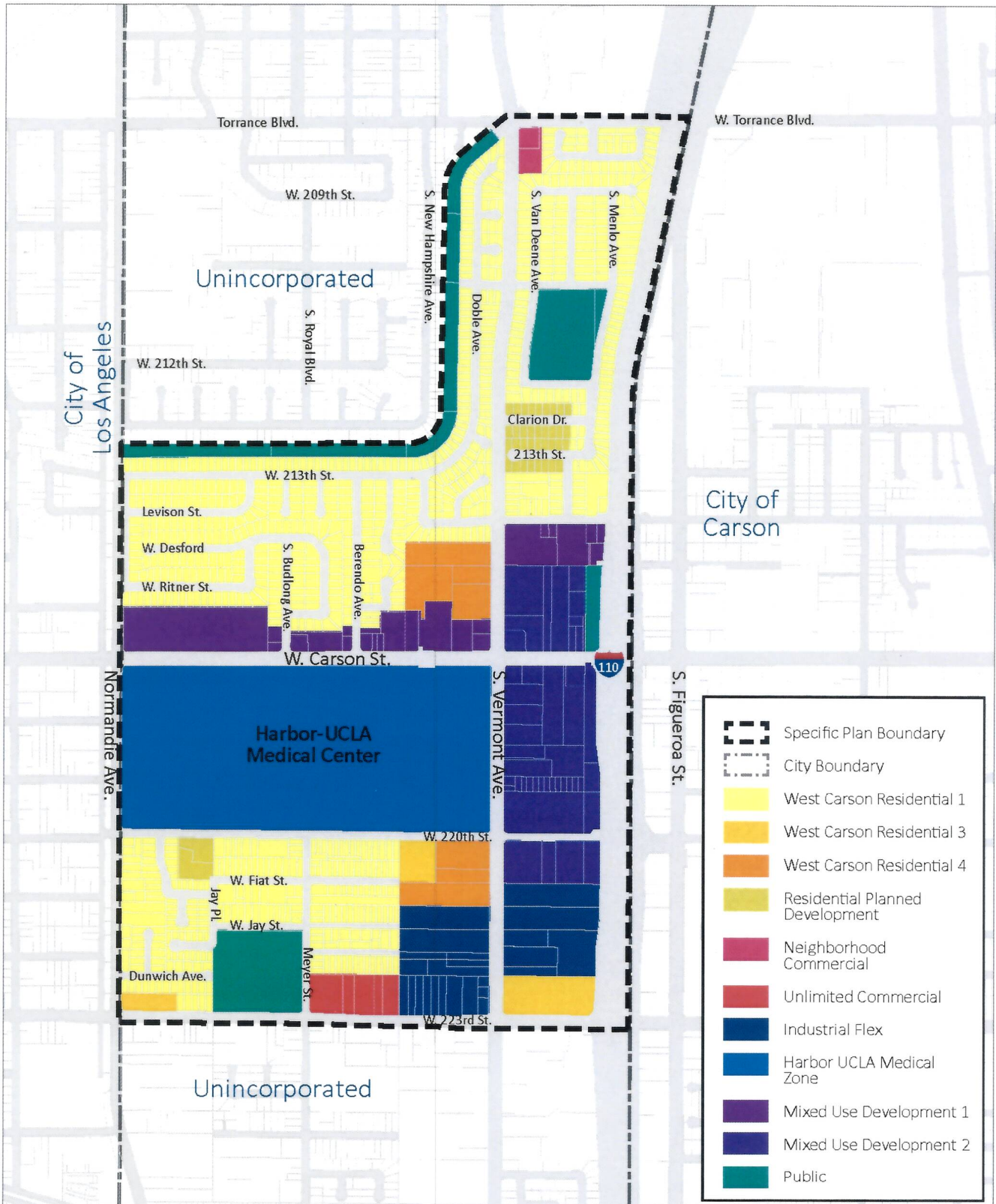
3.3 SPECIAL REQUIREMENTS

The following requirements shall be required for all zoning designations where applicable.

3.3.1 Housing Element Requirements

California law requires that cities and counties zone land to encourage and facilitate their fair share of housing growth—referred to as the regional housing needs assessment (RHNA). The preparation of this Specific Plan provides the County with the opportunity to create new affordable units to accommodate the needs of the residents.

Figure 3.1 Land Use Framework Plan



3.3.2 Existing Structures

Reuse of existing structures shall comply with applicable codes, including, but not limited to, Title 26 of the County Code (Building Code), Title 24 of the California Code of Regulations (physical access regulations), and requirements of the Americans with Disabilities Act (ADA).

Prior to issuance of use and occupancy permits, existing buildings, open space areas, and other site improvements shall be aesthetically upgraded through architectural and landscape improvements. Such improvements may include, but are not limited to:

- »» Upgraded façade treatments, including the use of plaster, brick, stone, and/or other approved materials.
- »» Upgraded window types and treatments.
- »» Upgraded entries, including doorways, covered walkways, decorative paving.
- »» Upgraded roofing materials and roof overhangs.
- »» Updated color scheme for buildings.
- »» Decorative treatment of all exposed site walls.
- »» Enhancement of private patios and balconies.
- »» Extensive planting of trees and shrubs throughout the site, including parking areas and common open space areas.
- »» Improved landscape design of front yards and common areas.
- »» Improved landscape design along building perimeters and entries.
- »» Improvements to common recreational areas including provision of shelters, lighting, and refurbishing of facilities.
- »» Addition of pedestrian amenities including paths, benches, shade trees, trash receptacles, drinking fountains, lighting, and decorative paving.
- »» Addition of bicycle facilities including bike racks.
- »» Creation of project entryways through signage and landscape design, as applicable.
- »» Creation of signage program for building identification and directional signs.
- »» Upgraded and consistent signage, including project identification, addressing, and directional signs.

- »» Enhanced lighting scheme for building entrances, common areas, paths, and parking areas.
- »» Application of defensible space techniques in landscaping and lighting.

3.3.3 Accessory Dwelling Units

An Accessory Dwelling Unit (ADU) is a secondary dwelling unit with complete independent living facilities for one or more persons and generally takes three forms: detached (separate from the primary structure), attached (connected to the primary structure), and repurposed existing space (space such as a master bedroom within the primary residence converted into an independent living unit).

3.3.4 Transitional Housing

"Transitional housing," as defined in California Government Code Section 65582(h), is housing configured as rental housing developments, but operated under program requirements that call for the termination of assistance and recirculation of the assisted unit to another eligible program recipient at some predetermined future point in time, which shall be no less than six months from beginning of assistance. Transitional housing shall be considered a residential use of property, and may be subject only to those restrictions that apply to other residential dwellings of the same type (single-family, multi-family) in the same zone.

3.3.5 Alternative Financial Service

"Alternative Financial Service" means a use that charges a percentage fee to provide a loan or cash a check. This term includes, but is not limited to, deferred deposit transaction (payday) lender, check casher, and motor vehicle (auto) title lenders. This term shall not include a check cashing service or any state or federally chartered bank, credit union, mortgage lender, savings and loan association, industrial loan company, or non-profit financial institution. Alternative Financial Services are prohibited within the Specific Plan area.

Table 3.1 Land Use Summary

ZONING DESCRIPTION	ACRES	PERCENT	ESTIMATED BUILDOUT	
			RESIDENTIAL	NON-RESIDENTIAL
RESIDENTIAL				
West Carson Residential 1	118	37.0%	851 units	-
West Carson Residential 3	7	2.2%	171 units	-
West Carson Residential 4	12	3.8%	484 units	-
Residential Planned Development	5	1.7%	88 units	-
NON-RESIDENTIAL				
Neighborhood Commercial	1	0.4%	-	14,787 sq. ft.
Unlimited Commercial	4	1.3%	30 units	50,620 sq. ft.
Industrial Flex	22	6.8%	486 units	1,133,779 sq. ft.
Harbor UCLA Medical	71	22.4%	100 units	1,941,339 sq. ft.
Mixed Use Development 1	20	6.2%	143 units	483,460 sq. ft.
Mixed Use Development 2	31	9.8%	1,223 units	978,675 sq. ft.
Public	27	8.5%	-	-
TOTAL	319	100%	3,574*	4,602,660 sq. ft.

* The total may not equal the sum of the components due to rounding.

3.4 REGULATING CODE

3.4.1 West Carson Residential 1 Zone (R-1)

3.4.1.1 Purpose and Intent

The West Carson Residential 1 (R-1) Zone is intended to preserve the scale and form of the area's existing single-family residential neighborhoods. The R-1 Zone provides primarily for single-family detached homes, up to nine dwelling units per acre.

Development Potential Summary

- »» Total Developable Area: 118 acres
- »» Residential: Max. 851 units

3.4.1.2 Use Regulations

The land use regulations define permitted, conditionally permitted, and prohibited uses in R-1 Zone, as shown in Table 3.2, Use Regulations: R-1 Zone. Use regulations shall remain consistent with Chapter 22.18, R-1 Single-Family Residence Zone, unless otherwise specified in this section.

Accessory uses and structures are permitted when customarily associated with, and subordinate to, a permitted use on the same site, and would include:

- »» Carport
- »» Accessory dwelling units
- »» Home occupation subject to provisions of Chapter 22.18
- »» Patio cover/trellis
- »» Swimming pool, spa, and jacuzzi

Figure 3.2 Proposed Zoning: West Carson Residential 1 Areas

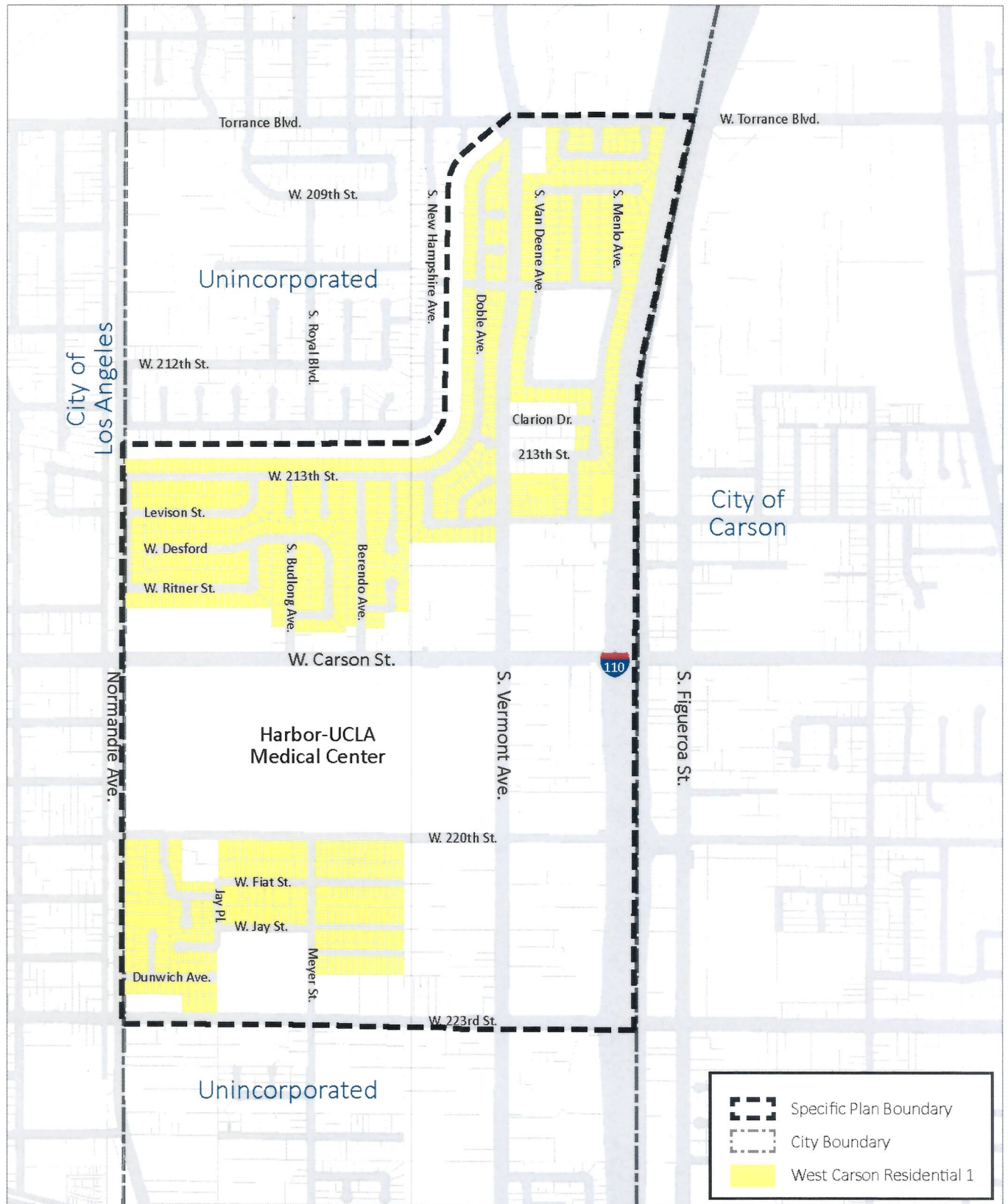


Table 3.2 Use Regulations: R-1 Zone

USE	
RESIDENTIAL	
Single-family dwelling units, attached/detached; residential planned unit developments.	Permitted
Multi-family dwelling unit (including apartment houses)	Prohibited
PUBLIC/INSTITUTIONAL	
School	Conditional
Parks and playgrounds with appurtenant facilities customarily found in conjunction therewith	Permitted
COMMERCIAL	
Juvenile halls	Prohibited

3.4.1.3 Development Standards

Standards for the R-1 Zone shall remain consistent with Chapter 22.18 (R-1 Single Family Residential Zone) in Title 22 of the County Code unless otherwise specified in this section and Section 3.6, Urban Design Standards, below. The following development standards regulate new site and building development by establishing standards for intensity, building height, open space, and other elements. They have been developed to accommodate the single-family residential uses anticipated. They are intended to preserve established single-family neighborhoods, improve connectivity, and provide screening and landscape design along roadways.

Table 3.3 Site Configuration Regulations: R-1 Zone

TYPE	MINIMUM	MAXIMUM
DENSITY		
Single-family	1 du/ac	9 du/ac
BUILDING SETBACK		
Front Setback	15 ft.	None
Side Setback	5 ft. ¹	None
Rear Setback	10 ft.	None
BUILDING HEIGHT		
Floors	None	2 stories
Building Height	None	35 ft. ²

1. *Where a lot or parcel of land is less than 50 feet in width, such lot or parcel of land may have interior side yards equal to 10 percent of the average width, but in no event less than three feet in width.*
2. *Building height shall exclude chimneys and rooftop antennas.*

a. *Landscaping*

- Drought-tolerant landscaping requirements shall be consistent with Section 4.106.5 of Title 31 (Green Building Standards) of the County Code.
- Front Yard Landscaping
 - »» With the exception of the required paved driveway and walkway, all areas within the street-fronting yard shall be landscaped and maintained.
- Wherever possible, existing trees shall be maintained.
- Fences
 - »» If perimeter fencing is required for security reasons, a Ministerial Site Plan Review (MSRP) application shall be required.

The Director may approve wrought-iron style fences up to five feet high that do not obscure views.

- »» Barbed wire and chain-link fencing are prohibited.
- Utility and mechanical equipment
 - »» All utility and mechanical equipment shall be placed in locations that are not exposed to view from the street or shall be screened from view. Satellite dishes shall be located away from public view.
 - »» Screening elements shall be an integral part of the building's design.

3.4.2. West Carson Residential 3 Zone (R-3)

3.4.2.1 Purpose and Intent

The West Carson Residential 3 (R-3) Zone is intended to provide opportunities for medium density developments containing multiple units, such as townhomes and stacked flats up to 30 dwelling units per acre. The development standards for this designation promote a variety of attached products types, including courtyard housing, row homes, garden apartments, and podium developments to provide a variety of housing options to serve the needs of the West Carson community.

a. *Development Potential Summary*

- »» Total Developable Area: 7 acres
- »» Residential: Max. 171 units

3.4.2.2 Use Regulations

The land use regulations define permitted uses within the R-3 Zone as shown in Table 3.5, Use Regulations: R-3. Use regulations shall remain consistent with Chapter 22.18, R-3 Limited Density Residence Zone, unless otherwise specified in this section.

Accessory uses and structures are permitted when customarily associated with, and subordinate to, a permitted use on the same site.

Figure 3.3 Proposed Zoning: West Carson Residential 3 Areas

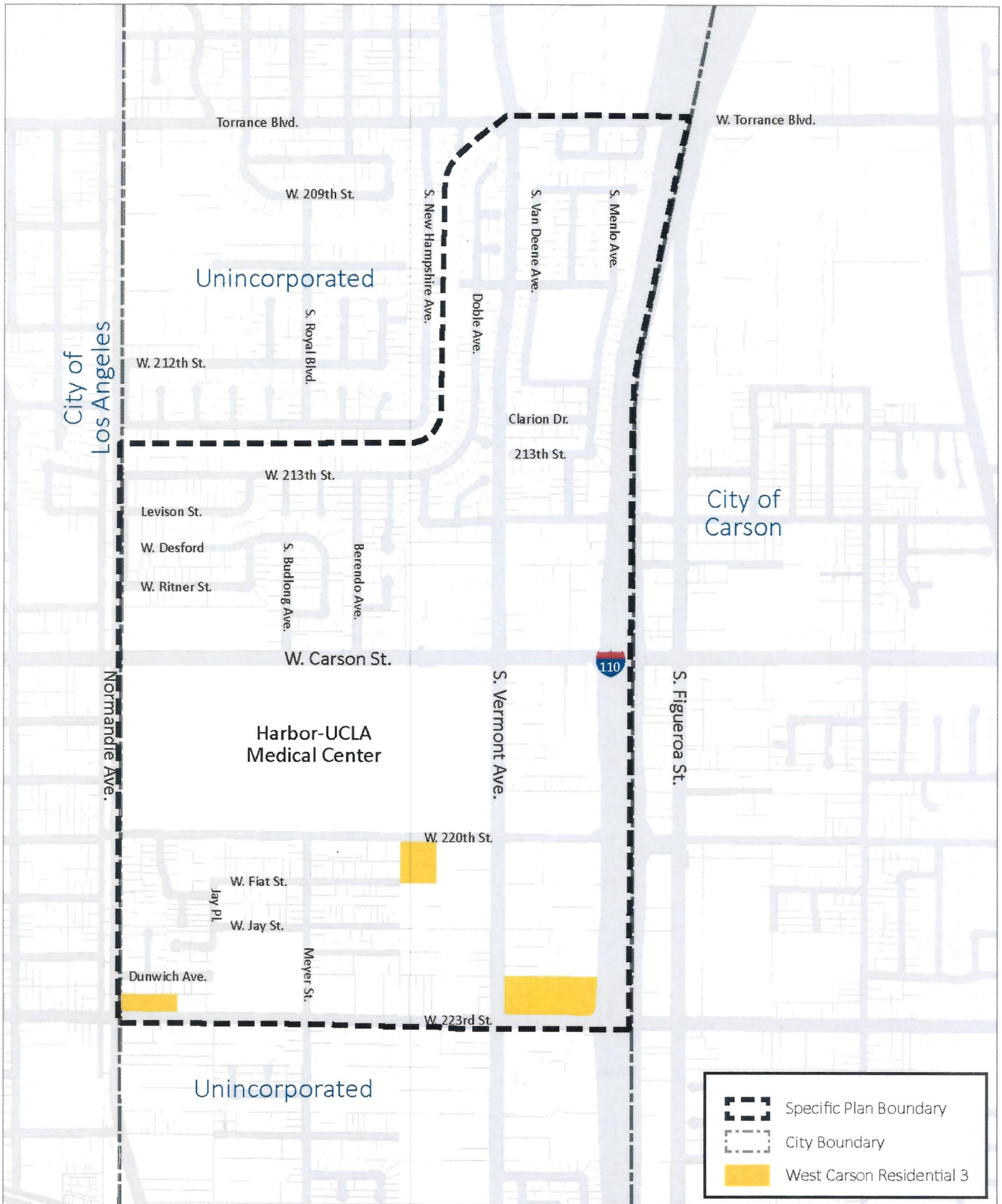


Table 3.4 Use Regulations: R-3 Zone

USE	
RESIDENTIAL	
Single-family dwelling units, attached/detached; residential planned unit developments.	Permitted
Multi-family dwelling units (including apartment houses)	Permitted
PUBLIC/INSTITUTIONAL	
School	Conditional
Churches, temples, and other places of worship	Conditional
Childcare Center	Conditional
Parks, playgrounds with appurtenant facilities customarily found in conjunction therewith	Permitted
SERVICE COMMERCIAL	
Hospital (including convalescent home, nursing home, and maternity home)	Conditional
Golf courses	Prohibited
Juvenile halls	Prohibited

3.4.2.3 Development Standards

Standards for the R-3 Zone shall remain consistent with Chapter 22.18, R-3 Limited Density Multiple Residence Zone, unless otherwise specified in this section and Section 3.6, Urban Design Standards, below. The following development standards regulate new site and building development by establishing standards for intensity, building height, open space, and other elements. They have been developed to accommodate the single- and multi-family residential uses anticipated. They are intended to buffer established single-family neighborhoods from non-residential uses, improve connectivity, and provide screening and landscape design along roadways.

Table 3.5 Site Configuration Regulations: R-3 Zone

TYPE	MINIMUM	MAXIMUM
DENSITY		
Multi-family	18 du/ac	30 du/ac
BUILDING SETBACK		
Front Setback	10 ft.	None
Side Setback	5 ft.	None
Rear Setback	10 ft.	None
BUILDING HEIGHT		
Floors	None	3 stories
Building Height	None	40 ft ¹

1. Building height shall exclude chimneys and rooftop antennas.

a. *Landscaping*

- Drought-tolerant landscaping requirements shall be consistent with Section 4.106.5 in Title 31 (Green Building Standards) of the County Code.
- Required Open Space: 200 square feet per dwelling unit for multi-family developments for common recreational-leisure areas, private areas, or a combination of both as follows:
 - »» Minimum dimension for private areas is seven-feet.
 - »» Minimum dimension for common areas is 20-feet.
 - »» Side and rear yards may be included in the calculation of open space, but not required front yard setback area.
 - »» Open space areas shall have no parking, driveway, or right-of-way encroachments.
 - »» All common areas shall be improved as either active or passive facilities, with landscaping or hardscape elements designed to serve the residents of the project. All common areas shall be developed and professionally maintained in accordance with approved landscape and irrigation plans.
 - »» Pedestrian walkways within a project shall be a minimum of four feet in width.
 - »» Private useable open space shall be contiguous to the residential units served and screened for privacy.
 - »» Courtyard internal to a project, or enclosed on at least three sides, shall have a minimum width of 40-feet.
- Front Yard Landscaping
 - »» With the exception of the required paved driveway and walkway, all areas within the street-fronting yard shall be landscaped and maintained.
 - »» Where possible, existing trees shall be maintained.
- Fences
 - »» If perimeter fencing is required for security reasons, a MSPR application shall be required. The Director may approve wrought-iron style fences up to five-feet high that do not obscure views.
 - »» Barbed wire and chain-link fencing are prohibited.

- Utility and Mechanical Equipment
 - »» All utility and mechanical equipment shall be placed in locations that are not exposed to view from the street or shall be screened from view. Satellite dishes shall be located away from public view.
 - »» Screening elements shall be an integral part of the building's design.

b. Circulation and Parking

- Bicycle and pedestrian circulation facilities shall provide connections to surrounding uses in the plan area and to regional bicycle facilities where applicable.
- On-site parking shall not occupy more than 30 percent of any street frontage.
- Parking shall not be located in any required front or side yard area.
- Carports shall be architecturally compatible with the design of the main structures in the project.
- Parking structures shall be architecturally integrated with the project design and shall be screened from view at street level by architectural detailing, façade treatment, artwork, landscaping, or similar visual features to enhance the street façade.
- Use of asphalt for paving driveways and walkways is prohibited.

3.4.3 West Carson Residential 4 (R-4)

3.4.3.1 Purpose and Intent

The West Carson Residential 4 (R-4) Zone is established to provide opportunities for development of high density multiple units, as either apartments or condominiums, up to 50 dwelling units per acre. The intent is to promote desirable high density projects close to transit and other services. The development standards for this designation promote a variety of product types given the range of lot sizes and configurations. This designation is also intended to encourage the development of affordable and workforce housing to serve the needs of the West Carson community, and especially associated with Harbor- UCLA Medical Center.

a. Development Potential Summary

- »» Total Developable Area: 12 acres
- »» Residential: Max. 484 units

3.4.3.2 Use Regulations

The land use regulations that define permitted, conditionally permitted, and prohibited uses within R-4 Medium Density Multiple Residence Zone are consistent with Chapter 22.18 except as noted in Table 3.6, Use Regulations: R-4 Zone, or as noted below under Accessory Uses.

Accessory uses and structures are permitted when customarily associated with, and subordinate to, a permitted use on the same site, and would include:

- »» Administrative office
- »» Caretaker's quarter
- »» Assembly/multi-purpose building
- »» Maintenance/storage facility and structure
- »» Patio cover/trellis
- »» Swimming pool, spa, and Jacuzzi
- »» Tennis court, basketball courts, and other multi-purpose courts, recreation and community buildings.

Figure 3.4 Proposed Zoning: West Carson Residential 4 Areas

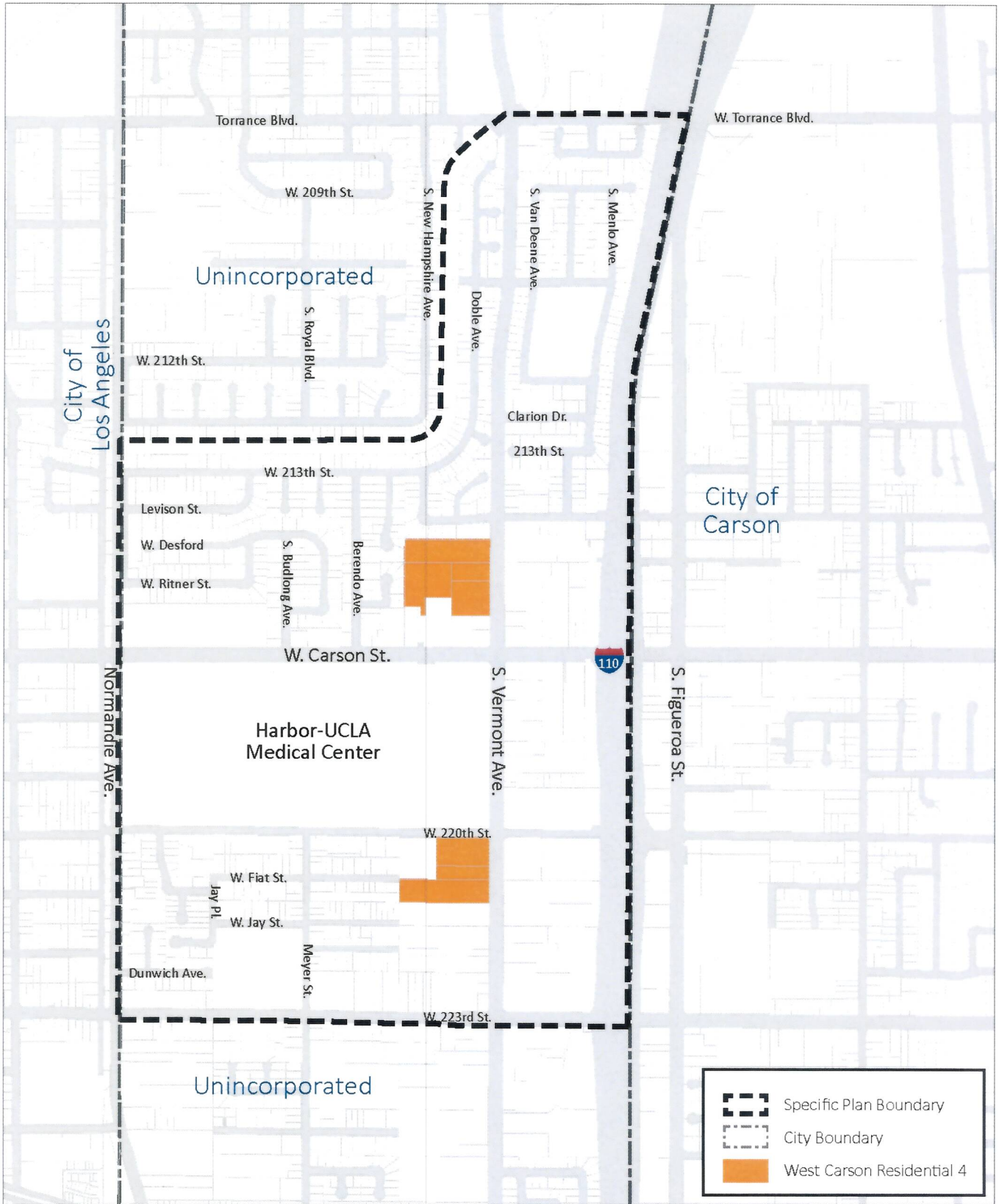


Figure 3.5 Streets with Frontage Requirements

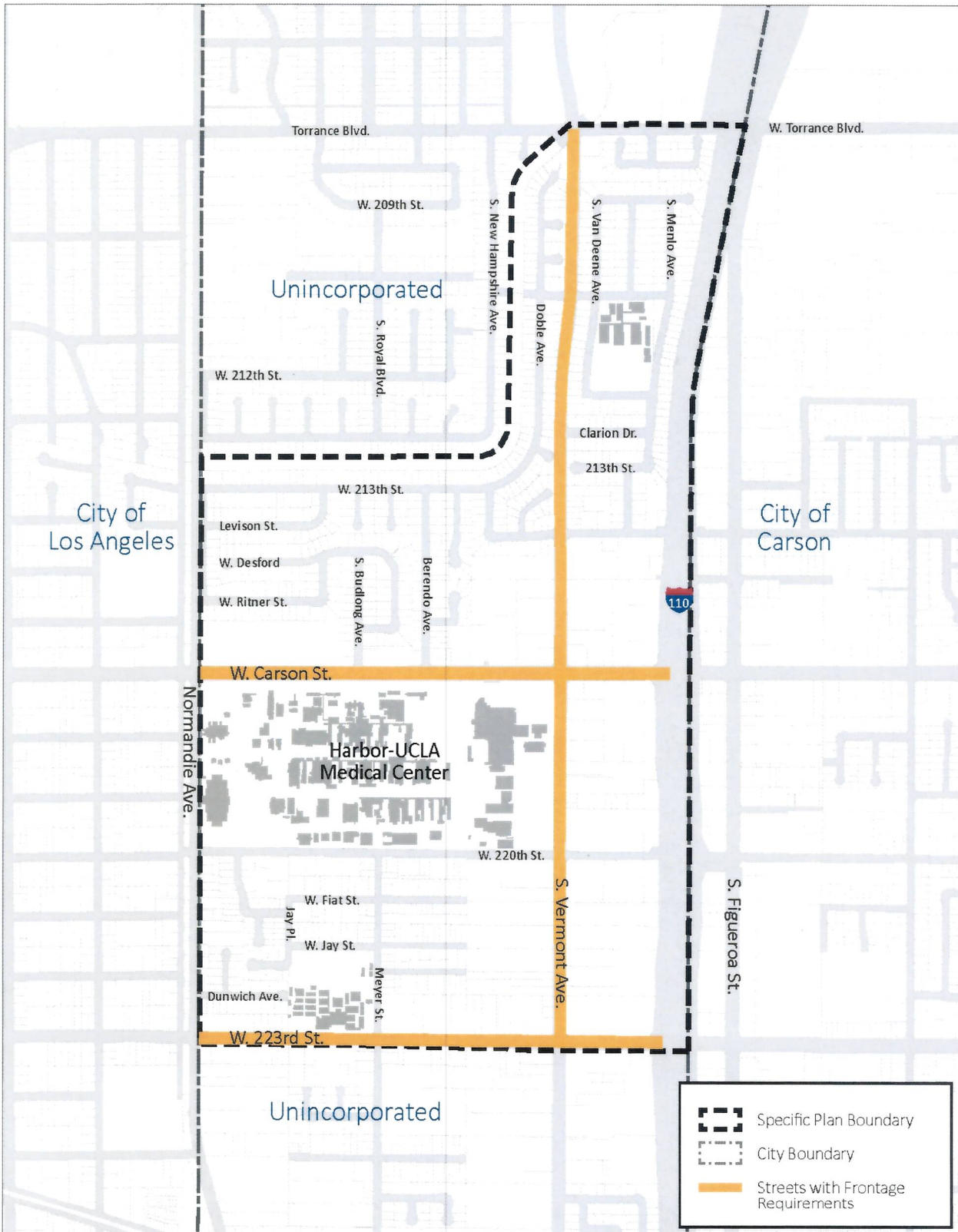


Table 3.6 Use Regulations: R-4 Zone

USE	
RESIDENTIAL	
Multi-family dwelling units (including apartment houses)	Permitted
PUBLIC/INSTITUTIONAL	
Churches, temples and other places of worship	Conditional
School	Conditional
Parks, playgrounds with appurtenant facilities customarily found in conjunction therewith	Permitted
SERVICE COMMERCIAL	
Hospital (including convalescent home, nursing home, and maternity home.)	Conditional
Golf courses	Prohibited
Hotel	Prohibited

3.4.3.3 Development Standards

Standards for the R-4 Zone shall remain consistent with Chapter 22.18, R-4 Medium Density Multiple Residence Zone, unless otherwise specified in this section and Section 3.6, Urban Design Standards, below. The following development standards regulate new site and building development by establishing standards for intensity, building height, open space, and other elements. They have been developed to accommodate the multi-family residential uses anticipated.

Table 3.7 Site Configuration Regulations: R-4 Zone

TYPE	MINIMUM	MAXIMUM
DENSITY		
Multi-family	30 du/ac	50 du/ac
BUILDING SETBACK		
Front Setback	10 ft.	None
BUILDING HEIGHT		
Floors	None	4 stories
Building Height	None	50 ft ¹

1. Building height shall exclude chimneys and rooftop antennas.

a. *Frontages*

All new development within the R-4 Zone adjacent to a street with frontage requirements, as shown in Figure 3.5, Streets with Frontage Requirements, shall have a primary building façade and entry from the identified street and shall adhere to the following building frontage requirements. These frontages dictate the relationship between the street (back of right-of-way) and the façade of the ground floor of the building (see Section 3.6.2, Building Design, for building frontage design standards and guidelines).

- Building orientation shall be determined by the location of the primary entrance, which shall indicate the front of the building.
- Where not a designated frontage, all building sides abutting major public rights-of-way shall require architectural treatment.

- Pedestrian access to public right-of-way is required either through common corridors or courtyards from buildings adjacent to the road.
- Open fencing is allowed, but a solid wall higher than three-feet is not allowed.

Table 3.8 Frontage Categories: R-4 Zone

TYPE	VERMONT AVENUE
Shopfront	Permitted
Forecourt	Permitted
Gallery	Permitted
Arcade	Permitted
Terrace	Permitted
Stoop	Permitted

See Section 3.6.2, Building Design, for building frontage design standards and guidelines.

b. Built Form & Landscaping

- Residential development shall provide adequate private or common laundry facilities reserved for the exclusive use of the residents residing in the development.
- Use of asphalt for paving driveways and walkways is prohibited.
- Required Open Space: 200 square feet per dwelling unit for multi-family developments for common recreational-leisure areas, private areas, or a combination of both as follows:
 - »» Minimum dimension for private areas is seven-feet.
 - »» Minimum dimension for common areas is 20-feet.
 - »» Side and rear yards may be included in the calculation of open space, but not the required front yard setback area.
 - »» Open space areas shall have no parking, driveway, or right-of-way encroachments.
 - »» All common space areas shall be improved as either active or passive facilities, with landscaping or hardscape elements designed to serve the residents of the project, and should have strong connections to building entries and pedestrian paseos.

- »» All common areas shall be developed and professionally maintained in accordance with approved landscape and irrigation plans.
- »» Pedestrian walkways within a project shall be a minimum of four-feet in width.
- »» Private useable open space shall be contiguous to the residential units served and screened for privacy.
- »» Courtyard internal to a project, or enclosed on at least three sides, shall have a minimum width of 40-feet.
- Site Landscaping
 - »» A minimum of 20 percent of the project site shall be landscaped or hardscaped with open, useable outdoor space.
 - »» With the exception of the required paved driveway and walkway, all areas within the street-fronting setback area shall be landscaped and maintained.
 - »» Where possible, existing trees shall be maintained.
 - »» Post-construction landscape designs shall comply with all Green Building Landscape Design standards per Section 4.106.5 in Title 31 (Green Building Standards) of the County Code. Specifically:
 - » Turf areas shall not exceed 25 percent of the total landscaped area.
 - » Non-invasive drought-tolerant plant and tree species appropriate for the climate zone region shall be utilized in at least 75 percent of the total landscaped area.
 - » Hydrozoning irrigation techniques shall be incorporated into the landscape design.
- Fences
 - »» If perimeter fencing is required for security reasons, a MSPR application shall be required. The Director may approve wrought-iron style fences up to five feet high that do not obscure views.

- »» Barbed wire and chain-link fencing are prohibited.
- Utility and Mechanical Equipment
 - »» All utility and mechanical equipment shall be placed in locations that are not exposed to view from the street or shall be screened from view. Satellite dishes shall be located away from public view.
 - »» Screening elements shall be an integral part of the building's design.
- c. *Circulation and Parking*
 - Bicycle and pedestrian circulation facilities shall provide connections to surrounding uses and to the plan area networks.
 - On-site parking shall not occupy more than 30 percent of the total frontage on any linear front or side street.
 - Parking shall not be located in the street-fronting yard area.
 - Carports shall be architecturally compatible with the design of the main structures in the project.
 - Parking structures shall be architecturally integrated with the project design and shall be screened from view at street level by architectural detailing, façade treatment, artwork, landscaping, or similar visual features to enhance the street façade.

3.4.4. Residential Planned Development

3.4.4.1 Purpose and Intent

The Residential Planned Development Zone is established, consistent with Section 22.18, Residential Planned Development (RPD) in Title 22, to promote residential amenities beyond those expected under conventional single-family development, to achieve greater flexibility in design, to encourage well-planned neighborhoods through creative and imaginative planning as a unit, and to provide for appropriate use of land that is sufficiently unique in its physical characteristics or other circumstances to warrant special methods of development.

a. *Development Potential Summary*

- »» Total Developable Area: 5 acres
- »» Residential: 88 dwelling units

3.4.4.2 Use Regulations

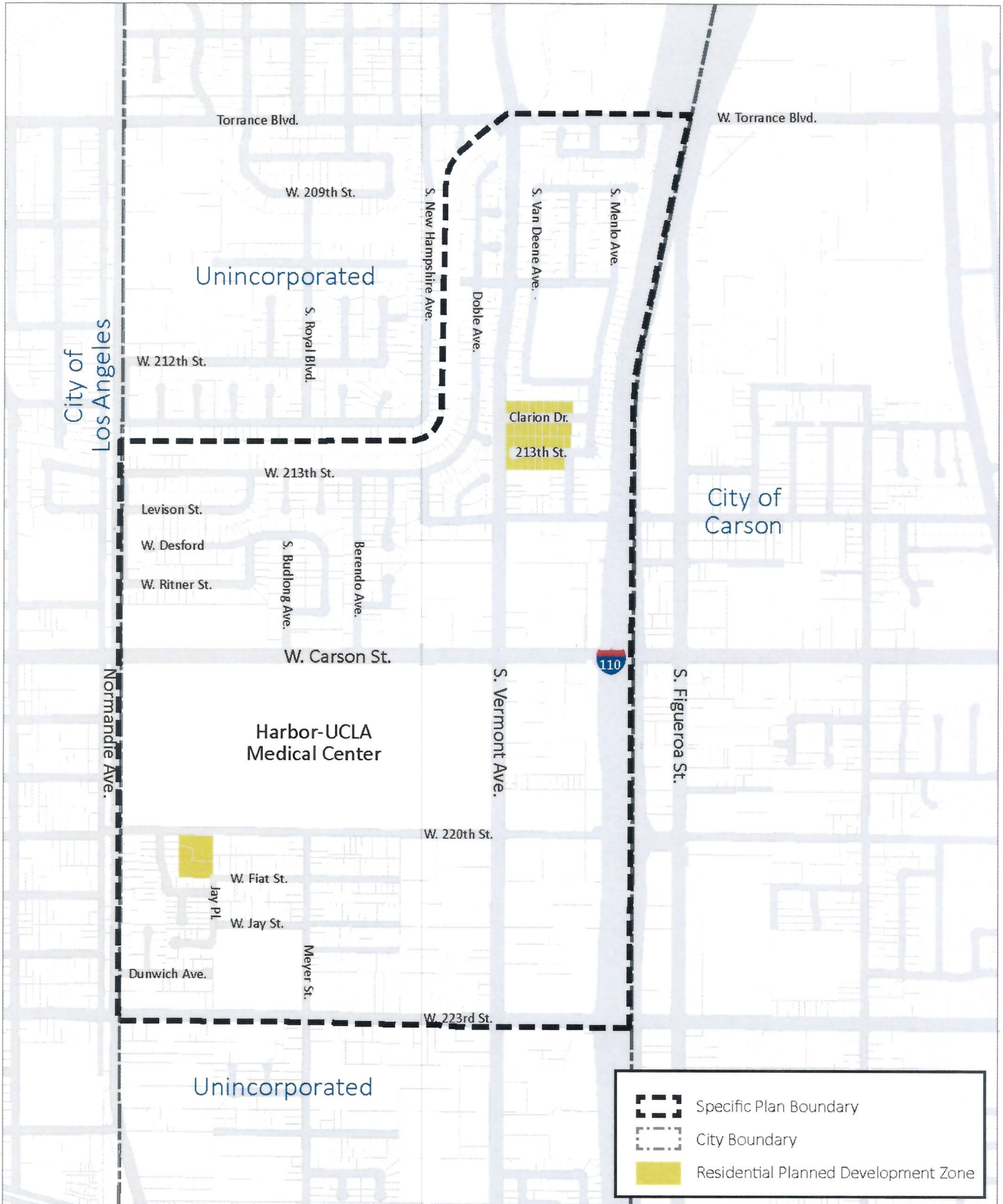
The land use regulations that define permitted, conditionally permitted, and prohibited uses within RPD Zone shall remain consistent with Chapter 22.18 in Title 22.

Accessory uses and structures are permitted when customarily associated with, and subordinate to, a permitted use on the same site.

3.4.4.3 Development Standards

Standards for the RPD Zone shall remain consistent with Chapter 22.18 (RPD Zone) in Title 22 of the County Code.

Figure 3.6 Proposed Zoning: Residential Planned Development Areas



3.4.5 Neighborhood Commercial (NC)

3.4.5.1 Purpose and Intent

The Neighborhood Commercial Zone (NC Zone) is established to serve the local retail and service needs of the residents in West Carson. This zone is suited for the two- to five-acre shopping center, often anchored by a grocery store, with ancillary goods, services, and restaurants that serve the daily needs of nearby residents. The intent is to maintain and promote the continuation of the neighborhood-service commercial center along the northern edge of the Study Area at Vermont Avenue. The NC Zone allows for the development of a range of neighborhood- serving retail and service uses.

a. *Development Potential Summary*

- »» Total Developable Area: 1 acre
- »» Non-residential: 14,787 sq. ft.

3.4.5.2 Use Regulations: NC Zone

The land use regulations define permitted, conditionally permitted, and prohibited uses within the NC Zone, as shown in Table 3.9, Use Regulations: NC Zone. Use regulations for the NC Zone shall remain consistent with Chapter 22.18, C-2 Neighborhood Business, unless otherwise specified in this section.

Accessory uses are permitted within a detached subordinate building or structure, the use of which is customarily identical to that of the main building or the main use of the land, and which is located in the same or a less restrictive zone on the same lot or parcel of land with the main building or use. A MSPR application shall be required for any proposed accessory uses. The Director may approve accessory uses that are consistent with the use regulations and development standards of the NC Zone.

Figure 3.7 Proposed Zoning: Neighborhood Commercial Areas

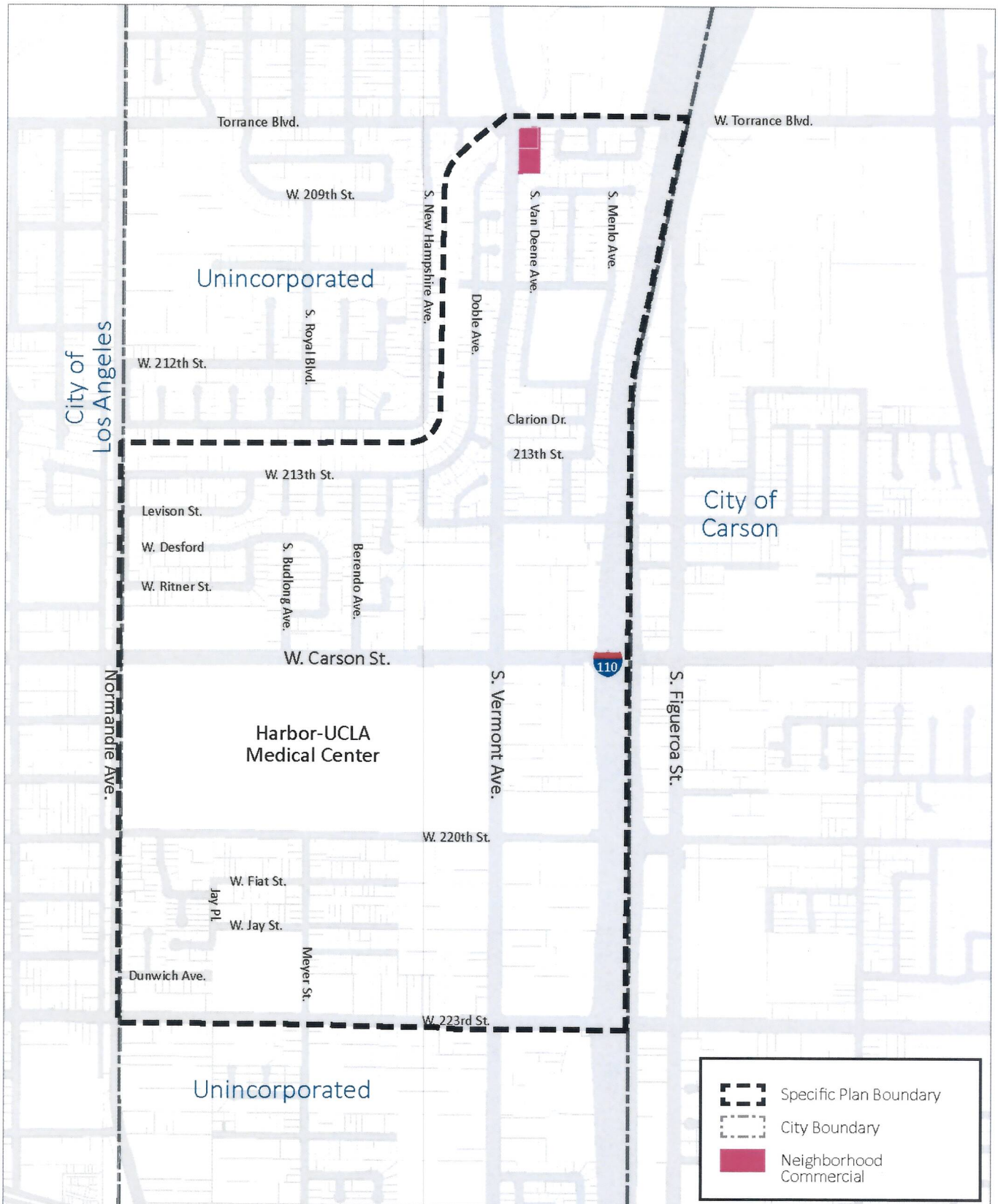


Table 3.9 Use Regulations: NC Zone

USE	
RESIDENTIAL	
Single-family dwelling units, attached/detached; residential planned unit developments.	Prohibited
Mixed use developments (commercial/residential)	Conditional
Multi-family dwelling units (including apartment houses)	Conditional
PUBLIC/INSTITUTIONAL	
Schools	Conditional
Fire station	Conditional
SERVICE COMMERCIAL	
Acute care/rehabilitation uses	Conditional
Alternative Financial Service	Prohibited
Automobile service station	Conditional
Automobile battery and repair shops, painting/auto body, muffler, washing, or sales.	Prohibited
Amusement rides and devices	Prohibited
Bulk recycling vending	Prohibited
Car wash	Prohibited
Drive-through establishments and drive-through lanes	Prohibited
Smoking-oriented, tobacco, pipe, and vape shops	Prohibited
Pawn shops	Prohibited
Union halls	Prohibited

3.4.5.3 Development Standards

Standards for the NC Zone shall remain consistent with Chapter 22.20 (C-2 Neighborhood Business Zone) in Title 22 of the County Code unless otherwise specified in this section and Section 3.6, Urban Design Standards, below. The following development standards regulate new site and building development by establishing standards for intensity, building height, open space, and other elements. They have been developed to accommodate the neighborhood-serving retail and service uses anticipated. They are intended to maintain and promote the continuation of the neighborhood-service commercial center along the northern edge of the study area at Vermont Ave.

Table 3.10 Site Configuration Regulations: NC Zone

TYPE	MINIMUM	MAXIMUM
FLOOR AREA RATIO (FAR)		
	None	.35
BUILDING SETBACK		
Carson Street	10 ft.	None
Torrance Boulevard	10 ft.	None
Interior Yard (Side or Rear)	0 ft.	None
Interior Yard Adjacent to Residential (Side or Rear)	15 ft.	None
BUILDING HEIGHT		
Building Height	None	45 ft. ¹

1. Building height shall exclude chimneys and rooftop antennas.

a. *Landscaping*

- Site Landscaping

- »» A minimum of 20 percent of the lot shall be landscaped with trees, shrubs, ground cover, flowering perennials and biennials, and shall be continuously maintained in good condition. Parking lot landscaping does not count toward this requirement. Pedestrian walkways, plazas, and outdoor dining areas may be developed in the landscape area.

Setback areas may be included in this requirement.

- »» Chapter 22.126 establishes site tree requirements, including those for surface parking lots.

- Sites with multiple buildings shall be clustered to the extent possible with shared outdoor spaces and with direct pedestrian access between uses, from parking areas, and from the street.
 - Buildings shall be oriented to the street.
- b. *Circulation & Parking*
- Bicycle and pedestrian circulation facilities shall provide connections to surrounding uses and to the plan area networks.

3.4.6 Unlimited Commercial (UC)

3.4.6.1 Purpose and Intent

The Unlimited Commercial Zone (UC Zone) is established to meet the commerce and service needs of the resident and business community of West Carson, while ensuring compatibility with adjacent land uses. Given proximity to the elementary school and single-family neighborhoods, multi-family residential is also allowed in a stand-alone or mixed-use configuration up to 30 dwelling units per acre. The intent is to encourage a range of retail, personal, and professional services, as well as multi-family residential in a well-designed, walkable environment.

a. *Development Potential Summary*

- »» Total Developable Area: 4 acres
- »» Non-residential: 50,620 square feet
- »» Residential: 30 units

3.4.6.2 Use Regulations: UC Zone

The land use regulations define permitted, conditionally permitted, and prohibited uses within the UC Zone, as shown in Table 3.11, Use Regulations: UC Zone. Use regulations for the Unlimited Commercial Zone shall remain consistent with Chapter 22.20, Part 4 (C-3 General Commercial Zone) in Title 22 of the County Code unless otherwise specified in this section.

Accessory uses and structure are permitted when customarily associated with, and subordinate to, a permitted use on the same site.

Table 3.11 Use Regulations: UC Zone

USE	
RESIDENTIAL	
Single-family dwelling units, attached/detached; residential planned unit developments.	Prohibited
Mixed use developments (commercial/residential)	Permitted
Multi-family dwelling units (including apartment houses)	Permitted
PUBLIC/INSTITUTIONAL	
Schools	Permitted
Preschool or childcare facility	Conditional

Figure 3.8 Proposed Zoning: Unlimited Commercial Areas

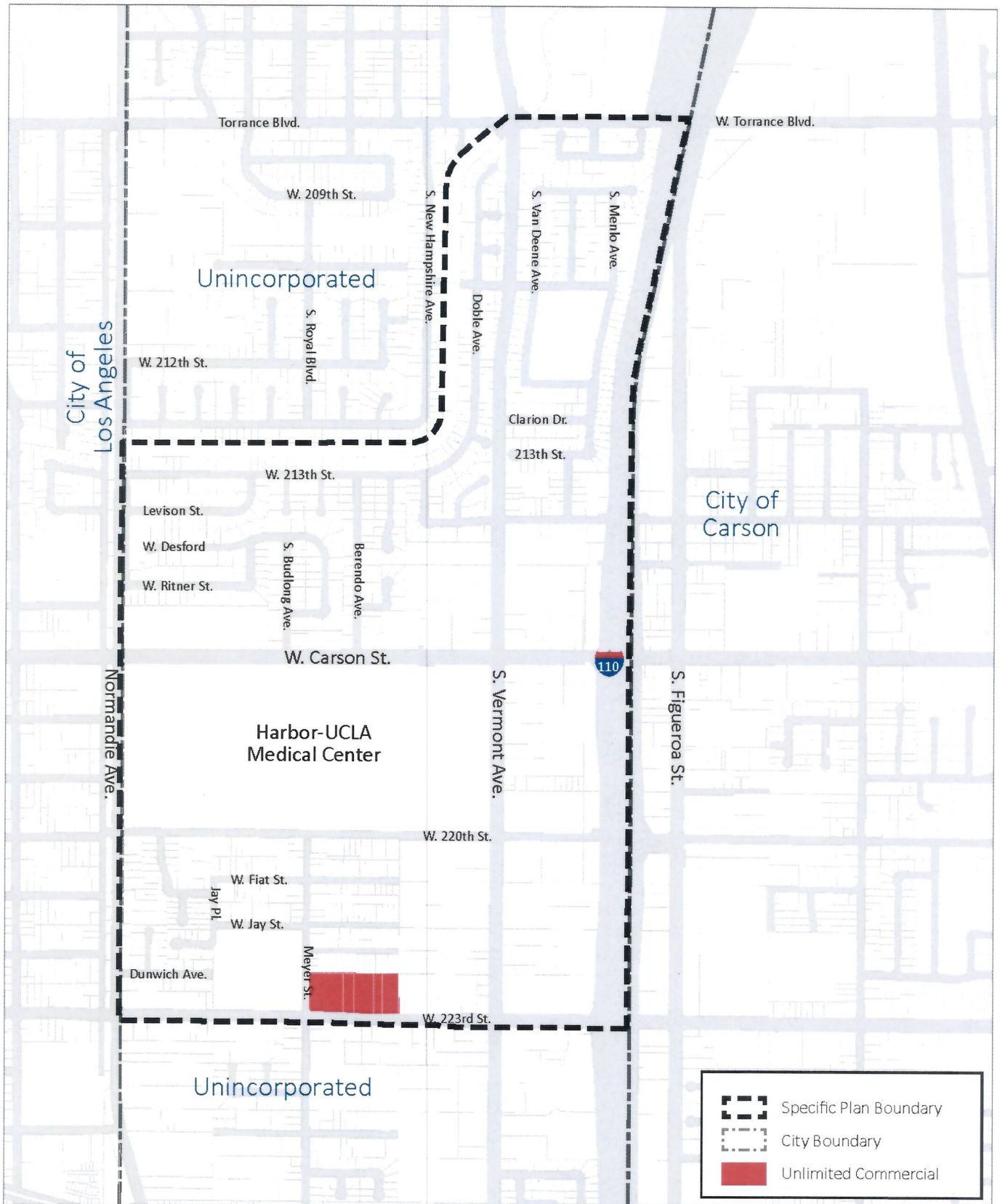


Table 3.11 Use Regulations: UC Zone

USE	
COMMERCIAL SALES	
Auction houses	Prohibited
Boat and other marine sales	Prohibited
Mobilehome sales	Prohibited
Recreational vehicle sales	Prohibited
Trailer sales, box, and utility	Prohibited
SERVICE COMMERCIAL	
Automobile related uses including repair, battery, painting/auto body, muffler, service, washing, sales, or rental.	Prohibited
Alternative Financial Service	Prohibited
Recreational vehicle/Truck rentals	Prohibited
Pawn Shops	Prohibited

3.4.6.3 Development Standards

Standards for the UC Zone shall remain consistent with Chapter 22.20, C-3 General Commercial Zone, unless otherwise specified in this section and Section 3.6, Urban Design Standards, below. The following development standards regulate new site and building development by establishing standards for intensity, building height, open space, and other elements. They have been developed to accommodate the retail, personal and professional services, and multi-family residential uses anticipated.

a. *Built Form & Landscaping*

- Drought-tolerant landscaping requirements shall be consistent with Section 4.106.5 in Title 31 (Green Building Standards) of the County Code.
- When sharing an interior property line with an existing single-family zone, the following requirements shall apply:
 - »» Windows, balconies, or similar openings shall be oriented to minimize any direct line-of-sight into adjacent units or onto private patios or backyards adjoining the property line.
 - »» The third floor shall be stepped back by a minimum of 10-feet.

Table 3.12 Site Configuration Regulations: UC Zone

TYPE	MINIMUM	MAXIMUM
DENSITY		
Multi-family	None	30 du/ac
FLOOR AREA RATIO (FAR)		
All Buildings	None	0.5
BUILDING SETBACK		
West 223rd Street	10 ft.	25 ft ¹
Meyler Street	10 ft.	25 ft
Interior Yard (Side or Rear)	0 ft.	None
Interior Yard Adjacent to Residential (Side or Rear)	15 ft.	None
BUILDING HEIGHT		
Floors		3 stories ²
Building Height	None	40 ft ^{3,4}

1. For 60% of the property frontage.
2. Building height shall exclude chimneys, rooftop antennas, and rooftop recreational spaces.
3. Excluding signs which are permitted by Part 10 of Chapter 22.114, chimneys and rooftop antennas.
4. Minimum floor-to-ceiling height of the ground floor for a retail use in a mixed-use development shall be 15 feet.

- New development fronting 223rd Street shall not exceed 40-feet and three stories in height. In addition, new development, including setbacks and stepbacks, will be a maximum of 50 feet in depth from the property line along the street.
- Site Landscaping:
 - »» A minimum of 20 percent of the lot shall be landscaped with trees, shrubs, ground cover, flowering perennials and biennials, and shall be continuously maintained in good condition.
 - » Parking lot landscaping does not count toward this requirement.

- » Pedestrian walkways, plazas, and outdoor dining areas may be developed in the landscape area.
- » Setback areas may be included this requirement.
- »» Tree planting and maintenance requirements shall be consistent with Chapter 22.126, Tree Requirements, except as follows:
 - » A minimum of three trees shall be planted for every 10,000 square feet of developed lot area regardless of number of parking spaces.
 - » Wherever possible, existing trees shall be maintained.
- »» With the exception of the required paved driveway and walkway, all areas within the street-fronting setback area shall be landscaped and maintained.
- Required Open Space: 200 square feet per dwelling unit for multi-family developments for common recreational-leisure areas, private areas, or a combination of both.
 - »» Minimum dimension for private areas is seven-feet.
 - »» Minimum dimension for common areas is 20-feet.
 - »» Side and rear yards may be included in the calculation of open space, but not the required front yard setback area.
 - »» Open space areas shall have no parking, driveway, or right- of-way encroachments.
 - »» All common space areas shall be improved as either active or passive facilities, with landscaping or hardscape elements designed to serve the residents of the project and should have strong connections to building entries and pedestrian paseos.
- b. *Circulation & Parking*
 - Bicycle and pedestrian circulation facilities shall provide connections to surrounding uses and to the plan area networks.

3.4.7 Industrial Flex (IF)

3.4.7.1 Purpose and Intent

The Industrial Flex District is intended to allow opportunities for non-industrial uses, including commercial, office, and multi-family residential uses, where appropriate, to occur within the light industrial areas south of 220th Street. The Industrial Flex District recognizes the transition that is occurring in the West Carson area from traditional, small-scale light industrial uses, to a broader range of uses to serve the community, including service commercial uses, professional and medical office, and multi-family residential. Over time, the Industrial Flex District will provide a more compatible transition to the existing single-family and multi-family residential areas. Residential uses are intended to provide for housing options and affordability, particularly workforce housing and dwelling units in proximity to both employment uses and transit. The development standards for this designation include conditions for allowing multi-family residential, including minimum project size and adjacency requirements to address potential compatibility concerns.

a. *Development Potential Summary*

- »» Total Developable Area: 22 acres
- »» Non-Residential: 1,133,779 sq. ft.
- »» Residential: Max. 486 units

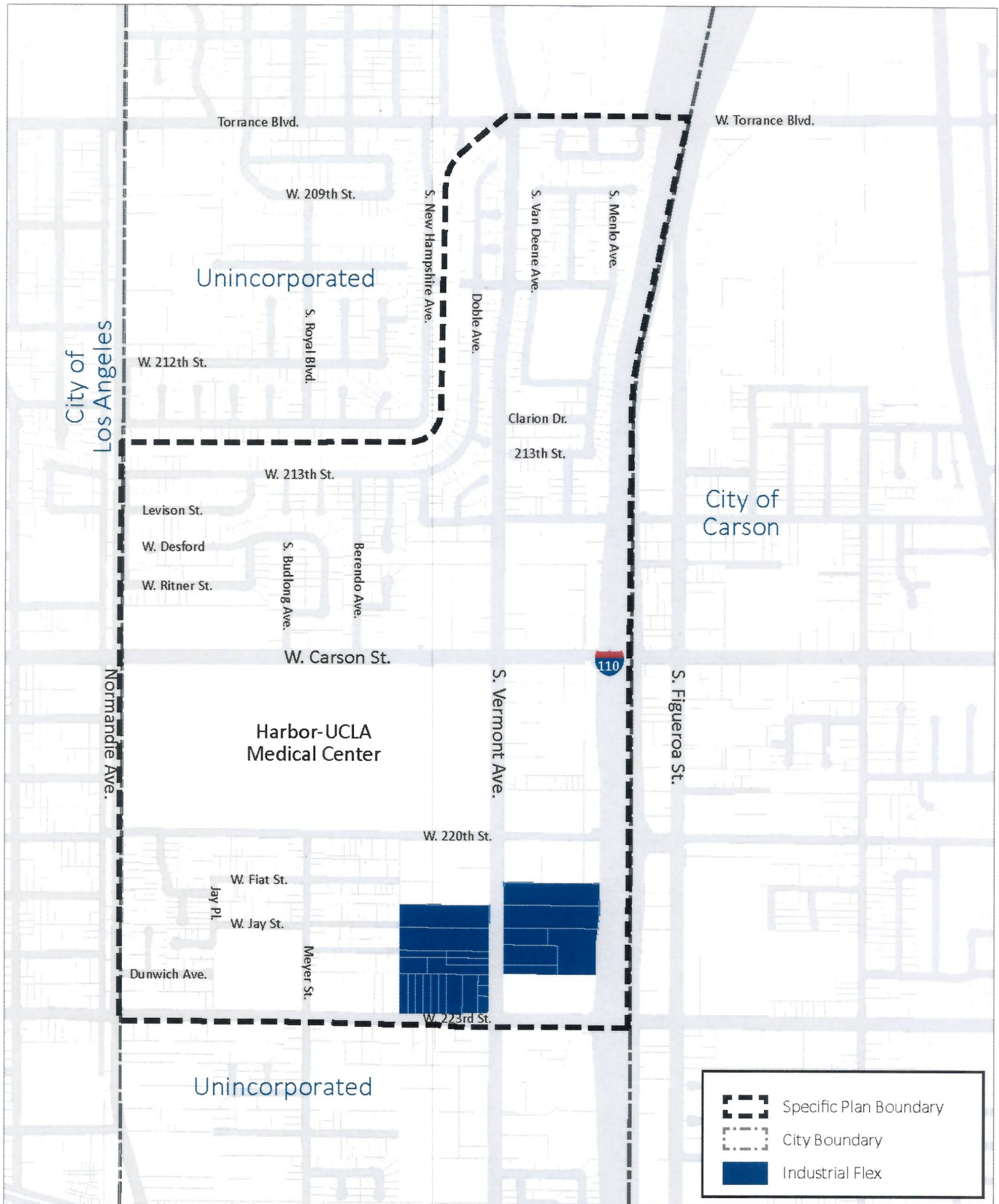
3.4.7.2 Use Regulations: IF Zone

The land use regulations define permitted, conditionally permitted, and prohibited uses in the Industrial Flex zone, as shown in Table 3.13, Regulations (IF). Standards for the Industrial Flex Zone shall remain consistent with Chapter 22.22 (M-1 Light Manufacturing Zone) in Title 22 of the County Code unless otherwise specified in this section.

Accessory uses and structures are permitted when customarily associated with, and subordinate to, a permitted use on the same site, and would include:

- »» Administrative office
- »» Assembly/multi-purpose building
- »» Caretaker's quarter
- »» Enclosed, screened, outdoor storage
- »» Maintenance/storage facility and structure
- »» Patio cover/trellis

Figure 3.9 Proposed Zoning: Industrial Flex Areas



- »» Swimming pool, spa, and Jacuzzi
- »» Tennis court, basketball courts, and other multi-purpose courts, recreation and community buildings

Table 3.13 Use Regulations: IF Zone

USE	
RESIDENTIAL	
Mixed use developments (commercial/residential)	Permitted
Multi-family dwelling units (including apartment houses)	Permitted
Single-family dwelling units, attached/detached; residential planned unit developments.	Prohibited
PUBLIC/INSTITUTIONAL	
Churches, temples and other places of worship	Permitted
Hospital (including convalescent home, nursing home, and maternity home.)	Conditional
Parks, playgrounds with appurtenant facilities customarily found in conjunction therewith	Permitted
School	Conditional
SERVICE COMMERCIAL	
Acute care/rehabilitation uses	Conditional
Alternative Financial Service	Prohibited
Automobile service station	Prohibited
Automobile related uses including repair, battery, painting/auto body, muffler, service, washing, sales, or rental.	Prohibited
Amusement rides and devices	Prohibited
Health club/gymnasium	Permitted
Hotel	Permitted
Movie theater	Permitted
Alcoholic beverage sales	Conditional

Table 3.13 Use Regulations: IF Zone

USE	
Restaurant, family, specialty, and food hall	Permitted
Theater, including live performance	Permitted
Drive-through establishments and drive-through lanes	Prohibited
Pawn Shops	Prohibited
Smoking-oriented including tobacco, pipe, and vape shops	Prohibited
INDUSTRIAL	
Assembly plants	Prohibited
Auction houses	Prohibited
Concrete batching	Prohibited
Heavy industrial uses, including distribution	Prohibited
Light industrial use, general (including fabrication, manufacturing, and assembly) directly adjacent to residential.	Conditional
Meat markets/slaughter	Prohibited
Moving van/operations yards	Prohibited
Oil wells and accessory facilities	Prohibited
Recreational vehicle sales and rentals	Prohibited
Refrigeration plants	Prohibited
Technology exchange/transfer service	Permitted
OFFICE	
General office	Permitted
Medical office/healthcare center	Permitted
Professional office	Permitted

3.4.7.3 Development Standards

Standards for the Industrial Flex Zone shall remain consistent with Chapter 22.22 (M-1 Light Manufacturing Zone) in Title 22 of the County Code, unless otherwise specified in this section and Section 3.6, Urban Design Standards, below. The following development standards regulate new site and building development by establishing standards for intensity, building height, open space, and other elements. They have been developed to accommodate the light industrial, limited commercial retail, and multi-family residential uses anticipated.

a. Frontages

All new development adjacent to a street with frontage requirements as shown in Figure 3.5, Streets with Frontage Requirements, shall have a primary building façade and entry from the identified street and shall adhere to the following

Table 3.14 Site Configuration Regulations: IF Zone

TYPE	MINIMUM	MAXIMUM
DENSITY		
Multi-family	30 du/ac	70 du/ac
FLOOR AREA RATIO (FAR)		
All Buildings	0.25	2.5
BUILDING SETBACK		
West 223rd Street	10 ft.	25 ft. ¹
Interior Yard (Side or Rear)	0 ft.	None
Interior Yard Adjacent to Residential (Side or Rear)	15 ft.	None
BUILDING HEIGHT		
Floors		4 stories ²
Building Height	None	50 ft. ³

1. For 60% of the property frontage.
2. Building height shall exclude chimneys, rooftop antennas, and rooftop recreational spaces.
3. Minimum floor-to-ceiling height of the ground floor for a retail use in a mixed-use development shall be 15 feet.

building frontage requirements. These frontages dictate the relationship between the street (back of right-of-way) and the façade of the ground floor of the building (see Section 3.6.2, Building Design, for building frontage design standards and guidelines).

- »» Building orientation shall be determined by the location of the primary entrance, which shall indicate the front of the building.
- »» All building sides abutting major public rights-of-way shall require architectural treatment.
- »» Pedestrian access to public right-of-way is required either through common corridors or courtyards from buildings adjacent to the road.
- »» Open fencing is allowed, but a solid wall greater than three-feet high shall not be allowed.

Table 3.15 Frontage Categories: IF Zone

TYPE	VERMONT AVENUE	223 RD STREET
Shopfront	Permitted	Permitted
Forecourt	Permitted	Permitted
Gallery	Permitted	Permitted
Arcade	Permitted	Not permitted
Terrace	Permitted	Permitted
Stoop	Not permitted	Permitted

See Section 3.6.2, Building Design, for building frontage design standards and guidelines.

b. *Built Form & Landscaping*

- Multi-family residential units shall only be allowed under specific conditions to address potential compatibility concerns as follows:
 - »» Minimum project size of two-acres whichever is greater.
 - »» Adjacency uses are non-industrial or light industrial uses that do not emit excessive emissions, odor, noise, or vibration.
 - »» Environmental remediation of the land if required.
- Site Landscaping
 - »» Drought-tolerant landscaping requirements shall be consistent with Section 4.106.5 in Title 31 (Green Building Standards) of the County Code.
 - »» A minimum of 20 percent of the lot shall be landscaped with trees, shrubs, ground cover, flowering perennials and biennials, and shall be continuously maintained in good condition.
 - » Parking lot landscaping does not count toward this requirement.
 - » Pedestrian walkways, plazas, and outdoor dining areas may be developed in the landscape area.
 - » Setback areas may be included this requirement.
 - »» Tree planting and maintenance requirements shall be consistent with Chapter 22.126, Tree Requirements, except as follows:
 - »» A minimum of three trees shall be planted for every 10,000 square feet of developed lot area regardless of number of parking spaces.
 - »» Wherever possible, existing trees shall be maintained.
 - »» With the exception of the required paved driveway and walkway, all areas within the street-fronting setback area shall be landscaped and maintained.
 - »» Required Open Space: 200 square feet per dwelling unit for multi-family developments for common recreational-leisure areas, private areas, or a combination of both.
 - » Minimum dimension for private areas is seven-feet.

-) Minimum dimension for common areas is 20-feet.
-) Side and rear yards may be included in the calculation of open space, but not the required front yard setback area.
-) Open space areas shall have no parking, driveway, or right-of-way encroachments.
-) All common space areas shall be improved as either active or passive facilities, with landscaping or hardscape elements designed to serve the residents of the project and should have strong connections to building entries and pedestrian paseos.

- Lighting

- »» All outdoor lighting systems shall be directed away from the window of a residential uses.

- c. *Circulation & Parking*

- Bicycle and pedestrian circulation facilities shall provide connections to surrounding uses and to the plan area networks.
- Provisions for common vehicular access points and shared parking shall be encouraged and coordinated with any adjacent/proximate development plans where practical.

- d. *Special Requirements*

- When sharing an interior property line with an existing single-family zone, the following requirements shall apply:
 - »» Windows, balconies, or similar openings shall be oriented to minimize any direct line-of-sight into adjacent units or onto private patios or backyards adjoining the property line.
 - »» The third and fourth floor shall be stepped back by a minimum of 10-feet.
- Residential units may not be located within 500-feet of the edge of the freeway right-of-way. Other uses such as parking are allowed.
- Developments greater than 100 units will be conditioned to provide public benefit to serve the greater community, including public open space, street trees, public art, community/entrance monument, public open space furnishings, or other features needed within the plan area and approved by the Director.

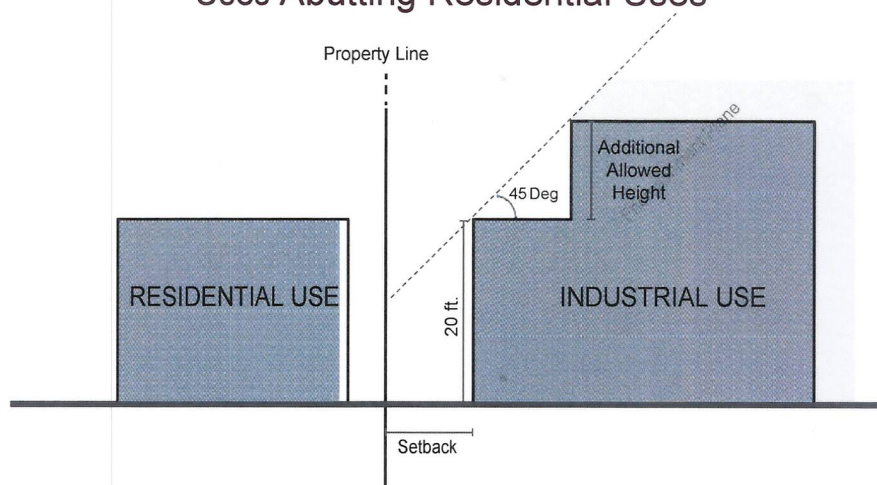
- An application for new construction or expansion of development within the IF Zone shall be subject to the requirements included in Section 22.26.020 for review of the project's impacts on infrastructure.

e. *Special Requirements: Adjacency to Residential Use*

If an industrial use is next to a residential use, then these additional standards apply:

- **Building Height:** Any new industrial building or structure shall be within an encroachment plane sloping upward and inward at a 45 degree angle commencing 20-feet above the existing grade at the inside line of the side yard setback as illustrated in Figure 3.10.

Figure 3.10 Encroachment Plane for Industrial Uses Abutting Residential Uses



- **Enclosures:** Materials and equipment that emit dust, smoke, gas, fumes, cinder, or refuse matter shall be completely enclosed and mechanically ventilated to prevent fugitive emissions unless another regulatory agency requires natural ventilation. Stacks, vents, and flares are exempt from the enclosure requirement.
- **Yard Setback:** A new industrial building, structure, or addition shall provide the yard setbacks identified in Table 3.16 when abutting a residential use.

**Table 3.16 Yard Setback for New Industrial Uses
Abutting a Residential Use**

TYPE	FRONT	SIDE	REAR
MINIMUM REQUIRED SETBACK			
Lot depth > 100 ft	15 ft.	-	15 ft.
Lot depth ≤ 100 ft	5 ft.	-	5 ft.
Lot width ≥ 50 ft	-	15 ft.	-
Lot width > 30 ft < 50 feet	-	5 ft.	-
Lot width ≤ 30 ft	-	3 ft.	-

- Noise generating uses or activities such as fabrication, manufacturing, and assembly, shall not exceed the presumed ambient noise level specified in Chapter 12.08 of Title 12 of the County Code.
 - »» Applicants shall submit to Regional Planning an acoustic evaluation report issued by a licensed noise consulting professional which identifies compliance options for noise mitigation. Applicants shall comply with the stated performance-based mitigation measures.
 - »» Baseline and other ambient noise levels shall be measured at the property line. If the ambient sound levels at the site exceed the allowable ambient levels, the existing site's ambient level becomes the new allowable baseline and no increase in that level shall be allowed.
- For new residential development next to industrial use, required common open space shall be fully buffered from an industrial use by a building on the same project site.

3.4.8 Harbor-UCLA Medical Zone

3.4.8.1 Purpose and Intent

The Harbor-UCLA Medical Zone is established to support the existing and future needs of the Harbor-UCLA Medical Center campus while ensuring compatibility with adjacent land uses. The intent is to accommodate the Harbor-UCLA Master Plan, which seeks to maintain, improve, and expand the hospital facilities; clinic facilities; medical office; research and development; community-serving uses and facilities; and associated supportive uses such as transitional housing, incidental retail, parking, and public open space. It is also intended to encourage expanded pedestrian walkways to improve connectivity between the Harbor-UCLA Medical Center, LA BioMed research and development facility, Carson Street uses, Carson Street Station, and the West Carson community.

a. *Development Potential Summary*

- »» Total Developable Area: 71 acres
- »» Non-Residential: 1,941,339 sq. ft.
- »» Residential: Max. 100 units

3.3.8.2 Use Regulations (Harbor-UCLA Medical)

- Uses shall be consistent with the Harbor-UCLA Medical Center Campus Master Plan.

3.4.8.3 Development Standards

The following development standards regulate new site and building development by establishing standards for intensity, building height, open space, and other elements. They have been developed to accommodate the mix of commercial, office, and other hospital-supportive uses anticipated.

b. *Built Form & Landscaping*

- Site, building, and landscaping design shall be consistent with the Harbor-UCLA Medical Center Campus Master Plan and as follows:
- Landscaping
 - »» Large facades/walls of structures within 20-feet of a roadway, such as parking structures, operations plant, or other buildings, shall be screened with trees, large shrubs, and other vegetation to soften and buffer massing from the surrounding community.

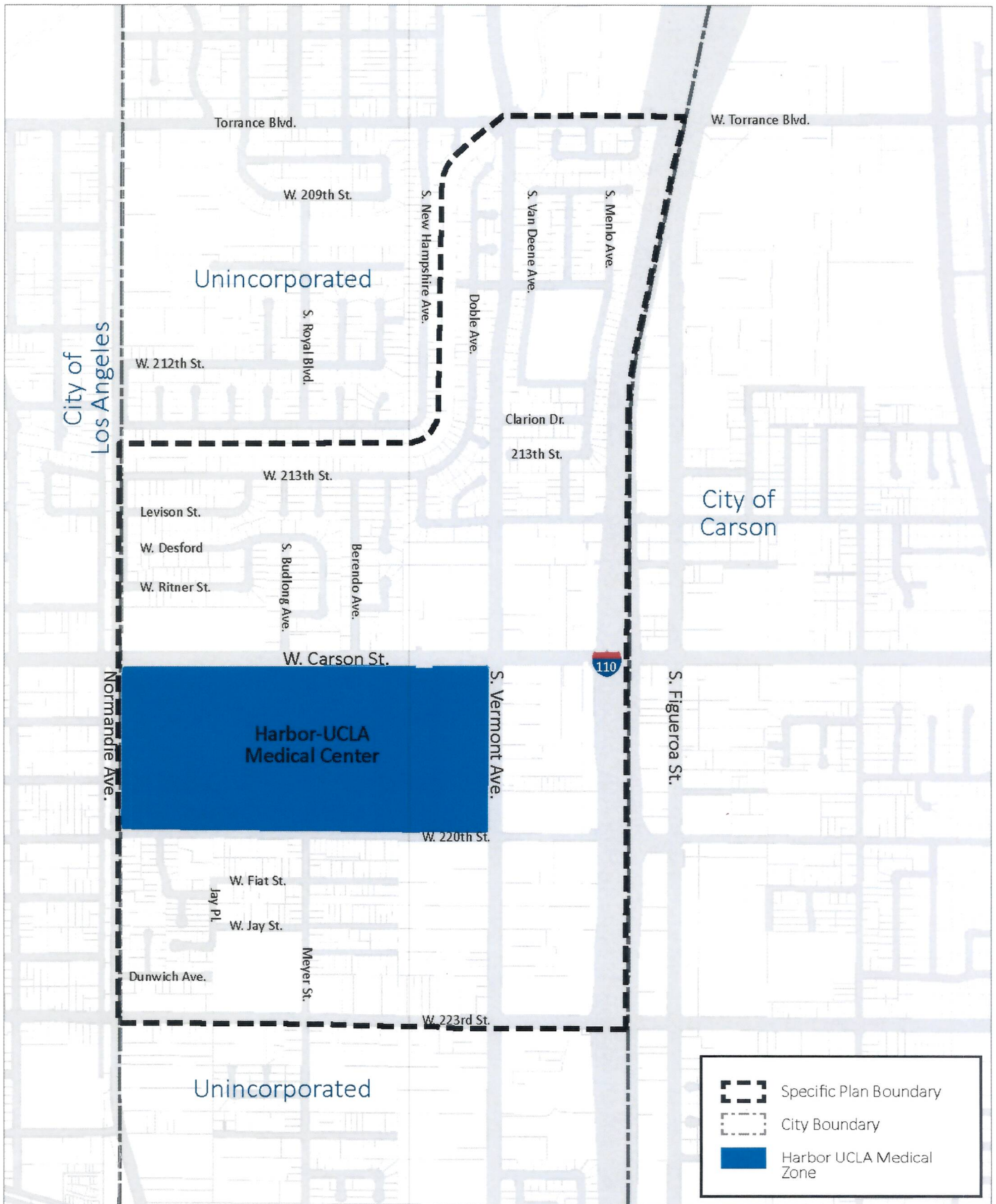
»» Landscaping along Carson Street shall be permeable and open to the street to allow visibility; encourage access and connectivity to/from the walking path along Carson Street and hospital campus; and create an attractive, inviting pedestrian experience.

Table 3.17 Site Configuration Regulations (Harbor-UCLA Medical)

TYPE	MINIMUM	MAXIMUM
DENSITY		
Residential	None	30 du/ac
FLOOR AREA RATIO (FAR)¹		
Non-residential	None	.78
BUILDING SETBACK		
South Vermont Avenue	5 ft.	None
Carson Street	5 ft.	None
Local Road	15 ft.	None
BUILDING HEIGHT		
Floors		
Building Height	None	None ¹

¹ Building height will be consistent with the Harbor-UCLA Medical Center Campus Master Plan.

Figure 3.1 | Proposed Zoning: Harbor- UCLA Medical Zone Areas



3.4.9 Mixed Use 1 (MU1) Zone

3.4.9.1 Purpose and Intent

The Mixed Use 1 Zone (MU1 Zone) is located primarily along Carson Street and is intended to promote development of a mix of commercial, office, and residential, with an emphasis on neighborhood and medical campus serving retail, restaurant and services uses. Uses can either be developed in a stand-alone or vertical mixed-use configuration. The MU1 Zone provides for a range of small to large retail or mixed use centers, multi-family residential uses up to 30 dwelling units per acre, and private/public open space components, with strong bicycle and pedestrian connections to the Carson Street Station, medical campus to the south, and broader West Carson community.

a. *Development Potential Summary*

- »» Total Developable Area: 20 acres
- »» Non-Residential: 483,460 sq. ft.
- »» Residential: Max. 143 units

3.4.9.2 Use Regulations: MU1 Zone

The land use regulations define permitted, conditionally permitted, and prohibited uses in the Mixed Use 1 Zone, as shown in Table 3.18, Use Regulations: MU1 Zone.

Accessory uses and structures are permitted when customarily associated with, and subordinate to, a permitted use on the same site, and would include:

- »» Administrative office
- »» Assembly/multi-purpose building
- »» Caretaker's quarter
- »» Enclosed, screened, and outdoor storage
- »» Maintenance/storage facility and structure
- »» Patio cover/trellis
- »» Swimming pool, spa, and jacuzzi
- »» Tennis court, basketball courts, and other multi-purpose courts, recreation and community buildings

Figure 3.12 Proposed Zoning: Mixed Use 1 Zone Areas

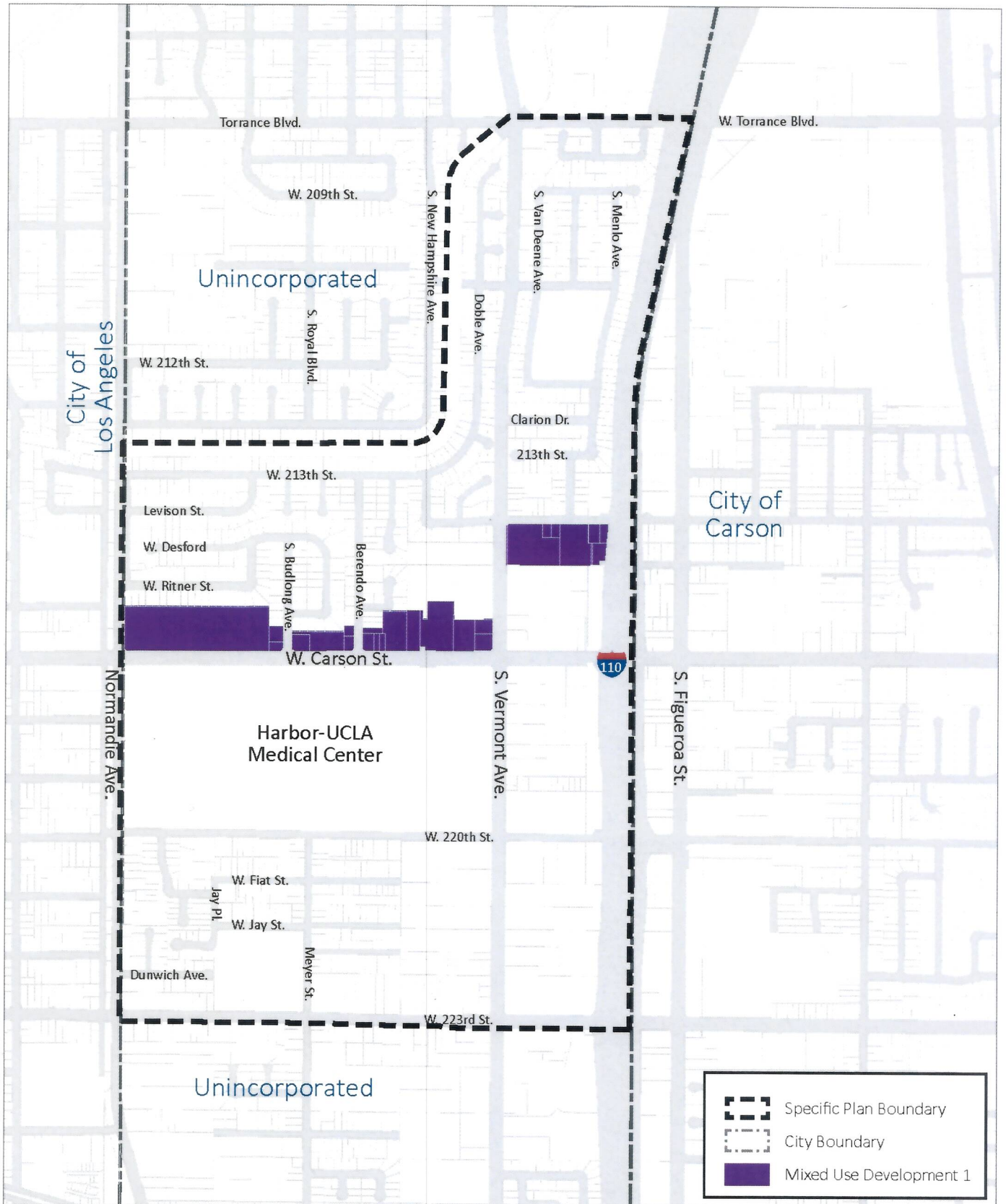


Table 3.18 Use Regulations: MU1 Zone

USE	
RESIDENTIAL	
Mixed use developments (commercial residential)	Permitted
Multi-family dwelling units (including apartment houses)	Permitted
PUBLIC/INSTITUTIONAL	
Art and cultural facility	Permitted
Churches, temples, and other places of worship	Permitted
Fire station	Conditional
Parks, playgrounds with appurtenant facilities customarily found in conjunction therewith	Permitted
School	Conditional
SERVICE/RETAIL COMMERCIAL	
Acute care/rehabilitation uses	Conditional
Automobile service station	Prohibited
Automobile related uses including repair, battery, painting/auto body, muffler, service, washing, sales, or rental.	Prohibited
Amusement rides and devices	Prohibited
Alternative Financial Service	Prohibited
Bakery, coffee house/café, delicatessen/cafeteria	Permitted
Bank and financial institution	Permitted
childcare Center	Conditional
Commercial recreational facility	Permitted
Entertainment uses, including restaurants, non- adult-only theaters, and other similar venues	Permitted
Grocery/market	Permitted
Health club/gymnasium	Permitted
Hotel	Permitted
Movie theater	Permitted
On-site alcoholic beverage sales establishment	Conditional

Table 3.18 Use Regulations: MU1 Zone

USE	
Drive-through establishments and drive-through lanes	Prohibited
Pawn Shops	Prohibited
Theater, including live performance	Permitted
Smoking-oriented including tobacco, pipe, and vape shops	Prohibited
OFFICE	
General office	Permitted
Medical office	Permitted
Professional office	Permitted

3.4.9.2 Development Standards

The following development standards regulate new site and building development by establishing standards for intensity, building height, open space, and other elements. They have been developed to accommodate the mix of commercial, office, and residential uses anticipated. The standards are intended to encourage the development of medium density housing as well as daily retail and service commercial uses to serve the needs of the West Carson community. Development standards include street-based frontage standards regulating building frontages along existing and new streets to form adequate and pedestrian-friendly building faces. The following development standards apply in conjunction with Section 3.6, Urban Design Standards, below.

Table 3.19 Site Configuration Regulations: MU1 Zone

TYPE	MINIMUM	MAXIMUM
DENSITY		
Residential	18 du/ac	30 du/ac
FLOOR AREA RATIO (FAR)		
Non-residential	0.5	1.0
BUILDING SETBACK		
South Vermont Ave	5 ft.	15 ft.
Carson Street	5 ft.	15 ft.
Local Road	15 ft.	None
Interior Yard (Side or Rear)	0 ft.	None
Interior Yard Adjacent to Residential (Side or Rear)	15 ft.	None
BUILDING HEIGHT		
Floors		3 stories
Building Height	None	40 ft ¹

1. Building height shall exclude chimneys, rooftop antennas, and rooftop recreation spaces.

a. *Frontages*

All new development adjacent to a street with frontage requirements, as shown in Figure 3.5, Streets with Frontage Requirements, shall have a primary building façade and entry from the identified street and shall adhere to the following building frontage requirements. These frontages dictate the relationship between the street (back of right-of-way) and the façade of the ground-floor of the building (see Section 3.6.2, Building Design, for building frontage design standards and guidelines).

- »» Building orientation shall be determined by the location of the primary entrance, which shall indicate the front of the building.
- »» All building sides abutting major public rights-of-way shall require architectural treatment.

- »» Pedestrian access to public right-of-way is required either through common corridors or courtyards from buildings adjacent to the road.
- »» Open fencing is allowed, but a solid wall greater than three-feet high shall not be allowed.

Table 3.20 Frontage Categories: MU1 Zone

TYPE	CARSON STREET	VERMONT AVENUE
Shopfront	Permitted	Permitted
Forecourt	Permitted	Permitted
Gallery	Permitted	Permitted
Arcade	Permitted	Permitted
Terrace	Permitted	Permitted
Stoop	Not permitted	Permitted

See Section 3.6.2, *Building Design*, for building frontage design standards and guidelines.

b. Built Form & Landscaping

- When sharing a property line with an existing single-family zone, the following requirements shall apply:
 - »» Windows, balconies, or similar openings shall be oriented to minimize any direct line-of-sight into adjacent units or onto private patios or backyards adjoining the property line.
 - »» Required interior yards (side or rear) shall feature a landscaped buffer designed and continuously maintained to maximize the privacy of adjoining single-family homes. At the time of planting, plant materials shall be of a size that maturity and maximum privacy will be achieved within five years.
 - »» The third floor shall be stepped back by a minimum of 10-feet.
 - »» New development fronting Vermont Avenue or 214th Street shall not exceed 40-feet and three stories in height. In addition, new development, including setbacks and stepbacks, will be a maximum of 50-feet in depth from the property line along the street.

- Any development that includes dwelling units shall provide adequate private or common laundry facilities reserved for the exclusive use of the residents of the development.
- Any development that includes dwelling units shall be set back a minimum of 100-feet from Interstate 110.
- An increase in FAR is allowed to encourage lot consolidation in the MU1 Zone as follows:
 - »» 0.5 acre to 1.0 acre = 0.10 increase in FAR
 - »» 1.1 acre to 2 acres = 0.25 increase in FAR, and five-feet increase in height
 - »» 2.1 acre or more = 0.50 increase in FAR, and 10-foot increase in height
- Facades
 - »» Street wall facades shall be architecturally enhanced. This may be accomplished through the use of arcades, colonnades, recessed entrances, window details, bays, and variation in building materials, color, and other details. The maximum total blank wall area (without windows or entrances) shall not exceed 30 percent of the first-story wall for non-residential and 50 percent for residential.
 - »» The façade detailing of mixed-use buildings shall visually differentiate ground-floor uses from upper-story uses. The base shall visibly anchor the building to the ground with a treatment of higher quality materials.
 - »» The façade detailing of commercial or retail entries shall be differentiated from residential entries.
- Design Features
 - »» Projecting features, such as balconies, porches, bays, and dormer windows, are encouraged to create distinction between units and to provide "eyes" on the street.
 - »» Private balconies shall be screened by translucent materials that shield visibility of personal items from public view.
 - »» All primary ground-floor common entries or individual dwelling unit or commercial entries fronting on streets shall be or oriented to the street, not to the interior or to a parking lot.
 - »» Buildings having 100-feet or more of street frontage shall be designed to provide roofs of varying heights.

- »» All glass in windows or entrances on the first two stories shall be either clear or lightly tinted to maximize pedestrian visibility of building interiors from the sidewalk area. Mirrored, highly reflective, or densely tinted glass shall be prohibited for use in windows and entrances.
- »» Operable windows that front Interstate 110 should be avoided.
- Utility and Mechanical Equipment
 - »» All utility and mechanical equipment shall be placed in locations that are not exposed to view from the street or shall be screened from view. Satellite dishes shall be located away from public view.
 - »» Screening elements shall be an integral part of the building's design.
- Required Residential Open Space
 - »» 100 square feet per dwelling unit for common recreational-leisure areas, private areas, or a combination of both.
 - »» Minimum dimension for private areas is seven-feet.
 - »» Minimum dimension for common areas is 20-feet.
 - »» Side and rear yards may be included in the calculation of open space, but not the required front yard setback area.
 - »» Open space areas shall have no parking, driveway, or right-of-way encroachments.
 - »» All common areas shall be improved as either active or passive facilities, with landscaping or hardscape elements designed to serve the residents of the project. All common areas shall be developed and professionally maintained in accordance with approved landscape and irrigation plans.
 - »» Common recreational space shall be located on the same property as the residential it serves and shall be available exclusively for the use of all residents of the development.
 - »» Where a rooftop is used for common recreational space, the rooftop shall incorporate landscaping, decorative paving and materials, and amenities. Mechanical equipment storage areas shall not be counted toward meeting the requirement.
 - »» Pedestrian walkways within a project shall be a minimum of four-feet in width.

- »» Private useable open space shall be contiguous to the residential units served and screened for privacy.
- »» Courtyard internal to a project, or enclosed on at least three sides, shall have a minimum width of 40-feet.

- Required Non-residential Open Space

- »» 500 square feet of non-residential open space requirement for projects less than two acres.
- »» 2,500 square feet of non-residential open space requirement for projects greater than two acres.
- »» Non-residential open space requirement may be satisfied by outdoor dining areas, plazas, or other useable outdoor use as approved by the Director.
- »» Public plazas, urban pocket parks, outdoor dining, promenades, public art, and other outdoor public amenities shall be designed to activate ground-floor uses, engage residents and visitors.
- »» Open spaces shall be appropriately landscaped and provide adequate shade through the placement of trees or other shade devices, including umbrellas, awnings, trellises, and canopies that are integrated into the building or over the open space.

c. *Special Requirements*

- An application for new construction or expansion of development within the MU1 Zone shall be subject to the requirements included in Section 22.26.020 for review of the project's impacts on infrastructure.

3.4.10 Mixed Use 2 (MU2) Zone

3.4.10.1 Purpose and Intent

The Mixed Use 2 Zone (MU2 Zone) is intended to be developed over time as a transit-supportive environment providing a higher-intensity mix of retail, office, restaurant uses, and residential development in a compact, walkable setting. This designation encourages a range of multiple-family residential housing products in either a stand-alone or mixed-use configuration, up to 70 dwelling units per acre. The development standards and design requirements address vital private/public open space components, bicycle and pedestrian connections to the Carson Street Station and medical campus to the west, and proximity to the Interstate 110 freeway. The MU2 Zone is intended to promote community redevelopment through higher intensity, transit- supporting infill development.

a. *Development Potential Summary*

- »» Total Developable Area: 31 acres
- »» Non-residential: 978,675 sq. ft.
- »» Residential: Max. 1,223 units

3.4.10.2 Use Regulations: MU2 Zone

The land use regulations define permitted uses within the Mixed Use 2 zone, as shown in Table 3.21, Use Regulations: MU2 Zone.

Table 3.21 Use Regulations: MU2 Zone

USE	
RESIDENTIAL	
Mixed use developments (commercial/residential)	Permitted
Multi-family dwelling units (including apartment houses)	Permitted
PUBLIC/INSTITUTIONAL	
Art and cultural facility	Permitted
Churches, temples, and other places of worship	Permitted

Table 3.21 Use Regulations: MU2 Zone

USE	
Fire station	Conditional
Parks, playgrounds with appurtenant facilities customarily found in conjunction therewith	Permitted
School	Conditional
SERVICE/RETAIL COMMERCIAL	
Acute care/rehabilitation uses	Conditional
Alternative Financial Service	Prohibited
Automobile service station	Prohibited
Automobile related uses including repair, battery, painting/auto body, muffler, service, washing, sales, or rental.	Prohibited
Amusement rides and devices	Prohibited
Bakery, coffee house/café, and delicatessen/cafeteria	Permitted
Bank and financial institution	Permitted
Preschool or childcare center	Conditional
Commercial recreational facility	Permitted
Entertainment uses, including restaurants, non-adult-only theaters, and other similar venues	Permitted
Grocery/market	Permitted
Health club/gymnasium	Permitted
Hotel	Permitted
Movie theater	Permitted
Alcoholic beverage sales	Conditional
Drive-through establishments and drive-through lanes	Prohibited
Theater, including live performance	Permitted
Smoking-oriented including tobacco, pipe, and vape shops	Prohibited

Figure 3.13 Proposed Zoning: Mixed Use 2 Zone Areas

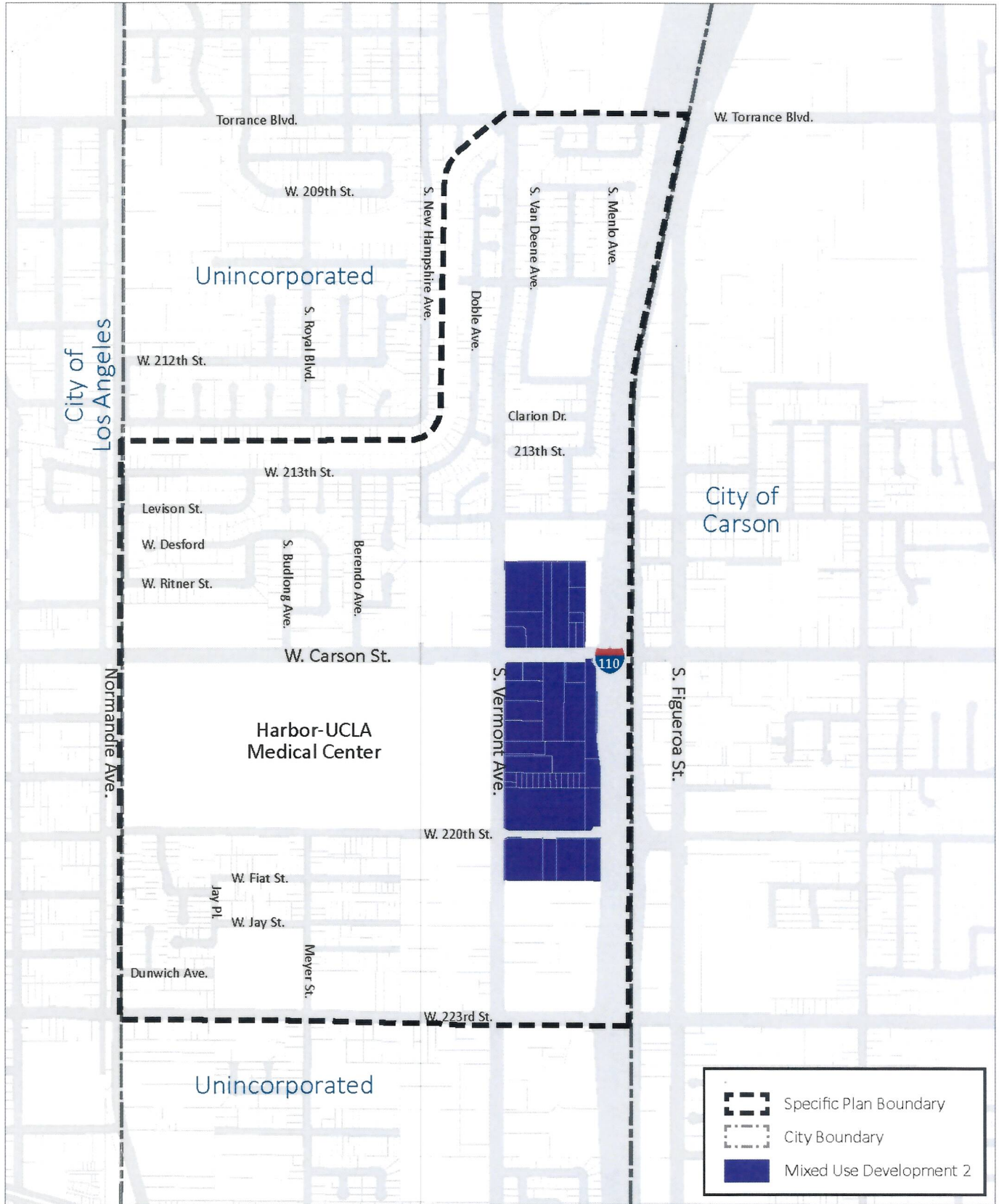


Table 3.21 Use Regulations: MU2 Zone

USE	
General office	Permitted
Medical office	Permitted
Professional office	Permitted

3.4.10.3 Development Standards

The following development standards regulate new site and building development by establishing standards for intensity, building height, open space, and other elements. They have been developed to accommodate the mix of retail, office, restaurant, and residential uses anticipated. The standards are intended to encourage the development of high density housing and retail and service commercial uses to serve the needs of the West Carson community. Development standards include street-based frontage standards regulating buildings frontages along existing and new streets to form adequate and pedestrian-friendly building faces. The following development standards apply in conjunction with Section 3.6, Urban Design Standards, below.

Table 3.22 Site Configuration Regulations: MU2 Zone

TYPE	MINIMUM	MAXIMUM
DENSITY		
Residential	31 du/ac	70 du/ac
FLOOR AREA RATIO (FAR)¹		
Non-residential	None	3.0
BUILDING SETBACK		
West 223rd Street/Meyler Street	10 ft.	25 ft.
Interior Yard Adjacent to Residential (Side or Rear)	15 ft.	None
BUILDING HEIGHT		
Floors	None	5 stories
Building Height		60 ft. ¹

¹ Height excluding signs that are permitted by Chapter 22.114, chimneys, and rooftop antennas.

a. *Frontages*

All new development adjacent to a street with frontage requirements, as shown in Figure 3.5, Streets with Frontage Requirements, shall have a primary building façade and entry from the identified street and shall adhere to the following building frontage requirements. These frontages dictate the relationship between the street (back of right-of-way) and the façade of the ground floor of the building (see Section 3.6.2, Building Design, for building frontage design standards and guidelines).

- »» Building orientation shall be determined by the location of the primary entrance, which shall indicate the front of the building.
- »» All building sides abutting major public rights-of-way shall require architectural treatment.
- »» Pedestrian access to public right-of-way is required either through common corridors or courtyards from buildings adjacent to the road.
- »» Open fencing is allowed, but a solid wall greater than three-feet high shall not be allowed.

Table 3.23 Frontage Categories: MU2 Zone

TYPE	CARSON STREET	VERMONT AVENUE
Shopfront	Permitted	Permitted
Forecourt	Permitted	Permitted
Gallery	Permitted	Permitted
Arcade	Permitted	Permitted
Terrace	Permitted	Permitted
Stoop	Not permitted	Not permitted

See Section 3.6.2, *Building Design*, for building frontage design standards and guidelines.

b. Built Form & Landscaping

- Development at the corner of the intersection of Carson Street and Vermont Avenue shall provide the following:
 - »» A corner entrance or an entrance oriented toward each street.
 - »» Architectural style and detail of buildings that create interest, including display windows, façade materials, colors, art features, rooftop elements, and setbacks.
- Any development that includes dwelling units shall provide adequate private or common laundry facilities reserved for the exclusive use of the residents of the development.
- Façades
 - »» Street wall façades shall be architecturally enhanced. This may be accomplished through the use of arcades, colonnades, recessed entrances, window details, bays, and variation in building materials, color, and other details. The maximum total blank wall area (without windows or entrances) shall not exceed 30 percent of the first story wall for non-residential and 50 percent for residential.
 - »» The façade detailing of mixed-use buildings shall visually differentiate ground-floor uses from upper-story uses. The base shall visibly anchor the building to the ground with a treatment of higher quality materials.

- »» The façade detailing of commercial or retail entries shall be differentiated from residential entries.
- Design Features
 - »» Projecting features, such as balconies, porches, bays, and dormer windows shall be used to create distinction between units and provide "eyes" on the street.
 - »» Private balconies shall be screened by translucent materials that shield visibility of personal items from public view.
 - »» All primary ground-floor common entries or individual dwelling unit or commercial entries fronting on streets shall be oriented to the street, not to the interior or to a parking lot.
 - »» Buildings having 100-feet or more of street frontage shall be designed to provide façade articulation and roofs of varying heights.
 - »» All glass in windows or entrances on the first two stories shall be either clear or lightly tinted to maximize pedestrian visibility of building interiors from the sidewalk area. Mirrored, highly reflective, or densely tinted glass shall be prohibited for use in windows and entrances.
- Utility and Mechanical Equipment
 - »» All utility and mechanical equipment shall be placed in locations that are not exposed to view from the street or shall be screened from view. Satellite dishes shall be located away from public view.
 - »» Screening elements shall be an integral part of the building's design.
- Required Residential Open Space
 - »» 100-square feet per dwelling unit for common recreational-leisure areas, private areas, or a combination of both.
 - »» Minimum dimension for private areas is seven-feet.
 - »» Minimum dimension for common areas is 20-feet.
 - »» Side and rear yards may be included in the calculation of open space, but not the required front-yard setback area.
 - »» Open space areas shall have no parking, driveway, or right-of-way encroachments.
 - »» All common areas shall be improved as either active or passive facilities, with landscaping or hardscape elements designed to serve the residents of the project. All common areas shall be developed and professionally maintained in accordance with approved landscape and irrigation plans.

- »» Common recreational space shall be located on the same property as the residential it serves and shall be available exclusively for the use of all residents of the development.
- »» Where a rooftop is used for common recreational space, the rooftop shall incorporate landscaping, decorative paving and materials, and amenities. Mechanical equipment storage areas shall not be counted toward meeting the requirement.
- »» Pedestrian walkways within a project shall be a minimum of four-feet in width.
- »» Private useable open space shall be contiguous to the residential units served and screened for privacy.
- »» Courtyard internal to a project, or enclosed on at least three sides, shall have a minimum width of 40-feet.
- Required Non-residential Open Space
 - »» 500 square feet non-residential open space requirement for projects less than two acres.
 - »» 2,500 square feet non-residential open space requirement for projects greater than two acres.
 - »» Non-residential open space requirement may be satisfied by outdoor dining areas, plazas, or other useable outdoor use, as approved by the Director.
 - »» Public plazas, urban pocket parks, outdoor dining, promenades, public art, and other outdoor public amenities shall be designed to activate ground-floor uses, engage residents and visitors.
 - »» Open spaces shall be appropriately landscaped and provide adequate shade through the placement of trees or other shade devices, including umbrellas, awnings, trellises, and canopies that are integrated into the building or over the open space.

c. *Special Requirements*

- An application for new construction or expansion of development within the MU2 Zone shall be subject to the requirements included in Section 22.26.020 for review of the project's impacts on infrastructure.

3.4.11 Public Zone (P Zone)

3.4.11.1 Purpose and Intent

The West Carson Public Zone provides for established public uses including schools, parks, 208th Street Drain channel, the Carson Street Transit Station park-and-ride, and other public uses. This designation is intended to promote the use of publicly owned land for the purposes of community open space, connection, and activity.

a. Development Potential Summary

- »» Total Developable Area: 27 acres
- »» Non-residential: 0 sq. ft.

3.4.11.2 Use Regulations: P Zone

The land use regulations define permitted uses within the Public zone, as shown in Table 3.24, Use Regulations: P Zone.

Table 3.24 Use Regulations: P Zone

USE	
PUBLIC/INSTITUTIONAL	
Parks, playgrounds with appurtenant facilities customarily found in conjunction therewith	Permitted
School	Permitted

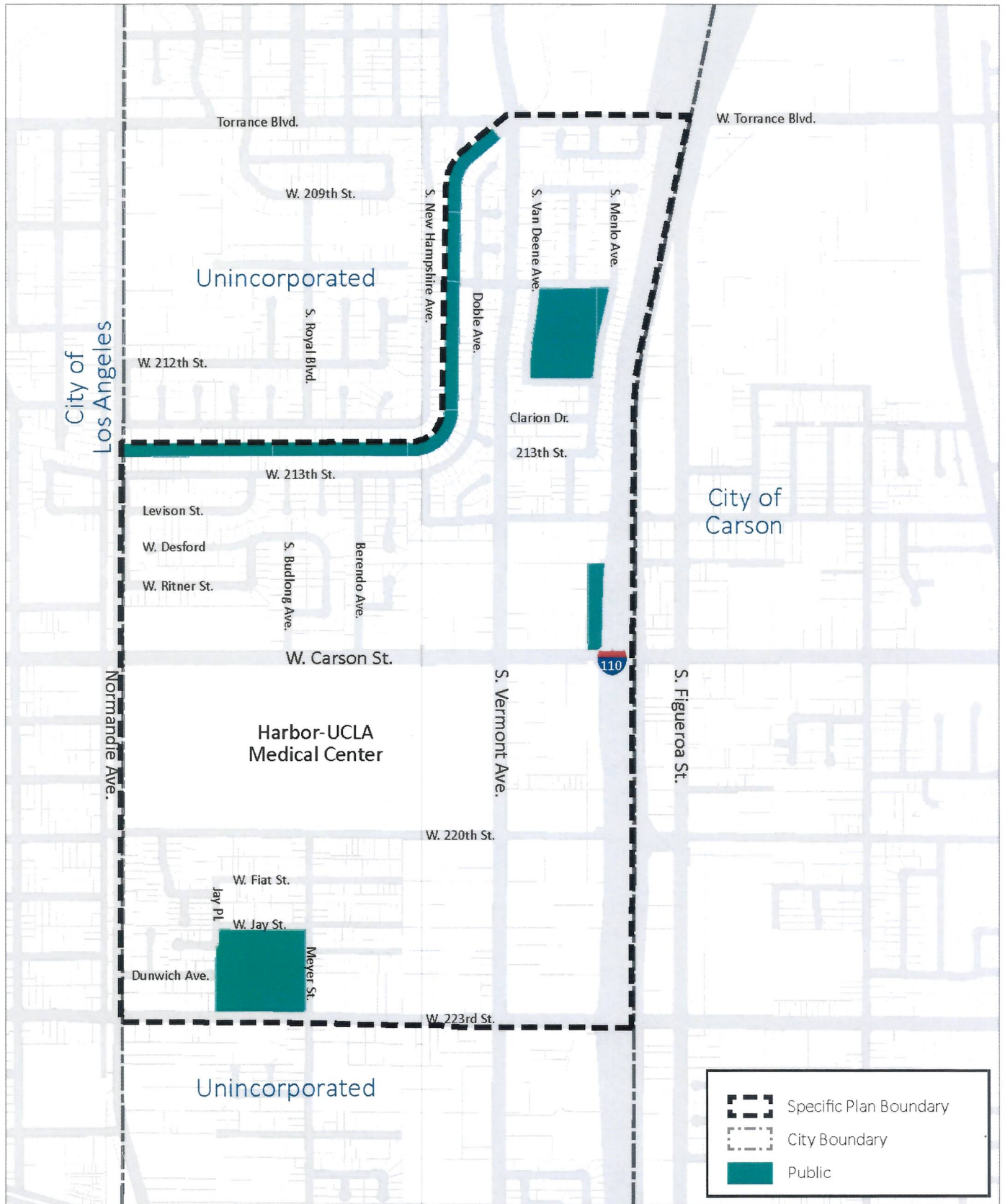
3.4.11.3 Development Standards

The following development standards regulate new site development by establishing standards for intensity, open space, and other elements. The development standards for Public Zone have been tailored to its future use as additional recreational opportunities in the community occur and should be used in conjunction with Section 3.6, Urban Design Standards, below.

Table 3.25 Site Configuration Regulations: P Zone

TYPE	MINIMUM	MAXIMUM
FLOOR AREA RATIO (FAR)¹		
All areas	<i>None</i>	.10
BUILDING SETBACK		
Front	<i>None</i>	15 ft.
Rear	10 ft.	<i>None</i>
Side	10 ft.	<i>None</i>
BUILDING HEIGHT		
Floors		2 stories
Building Height	<i>None</i>	30 ft.

Figure 3.14 Proposed Zoning: Public Zone Areas



3.5 GENERAL DEVELOPMENT STANDARDS

The following general regulations shall apply to new development and the reuse of existing structures and facilities, unless specific exceptions are described elsewhere in the Specific Plan.

Use Regulations

Alcohol Beverage Sales

The County established standards for all on- and off-site alcoholic beverage sales establishments to promote and protect the public health, safety, and general welfare and preserve and enhance the quality of the community. Applicants shall refer to Chapter 22.158 for standards and guidelines relating to establishments that include the sales of alcoholic beverages for on- and off-site consumption. Alcohol beverage sales require a conditional use permit, as identified in the use regulations of the zone in which the establishment is located.

Outside Storage

All uses shall be conducted within a completely enclosed building, except for off-street parking, loading, approved horticultural nursery accessory uses, approved temporary uses, and any outdoor dining specifically permitted in conjunction with eating establishments. Minor ancillary outdoor storage (service vehicle parking, materials storage, or limited equipment assembly associated with a permitted use) may be located outside a building in certain planning areas as an accessory use, provided that there is solid screening and no negative noise or aesthetic impacts on adjacent properties.

Interim and Temporary Uses

Interim uses on County-owned properties shall require approval of a special event permit pursuant to Title 22 of the County Code, as applicable. Temporary uses shall be regulated pursuant to Title 22 of the County Code, as applicable.

Utilities

All utility lines serving a new development, with the exception of interim uses, shall be placed underground by the developer in accordance with the County's policies for locating utilities underground.

- »» Existing utility lines shall also be placed underground with development, where feasible and as required by the County.

- »» No structures shall be permitted to be developed over active pipelines, abandoned lines, or utility easements, except where approved by the Director of Public Works.
- »» All utility line and connection costs to the backbone infrastructure/utility system (water supply, sanitary sewer facilities, reclaimed water supply, storm drainage, and other utilities) needed to serve individual projects shall be the responsibility of the new user/owner, or as otherwise determined as a result of a negotiated development agreement or lease.
- »» All water supply, sanitary sewer facilities, reclaimed water supply, storm drainage, and other facilities shall be provided in accordance with adopted master plans of the respective service providers. Costs shall be assigned to each development lot or parcel in accordance with a cost-benefit formula established by the County, based on estimated engineering construction costs, or as otherwise determined as a result of a negotiated development agreement.

Antennae and Satellite Dish Standards

All provisions of the County Code, as applicable, shall apply.

Mechanical Equipment

Compressors, air conditioning units, vents, exhausts, or similar mechanical equipment located outside a building shall comply with the following:

- »» All such equipment shall be screened from view from any abutting street or adjacent use. Screening shall be an integral part of the overall architectural design of the project. The top of any screening shall be a minimum of six inches above the top of any mechanical equipment.
- »» All mechanical equipment shall be maintained in a clean and proper condition to prevent breakdown.

Roof-Mounted Solar Collector Panels

- »» Roof-mounted solar collector panels shall be mounted flush with the surface where possible. Where panels cannot effectively perform if flush mounted, justification in the form of efficiency calculations may be submitted to Regional Planning for consideration of alternative mounting configurations.

Refuse Collection Facilities

- »» All outdoor refuse collection facilities shall be screened visually from streets and highways. Collection areas shall be shielded from view in all directions, either within a building or within a solid masonry wall of sufficient height to conceal materials temporarily accumulated for collection. The enclosure shall be designed to complement the main building materials.

Encroachments and Site Development

Encroachments

The following encroachments into setback areas are allowed, subject to Title 26 (Building Code) of the County Code:

- »» Architectural features
- »» Eaves
- »» Fireplaces
- »» Mechanical equipment
- »» Steps and staircases (open)
- »» Covered patio trellis or canopies, unenclosed on at least two sides, may encroach into the required side- or rear-yard setback provided they do not cover more than 50 percent of the private yard or open space area on a residential property and comply with the California Building Code related to distance requirements between adjacent structures.

Park Provisions

- »» Requirements and standards in the Title 21 (Subdivisions) of the County Code will be utilized in reviewing public or private park proposals per parkland dedication requirement.

Environmental

Grading

- »» All earthwork shall be conducted in accordance with the grading regulations found in Title 26 of the County Code and manual and grading requirements in Los Angeles County. Grading permits shall consider consistency with the urban design concept. Compliance with National Pollutant Discharge Elimination System (NPDES) regulations related to storm drain runoff from construction sites, as implemented in the County, shall be required.

Vibration

No vibration associated with any use shall be permitted that is discernible beyond the boundary line of the property, unless the vibration does not negatively impact an adjacent property.

Fumes, Odors, and Other Forms of Air Pollution

If any use produces odors, toxic gases, or noxious matter in such quantities as may be readily detectable at any point outside the property lines of the premises, and/or may become a public nuisance or hazard, the use shall be modified to prevent such emissions. Emissions shall be in compliance with the South Coast Air Quality Management District standards.

Hazardous Materials

Ensure that the use, handling, storage, and transportation of hazardous materials comply with the California Government codes and Health and Safety Codes, and all provisions of Titles 11 (Health and Safety), 12 (Environmental Protection), and 32 (Fire Code) of the County Code.

Noise Control

Plans for noise attenuation of residential units near arterial highways and the freeway, which ensure that interior and exterior noise levels do not exceed state requirements and the Noise Control Ordinance in Title 12 (Environmental Protection) of the County Code, shall be submitted for review and approval prior to building permits being issued to accommodate reuse and/or subdivision approval for residential development, whichever occurs first.

All uses shall be subject to provisions of the Noise Control Ordinance in Title 12 (Environmental Protection) of the County Code.

Enforcement of EIS/EIR Mitigation Measures

Amendments to section 21081.6 of the California Public Resources Code, as applicable, shall apply. The amendments provide that measures to mitigate or avoid significant effects on the environment shall be fully enforceable through permit conditions, agreements, or other measures.

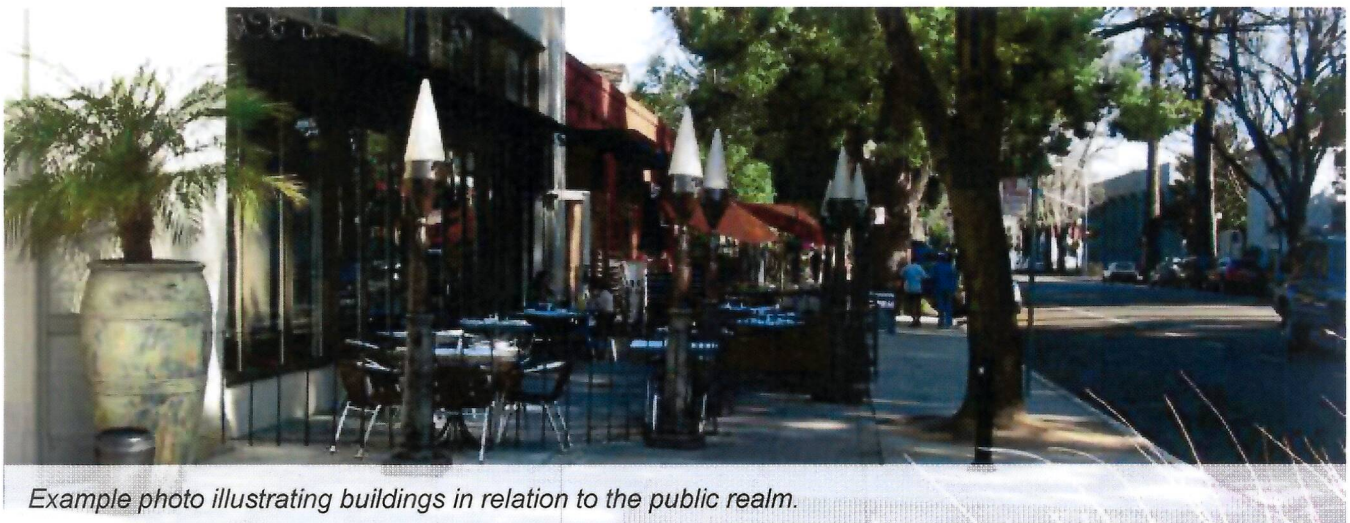
3.6 URBAN DESIGN STANDARDS

Design will play a crucial role in implementing the vision for West Carson. The layout of the blocks, site design, architectural character, outdoor spaces, landscaping, and views will contribute to a "sense of place" that is unique to West Carson.

General development design standards provide a regulatory framework for the physical design of the Specific Plan area to ensure the creation of a livable, accessible, and sustainable community. It is important to establish a strong set of design criteria that will create a strong identity for the project area while allowing for flexibility to meet market demands.

The purpose of these guidelines and standards is to provide direction that will create a comprehensive approach to high quality design as it relates to West Carson. These are provided in addition to the development standards under Section 3.4, Regulating Code, above. These standards and guidelines are not meant to dictate a particular architectural style in the area, but to foster innovative design features and site-appropriate architecture that is constructed with quality materials and complemented by landscape, open spaces, and connectivity between uses.

The terms "should" and "shall" are used throughout the Urban Design Standards and are used purposefully to indicate required and suggested standards. The use of shall indicates that a requirement is mandatory and the standards and specifications are applicable without deviation. The use of should indicates that a requirement is a recommendation and the standards and specifications are advisory.



Example photo illustrating buildings in relation to the public realm.

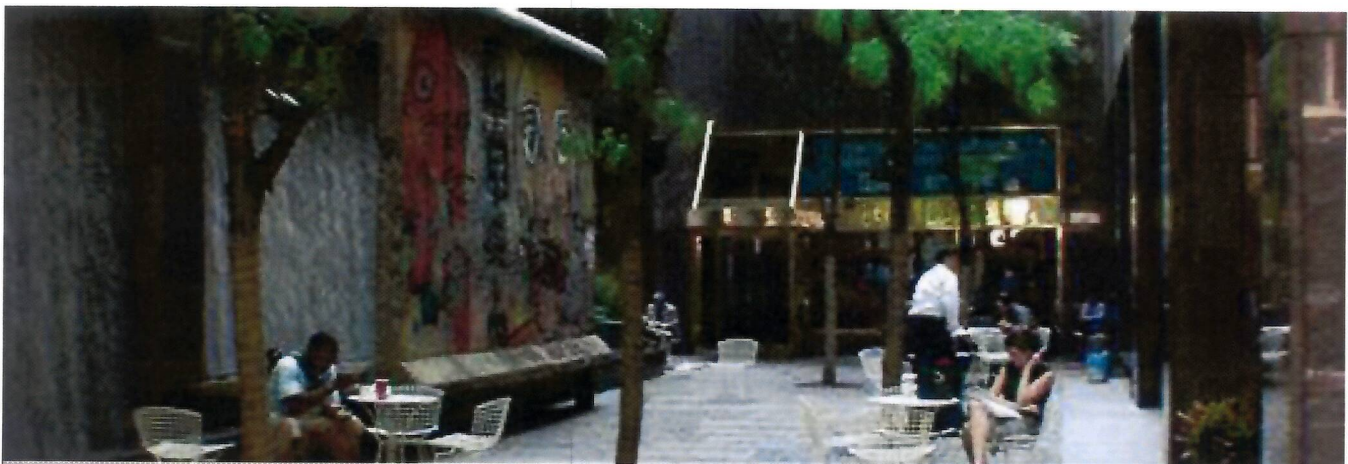
3.6.1 SITE DESIGN

Site design is an important process critical to any development that may occur in the Specific Plan area. The resulting outcome, conveyed in a project's site plan, will determine how buildings are placed on a site, where access will occur, and how structures and spaces are located in relation to each other and to adjacent off-site uses. The following standards and guidelines shall be integrated in the site design of all new projects. Alternatives will be permitted only if the intent of the design standard is met.

3.6.1.1 Building Placement and Orientation

Building placement and orientation have a large impact on the way people experience a developed site and on the building's relationship with local conditions such as views, wind, and sun movements. Visually interesting buildings that are oriented to the street shape the area's character and the visitor's experience. Locating parking behind buildings, placing buildings closer to the public street edge, and placing a majority of active ground-floor uses on the same frontage all contribute to making the public street frontage more inviting to pedestrians.

- »» Buildings shall be oriented toward public street, pedestrian pathways, or public open spaces to create a strong presence and encourage activity along the street frontage.
- »» Certain zones have street frontage requirements (see Figure 3.5, Streets with Frontage Requirements), and buildings shall comply with the frontage type requirement specified for the applicable planning area in Section 3.4. Furthermore, the design of the frontage shall comply with the frontage type standards provided in Section 3.6.2, Building Design.



Example photo illustrating a public space created as part of adjacent development.

- »» Buildings should be oriented for energy efficiency (e.g., to capture day lighting, minimize heat gain, take advantage of prevailing breezes, and for natural ventilation).
- »» There shall be an accessible, well-marked, and well-lit travel path of a minimum of four-feet in width provided between parking, buildings, and sidewalks.

3.6.1.2 Site Access

- »» Vehicular access points shall be designed to minimize conflicts with pedestrians through the consideration of curb-cut locations and widths, sight lines, and lighting. Entrance and exit points shall be well marked and lit.
- »» To slow traffic and enhance the overall site design, site entry and edge design features shall be incorporated, such as colored or textured paving treatments, landscaping, signage, and monuments.
- »» Areas between buildings and open spaces shall be linked to and connected by safe, convenient, and accessible pedestrian and bicycle facilities.
- »» Dead-end driving aisles should be avoided.
- »» Non-residential uses—especially multi-building development projects—shall use shared driveways to reduce conflicts with pedestrians.

3.6.1.3 Parking Structures

- »» Above-ground parking structures shall be internalized, screened, or wrapped with other active ground-floor uses (e.g., retail, office, or residential) along public streets so they are only visible at access points for vehicles and less visible from major streets.
- »» The façades of parking structures that are not lined with active uses shall be screened using architectural solutions and/or landscaping that is integrated into the structure's design (e.g., perforated panels, landscape/vine screens, columnar trees, or public art elements).
- »» Parking structures shall be designed with materials, color, and detail compatible with the principle building.
- »» Parking structures should have shaded structures/photovoltaic arrays on the top deck to reduce heat island effects.



Example photo illustrating a parking structure with architectural and screening features.

- »» Parking structures shall screen night lighting to avoid spillover and glare on nearby properties.
- »» Parking structures should incorporate usage technology to assist visitors and minimize the time spent searching for a space.

3.6.1.4 Parking (Access)

- »» Parking shall be located behind, at the side, or at the rear of buildings (away from the street) and can be provided in underground garages, above-ground garages, or interior parking courts.
- »» The perimeter of parking areas and driveways adjacent to streets and pedestrian pathways shall be screened from street views with a low street wall, berms, fences, or landscaping. Screening shall be low enough to ensure that parking remains visible to passerby views.
- »» New developments shall provide accessible, well-lit, and secure bicycle parking visible from buildings, right-of-way, or public open spaces.
- »» Surface parking lots shall take advantage of adjacent building shade where feasible, or provide sufficient tree coverage to reduce the urban heat island effect and provide shade for vehicles and pedestrians.

3.6.1.5 Service & Loading Areas

- »» Where feasible, access to service and loading areas should be provided from a secondary or service road.
- »» Service and loading areas shall be located behind primary structures or properly shielded through fences, gates, landscaping, berms, etc.
- »» Access to service and loading areas shall be clearly marked and not block adjacent vehicular or pedestrian circulation.
- »» To consider noise impacts on adjacent properties, service and loading areas should be located away from residential properties or have restricted hours of use.

3.6.1.6 Utility, Storage, Trash, and Recycling Facilities

- »» Utilities shall be located away from the public right-of-way either within a building recess or landscaped or gated area. Where feasible, utility lines should be undergrounded.

3.6.2 BUILDING DESIGN

This section addresses the design elements of a building that help create an interesting public realm, including building frontage treatment, façade design and composition, colors and materials, windows and doors, and roofs. New buildings shall contribute to defining the character of the street and shall represent a single architectural style that all materials and details are true to. Architects are encouraged to innovate, but with full awareness of and respect for appropriate height, massing, variety, and quality of materials that result in a building with architectural integrity.

3.6.2.1 Frontages

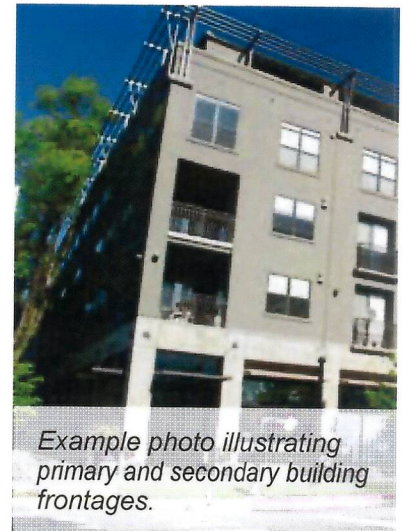
This Specific Plan identifies permitted ground-floor frontage types per applicable street type along Carson Street, Vermont Avenue, and 223rd Street. This section provides design standards for each frontage type to ensure that proposed development relates to the street and meets community design objectives. These frontages dictate the relationship between the street (back of right-of-way) and the façade of the ground floor of the building. Along each applicable roadway, buildings shall be designed with at least one of the permitted frontage types based on the street it fronts, per Table 3.26 Frontage Categories, Figure 3.5, Streets with Frontage Requirements.

All new development adjacent to a street with frontage requirements shall have a primary building façade and entry from the identified street and shall adhere to the following building frontage requirements.

These frontage standards shall be used along with other development and design standards herein. While this Specific Plan provides for a variety of frontage types, the actual choice, design, and architectural style are the decision of the property owner based on the proposed uses, site plan, and building design.

Guidelines for all frontage types are provided below. Tables 3.27 to on the following pages describe the intent of each frontage type and provide guidelines for application to the building façade and street front.

- »» Primary building façades shall align with the right-of-way, property lines, or easement line unless setbacks are allowed.
- »» Non-primary building walls shall be consistent in design with the primary building front to the extent possible. Non-primary building walls are not required to use frontage types provided in this Specific Plan.



- »» The term "clear" means that the identified area is free of encroachments.
- »» Canopies, awnings, signs, balconies, and other architectural projections shall clear eight-feet above the adjacent sidewalk and may encroach into the pedestrian zone up to a maximum of four-feet provided a minimum six-foot-wide clear and unobstructed path is provided, unless approved by the Director or the Director of Public Works (see also Section 3.19.3).
- »» Building orientation shall be determined by the location of the primary entrance, which shall indicate the front of the building.
- »» Modification of design standards due to site specific, utility conflicts, or other unforeseen factors shall require approval from the Director.

Table 3.26 Frontage Categories

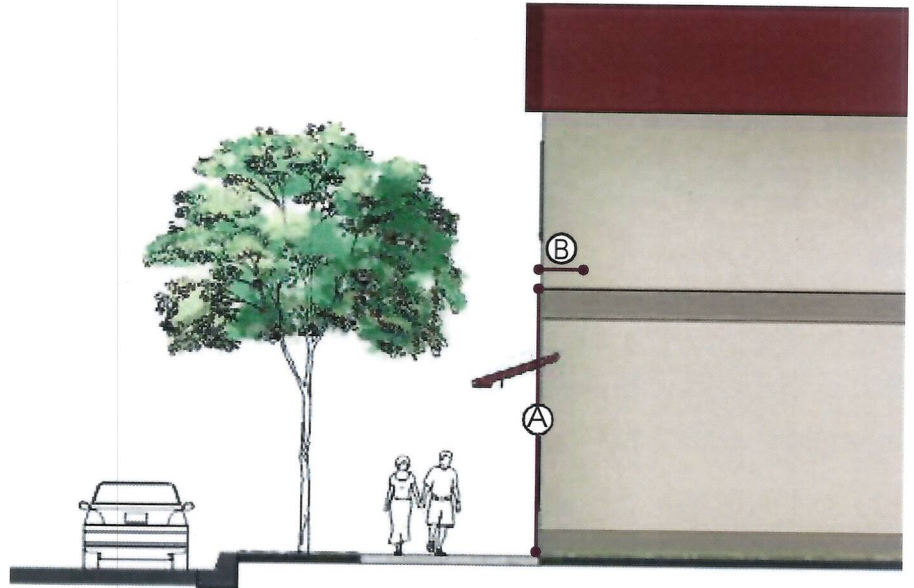
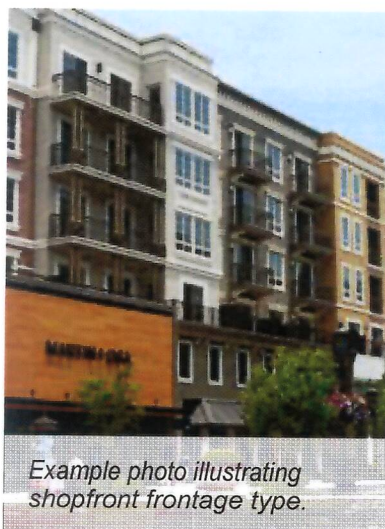
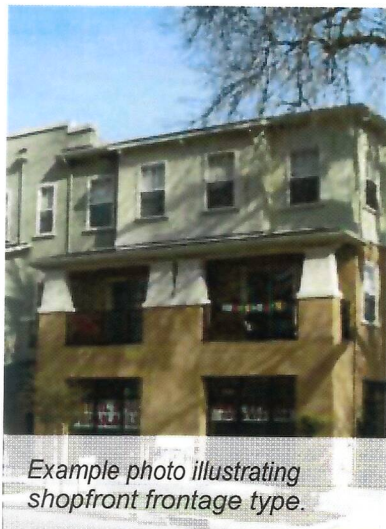
TYPE	CARSON STREET	VERMONT AVENUE	223RD STREET
Shopfront	Permitted	Permitted	Permitted
Forecourt	Permitted	Permitted	Permitted
Gallery	Permitted	Permitted	Permitted
Arcade	Permitted	Permitted	Not permitted
Terrace	Permitted	Permitted	Permitted
Stoop	Not permitted	Permitted	Permitted

See Section 3.6.2, Building Design, for building frontage design standards and guidelines.

Table 3.27 Shopfront Frontage Type

SHOPFRONT FRONTAGE	
DESCRIPTION	
<p>A shopfront is a frontage wherein the building façade and entrance are at sidewalk grade and close to the pedestrian zone. Shopfronts include large areas of transparent openings and doors and are commonly equipped with cantilevered roof(s) or awning(s). Shopfronts typically provide access directly from sidewalks and are oriented to display ground-level commercial uses.</p> <p>This frontage type is conventional for commercial use. This frontage type can be used in conjunction with terrace and/or forecourt to create a more engaging street.</p>	
GUIDELINES	
<p>A great variety of shopfront designs are possible, but the following should apply:</p> <ol style="list-style-type: none"> a. Desirable shopfront façade area is at least 15-feet tall, as measured from the adjacent walk, and minimum 10-feet wide. b. Shopfronts may be recessed from the primary building façade by up to five-feet. c. The shopfront should provide clear views of merchandise displays. d. A base of similar or visually "heavier" materials than the walls is recommended below display windows. e. Doors should be substantial, well detailed, and match the materials, design, and character of the display windows. f. Canopies and awnings should be integrated to shopfront openings. g. Remaining open areas within the frontage zone shall be landscaped per Section 3.6, Public Realm Design. 	

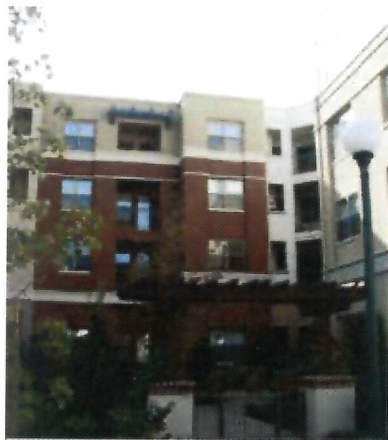
Figure 3.15 Shopfront Frontage Type



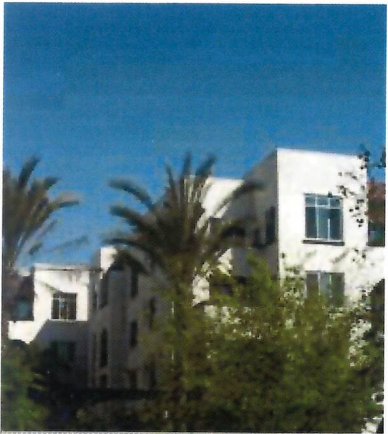
Images for illustrative purposes only.

Table 3.28 Forecourt Frontage Type

FORECOURT FRONTAGE
DESCRIPTION
<p>A forecourt is a frontage wherein a portion of the building façade is recessed from the primary building façade. The forecourt may be used as an entry court and open space for residential uses, or as additional shopping or seating areas for commercial uses. Forecourts with large trees and lush landscaping offer visual and environmental variety to the urban streetscape.</p> <p>This frontage type is appropriate for either residential and/or commercial uses. A combination of both uses can be achieved by using the forecourt as a residential entrance while commercial uses occupy street adjacent building space. This type can be used in conjunction with shopfronts and stoops as a transition into residential frontage.</p>
GUIDELINES
<p>A great variety of forecourt designs are possible, but the following should apply:</p> <ol style="list-style-type: none"> a. A minimum of 10 feet and maximum of 40-feet deep. b. A minimum of 20-feet and maximum of 50-feet wide or 50 percent of the lot width, whichever is less. c. One building entry shall front onto the forecourt. d. The forecourt may also be raised from the sidewalk, creating a small retaining wall at the property line with entry steps to the forecourt, but shall not exceed three-feet in height from the adjacent sidewalk grade, so long as ADA compliant. e. The proportions and solar orientation of the forecourt should be carefully considered for user comfort. Canopies of large trees placed within the forecourt may overhang into the pedestrian zone. f. A fence or wall at the property line may be used to define the private space of the court and shall comply with Section 3.6 Public Realm Design. g. Entrances and pedestrian "gateways" may be announced by posts or pilasters, and may be combined with trellises, special landscaping, decorative lighting, public art, or other special features. h. Remaining open areas within the frontage zone shall be landscaped per Section 3.6.3, Public Realm Design.



Example photo illustrating forecourt frontage type.



Example photo illustrating forecourt frontage type.

Figure 3.16 Forecourt Frontage Type

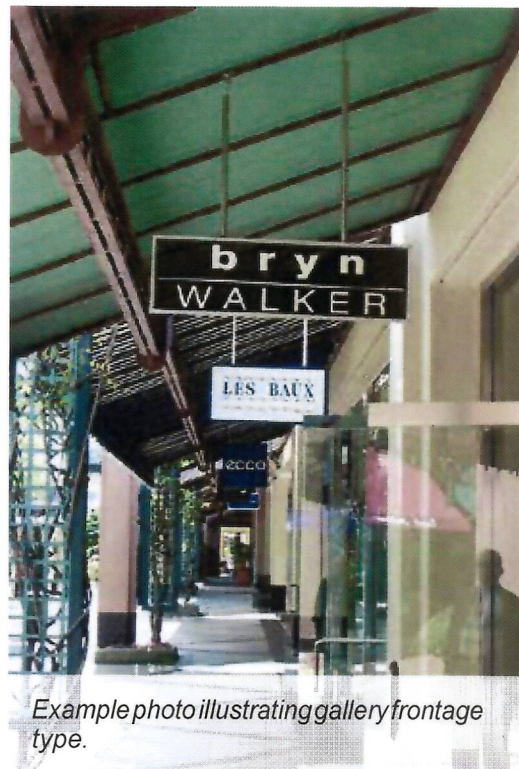
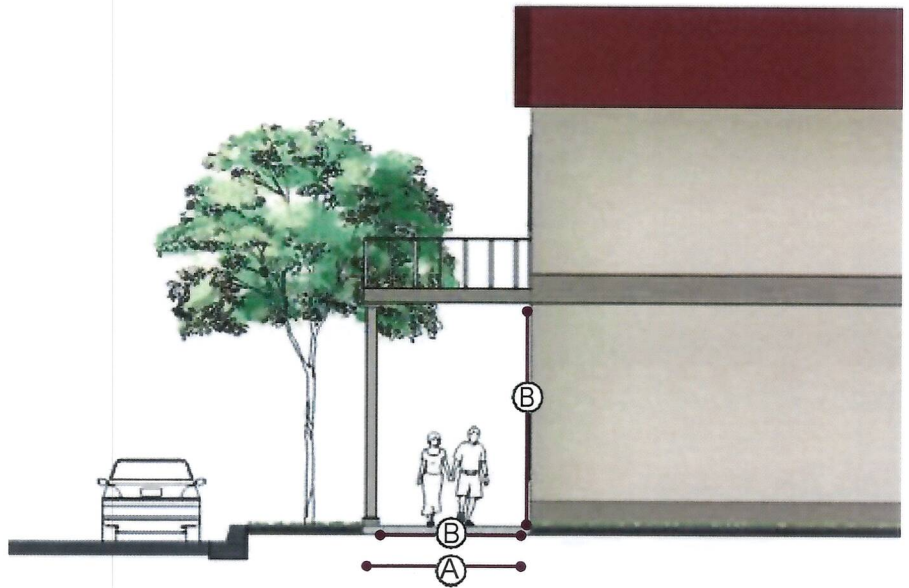


Images for illustrative purposes only.

Table 3.29 Gallery Frontage Type

GALLERY FRONTAGE
DESCRIPTION
<p>A gallery is a frontage wherein the building façade has an attached, cantilevered shed or a lightweight colonnade overlapping the sidewalk. The colonnade may support a roof or balcony above. Balconies may be used for outdoor dining. The gallery covering the sidewalk provides pedestrian shelter and opportunities for covered outdoor dining.</p> <p>This frontage type is conventional for commercial use.</p>
GUIDELINES
<p>A great variety of gallery designs are possible, but the following should apply:</p> <ol style="list-style-type: none"> a. A minimum of 10-feet and maximum of 15-feet deep. Galleries may overlap the whole width of the sidewalk within the pedestrian zone but may not encroach into the landscape area. b. Galleries should be no less than 12-feet wide and 10-feet tall. c. Galleries should align with adjacent galleries and/or arcades to the extent possible. d. Spacing between openings and/or columns should be a minimum of 10-feet. e. Shopfront openings within galleries should be at least 10-feet tall and shall provide clear views of merchandise displays within the space. f. The height and the proportions of the gallery should correspond to storefront openings and the building façade consistent with the architectural style of the building. g. Soffits, columns/arches should be treated consistent with the architecture of the building. h. Entrances should be at sidewalk grade. i. The pedestrian zone sidewalk and gallery frontage portions in the frontage zone should be consistent in design and material. j. Remaining open areas in the frontage zone shall be landscaped per Section 3.6.3, Public Realm Design.

Figure 3.17 Gallery Frontage Type



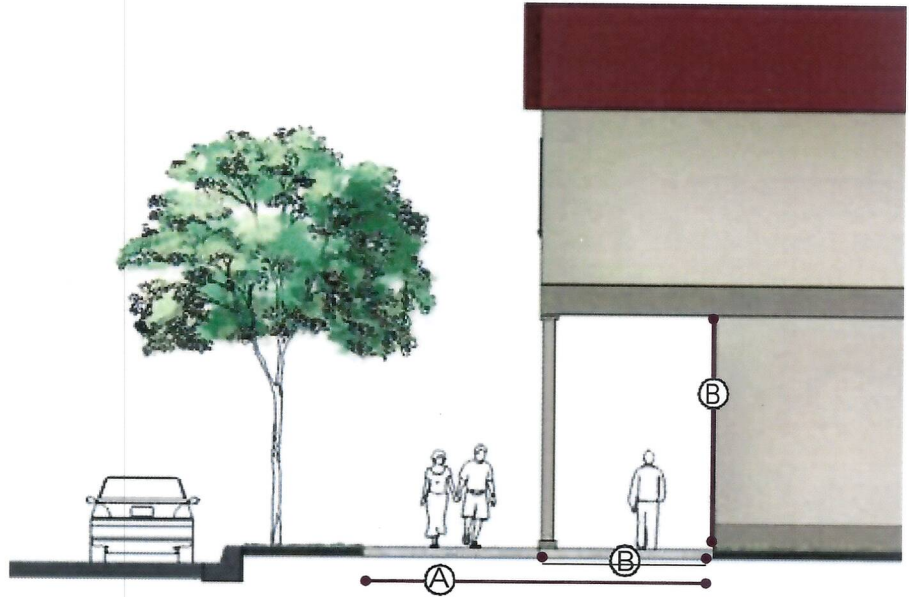
Example photo illustrating gallery frontage type.

Images for illustrative purposes only.

Table 3.30 Arcade Frontage Type

ARCADE FRONTAGE	
DESCRIPTION	
<p>An arcade is a frontage wherein the building façade is aligned close to the pedestrian zone with a colonnade recessed into the building. The colonnade supports habitable commercial or residential space above. The colonnade covering the sidewalk provides pedestrian shelter and opportunities for covered outdoor dining.</p> <p>This frontage type is conventional for commercial use.</p>	
GUIDELINES	
<p>A great variety of arcade designs are possible, but the following should apply:</p> <ol style="list-style-type: none"> a. A minimum of 10-feet and maximum of 15-feet deep. b. Arcades should be no less than 12-feet wide and 10-feet tall. c. Arcades should align with adjacent galleries and/or arcades to the extent possible. d. Spacing between openings and/or columns should be a minimum of 10-feet. e. Shopfront openings within arcades should be at least 10 feet tall and shall provide clear views of merchandise displays within the space. f. The height and the proportions of the arcade should correspond to storefront openings and the façade consistent with the architectural style of the building. g. Soffits, columns/arches should be treated consistent with the architecture of the building. h. Entrances should be at sidewalk grade. i. Remaining open areas within the frontage zone shall be landscaped per Section 3.6.3, Public Realm Design. 	

Figure 3.18 Arcade Frontage Type



Images for illustrative purposes only.



Example photo illustrating arcade frontage type.

Images for illustrative purposes only.

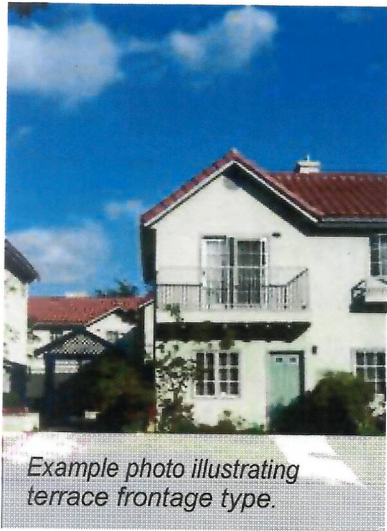
Table 3.31 Terrace Frontage Type

TERRACE FRONTAGE	
DESCRIPTION	
<p>A terrace is a frontage wherein the building façade is set back from the street, paseo, or open space, by an elevated open area that is paved or planted. This frontage type can effectively buffer building uses from the sidewalk.</p> <p>This type is recommended for residential and commercial use as it allows for semi-private use of frontage areas.</p>	
GUIDELINES	
<p>A great variety of terrace designs are possible, but the following should apply:</p> <ol style="list-style-type: none"> a. A minimum of five-feet to maximum of eight-feet deep. Terrace design should take into consideration landscape areas, where adjacent to public landscape easement, to the extent possible. b. Terraces should be raised to transition into the building but maintain ADA access. c. A minimum of six-feet wide clear for entry landing. d. Fences defining the terrace should not exceed three-feet in height from the highest adjacent grade of the terrace and comply with Section 3.6.3, Public Realm Design. e. Planted terraces and remaining open areas within the frontage zone shall be landscaped per Section 3.6.3, Public Realm Design. 	

Figure 3.19 Terrace Frontage Type



Example photo illustrating terrace frontage type.



Example photo illustrating terrace frontage type.



Images for illustrative purposes only.

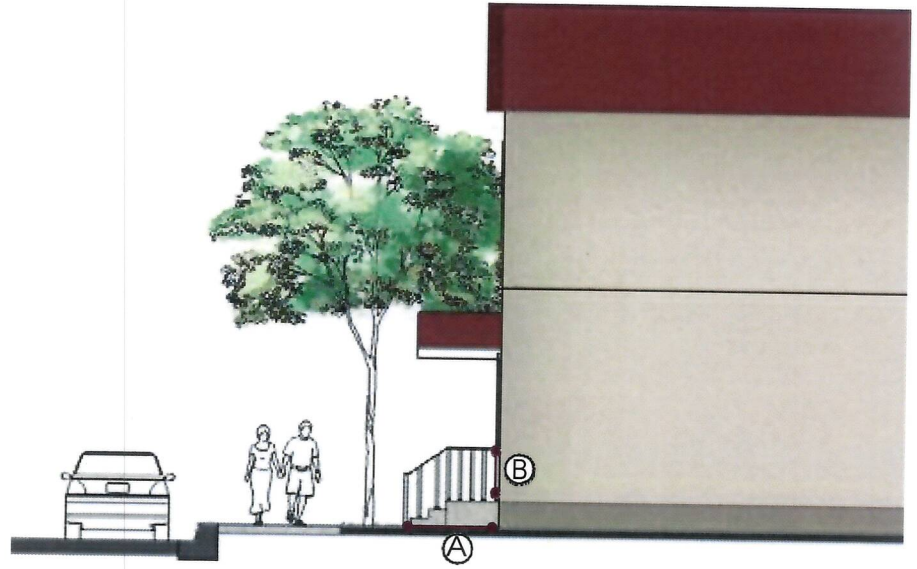
Table 3.32 Stoop Frontage Type

STOOP FRONTAGE	
DESCRIPTION	
<p>A stoop is a frontage wherein the building façade is separated from the street, paseo, or open space by an entrance to the elevated ground floor of the building. The entrance is usually an exterior stair and landing and may be covered.</p> <p>This type is recommended for ground-floor residential use as it facilitates a transition onto more residential frontage.</p>	
GUIDELINES	

A great variety of stoop designs are possible, but the following should apply:

- a. A minimum of three-feet and maximum of five-feet deep.
- b. Fences or walls defining the stoop should not exceed three-feet from the highest adjacent grade of the stoop and shall comply with Section 3.6.3, Public Realm Design.
- c. Stoops should be raised to transition into the building. The ground- story entry should not be elevated more than three-feet above the adjacent sidewalk.
- d. Stoops should correspond directly with the building entry(s) and be at least three-feet wide (perpendicular to or parallel with the adjacent walk).
- e. Building façade may be set back the depth of the entry stair from the sidewalk.
- f. The stoop may include a covered roof, awning, or door inset within the building front.
- g. Remaining open areas within the frontage zone shall be landscaped per Section 3.6.3, Public Realm Design.

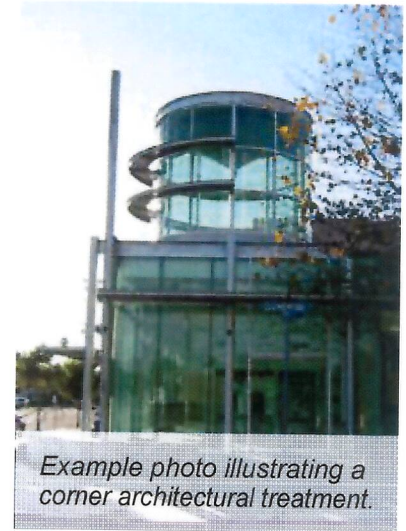
Figure 3.20 Stoop Frontage Type



Images for illustrative purposes only.

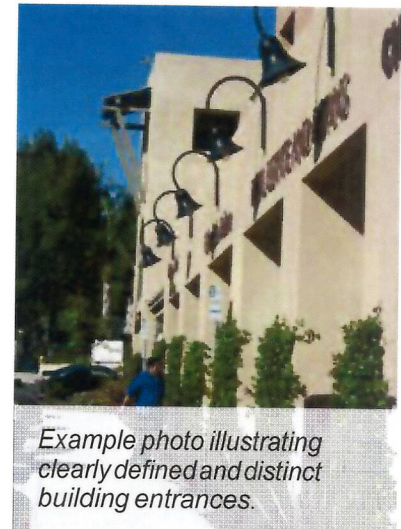
3.6.2.2 Corner Treatment

- »» Buildings at the corner of prominent intersections should have a corner entrance or an entrance oriented toward each adjacent street.
- »» Special attention should be paid to the architectural style and detail of buildings at prominent intersections, including display windows, façade materials, colors, art features, rooftop elements, and setbacks.
- »» Corner buildings along major roadways (Vermont Avenue, Carson Street, Torrance Boulevard, 223rd Street, and Normandie Avenue), should incorporate vertical elements such as towers, spires, domes, etc., to act as a landmark and orientation point.



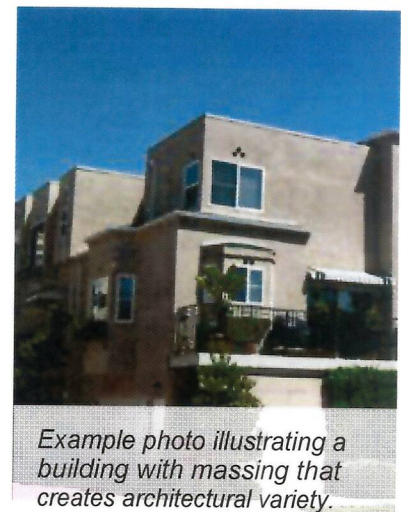
3.6.2.3 Building Entrances

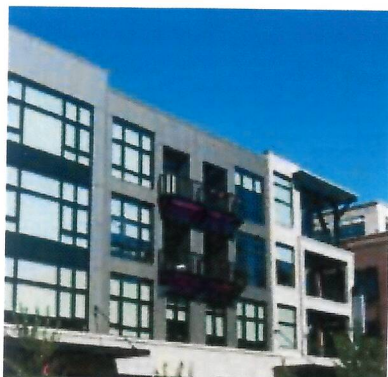
- »» Primary building entrances shall be located along major corridors and corners (Vermont Avenue, Carson Street, Torrance Boulevard, 223rd Street, and Normandie Avenue).
- »» Secondary building entrances shall be treated similarly in style and material as primary entrances to the extent possible.
- »» Each individual storefront entrance should be clearly defined and distinct from others.
- »» In mixed-use buildings, entrances to residential units shall use a separate main entrance located on the primary street.
- »» Residential uses shall have secured entrance areas that are separate from non-residential uses, but accessible from pedestrian pathways and residential parking areas.
- »» Incorporate Crime Prevention Through Environmental Design (CPTED) measures by creating well-lit and active entryways. Intimidating security features such as window grills or spiked gates are prohibited.



3.6.2.4 Scale, Mass, and Articulation

Building massing refers to how the development program is shaped into a structure that gives a building its architectural form. For example, a building can have a taller mass in one wing, step down in another wing, and have a tower that emphasizes its entrance—all of which is achieved by modeling its massing. Building massing can be used to frame public spaces, step down to adjacent uses, and provide architectural variety. It is generally more interesting to see multiple buildings with a variety of heights and massing rather than a uniform large building block.





Example photo illustrating a building façade with a distinct base and varying treatment to breakup building mass.



Example photo illustrating a building with a highlighted entryway.

- »» Building design shall avoid sprawling, monotonous façades, long straight-line building fronts, box shapes, and lackluster exterior treatments.
- »» For single-family residential, entrances and windows, not garages, should be the dominant elements of the front façades. Window and door placement, size, material, and style shall help define a building's architectural style.
- »» Multiple buildings on the same site shall be designed and grouped, to the extent possible, to provide a cohesive, visual relationship among buildings, while at the same time provide for pedestrian plazas, open space, and views.
- »» Massing breaks, such as entry courts and stepped-back corners, are required to promote visibility and allow block transparency.
- »» Buildings shall reflect the human scale both vertically and horizontally using inset windows, prominent rooflines, articulation, and highlighted entryways.
- »» A variety of roof forms and heights can be used to alleviate the overall mass of the building and add to its aesthetic quality. Roof forms shall match the overall architectural style of the building.

3.6.2.5 Façades

Façade generally refers to a building's external wall that faces a public street or open space. The design and composition of façades involves the arrangement of architectural elements, such as doors, windows, balconies, caps, and pilasters, on the walls of buildings. The façade and ground floor of a building are the most visible components seen by pedestrians, bicyclists, and motorists.

- »» Building façades shall be well defined with a distinct base, body, and roof or parapet.
- »» Façade elements such as materials, textures, patterns, colors, and detailing shall be used to lessen the perceived mass of larger buildings.

- »» The highest level of architectural detailing should be focused along the building's ground-floor façade or any area visible from the public realm.
- »» Along major corridors such as Vermont Avenue and Carson Street, breaks in the street wall should be restricted to activate the pedestrian realm.

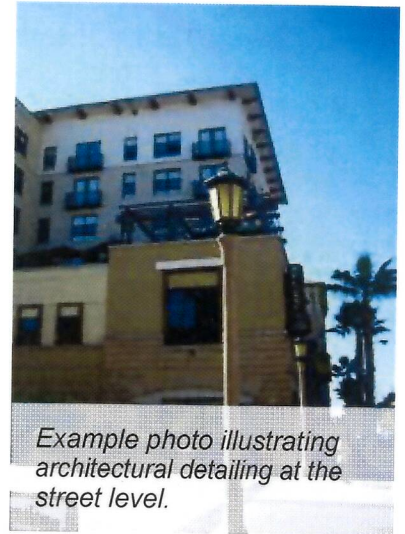
3.6.2.6 Awnings, Canopies, and Marquees

Encroachments such as awnings, canopies, and marquees are encouraged but shall be well designed and proportioned so they do not adversely impact the sidewalk environment. Canopies and awnings are encouraged along all retail street frontages.

- »» The minimum vertical clearance between the ground or street level and the encroachment shall be eight-feet. In areas of zero setback, awnings, canopies, and marquees should not project more than two-thirds the width of the sidewalk, subject to the California Building Code. At least two-feet of clearance shall be maintained between the encroachment and the street curb line.
- »» Awnings or canopies that are designed to require ground support are prohibited on sidewalks. In areas where setbacks are required, awnings, canopies, and marquees should not project past the setback line.
- »» For awnings and canopies, the materials, shape, dimensions, rigidity, reflectance, color, lighting, and signage shall relate to the architectural design of the building.

3.6.2.7 Architectural Lighting

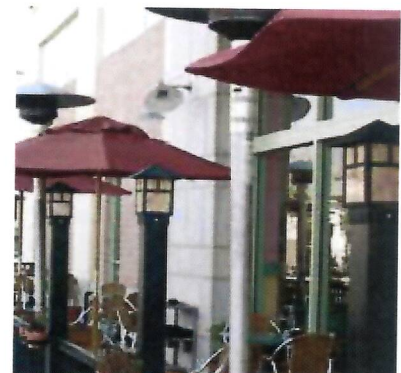
- »» Lighting shall enhance a building's form and enhance the pedestrian experience at night.
- »» Lighting shall not aim directly at the open sky or project off-site or onto adjacent uses.
- »» Architectural lighting shall highlight main building entrances and special architectural elements along the building façade.
- »» Secondary building entrances shall be properly lighted to maintain a safe environment.
- »» Internal and external storefront lighting shall be designed for ground-floor retail and restaurant spaces to augment the pedestrian space.
- »» Blinking, flashing, and oscillating lights are prohibited.



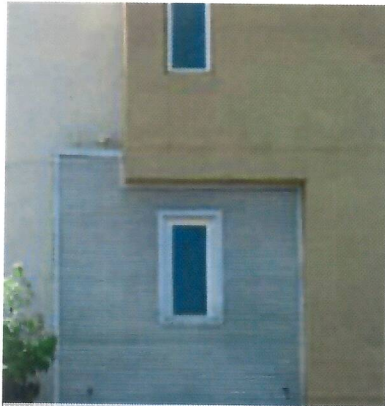
Example photo illustrating architectural detailing at the street level.



Example photo illustrating awnings and canopies that relate to the architectural design of the building.



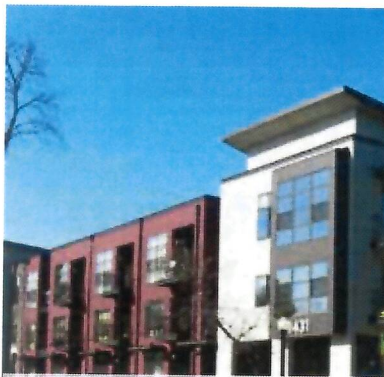
Example photo illustrating architectural lighting that enhances the pedestrian experience.



Example photo illustrating the use of color and material to create a modular façade.



Example photo illustrating the use of color and material that creates an interesting streetscape.



Example photo illustrating the use of colors and materials that are consistent with an overall architectural theme.

- »» Use warm white light where possible. Colored lights should be avoided and shall only be used if they are part of a comprehensive architectural lighting theme of commercial areas or establishments.
- »» Use automatic timers where feasible to maximize safety at night and conserve energy.

3.6.2.8 Colors and Materials

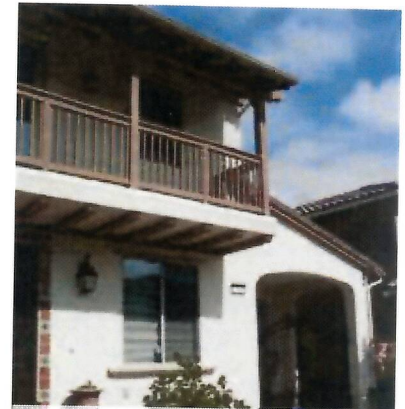
The colors and materials of buildings can effectively create a streetscape of interest when applied appropriately and in partnership with façade modulation. Quality materials not only last longer and wear better but preserve the quality of the public realm.

- »» Buildings shall use durable, high quality materials that can withstand weather and harsh external factors, are easy to maintain, and enhance the public realm. Where feasible, employ natural and local materials, including natural stone, brick, and precast concrete.
- »» Colors and materials should be consistent with the overall architectural theme and compatible with the adjacent landscape and development.
- »» In mixed-use projects, architectural style and materials can vary slightly to differentiate between residential and commercial portions of the project.
- »» All sides of a building shall be considered and should be finished appropriately to provide continuity. Backs of buildings may use more utilitarian materials provided they are compatible with the overall design.
- »» Material changes should not occur at corners; materials should continue for a minimum distance of four-feet around corners.
- »» Use variation in materials and colors to distinguish form changes at entrances, different uses or tenants, and between stories. Rough-coated stucco shall be prohibited.
- »» Concrete and similar finishes can be used as long as they are finished and part of an overall architectural composition.

3.6.2.9 Windows, Doors, and Balconies

Windows, the main source of natural light and fresh air into buildings, should be designed to maximize the light that enters and to take advantage of natural ventilation.

- »» Clear glass should be used on the ground floor of non-residential buildings with minor obstructions. Windows on the ground floor facing streets shall constitute a minimum of 30 percent of the building façade, with minimal obstruction from signs or interior displays.
- »» "Eyes on the street" along major corridors shall be emphasized by placing balconies and bay windows along upper stories.
- »» For residential buildings, windows shall be of high quality and afford a shadow line as well as depth. This may be achieved through inset windows with an integral frame or inseting the window into the exterior wall.



Example photo illustrating a balcony on the upper story to provide "eyes on the street."

3.6.2.10 Roofs

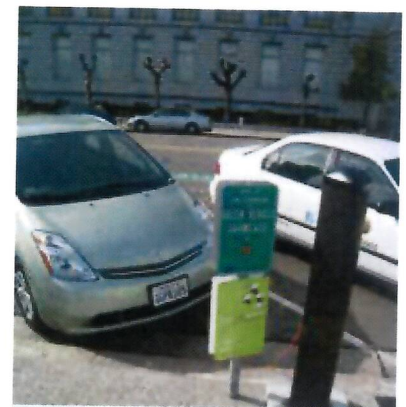
- »» Roof style shall complement the overall architectural style of the building. A variety in planes, heights, and styles shall be used.
- »» Roof access shall be provided from the interior of the building and not through exterior ladders.
- »» Rooftop mechanical equipment shall be located below the highest vertical element of the building, and properly screened from public view.
- »» Inclusion of green roofs that include living plants and growing medium on top of a typical roofing system are encouraged.



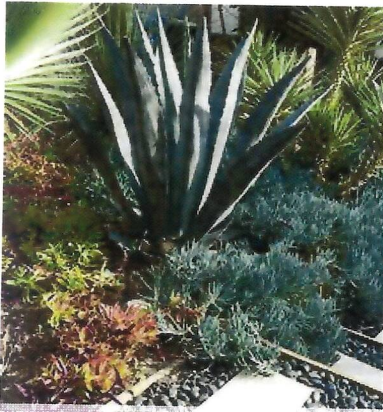
Example photo illustrating a roof style that complements the architecture and screens mechanical equipment.

3.6.2.11 Green/Sustainable Building Design

- »» Energy efficient, non-toxic, and recycled-content building materials shall be used whenever possible, such as EPA "Energy Star" labeled windows.
- »» Natural lighting shall be utilized where possible to maximize daylighting and reduce cooling and heating requirements.
- »» Use materials that reduce the transfer of heat into and/or out of the building, such as cool roofs.
- »» Buildings and parking garages shall integrate sustainable design features, such as photovoltaic panels, cool roofs, grey water systems, and other features to reduce energy consumption.
- »» In new development, incorporate zero emission and electric vehicle charging stations in parking areas.



Example photo illustrating an electric vehicle charging station.



Example photo illustrating landscaping in the public realm.



Example photo illustrating bio-filtration and bio-retention features integrated with landscaping.



Example photo illustrating landscaping used as a screening element.

- »» Where feasible, use recyclable and sustainable building materials in new development.

3.6.3 PUBLIC REALM DESIGN

3.6.3.1 Landscaping

In all zoning areas, all landscaping shall conform to the landscape standards contained in this Specific Plan or, for issues not addressed in the Specific Plan, Title 12 (Environmental Protection), Title 22 (Planning and Zoning), and Title 31 (Green Building Standards) of the County Code.

- »» All commonly owned property and landscaped setback areas, exclusive of structural improvements, shall be landscaped and maintained in a weed-free condition with a combination of trees, shrubs, and ground cover.
- »» Landscaping at intersections shall be designed to not restrict vehicular sight distance.
- »» Projects shall integrate drought-tolerant, native, and low-water-use plants and grasses where possible.
- »» Permeable surfaces should be used where feasible, and lawns should be limited to areas that serve a functional purpose.
- »» Landscaping shall be used to highlight building façades; screen less attractive elements; add color, texture, and visual interest; provide shade; and define the spatial organization of the site.
- »» Bio-filtration and bio-retention measures are encouraged to slow and treat stormwater runoff.
- »» Plantspecies and design shall avoid the creation of unsafe places hidden to the public.
- »» Landscaping requiring irrigation systems shall utilize highly efficient irrigation systems such as drip and bubbler irrigation and low-angle, low-flow spray heads.
- »» Inclusion of green roofs may contribute landscaping requirements.

3.6.3.2 Screening: Fences, Walls, and Gates

All provisions of the County Code shall apply to the construction of walls, fences, and hedges in the Specific Plan, with the exception of the following:

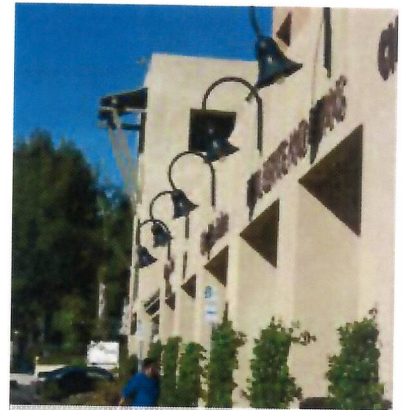
- »» If fencing is required for security reasons in the front yard, wrought-iron-style fences that do not obscure views are encouraged.
- »» The use of barbed wire, electrified fence, and chain-link fence in conjunction with any fence, wall, roof, or hedge is prohibited unless required by any law or regulation of the County, federal government, or agency thereof, as applicable.
- »» Walls and fences shall be constructed of durable materials and designed to complement the surrounding architecture.
- »» Defacement of walls and fences should be prevented through the use of trees, vines, and other landscaping.

3.6.3.3 Outdoor Lighting

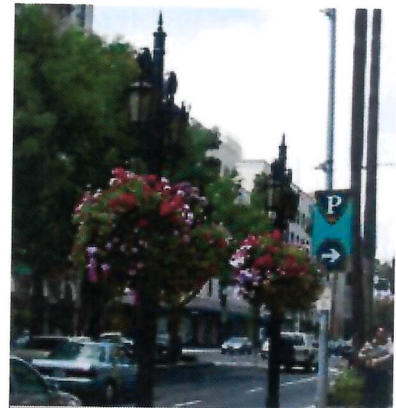
- »» Lighting should be human scale and shall be located at all building entryways, parking areas, seating areas, transit stops, open space areas, and pedestrian paths.
- »» Lighting fixtures shall be compatible with the architectural style of surrounding buildings to reflect the character of the area.
- »» Lighting shall be provided at intervals adequate for safety, while minimizing light spillage and glare onto adjacent uses.
- »» Freestanding light fixtures shall be placed outside of pedestrian and bicycle pathways.
- »» Light fixtures shall provide a warm light and use energy-efficient technology, such as solar-powered lighting.

3.6.4 SIGNAGE

- »» All signage shall be consistent with Chapter 22.114 of Title 22.



Example photo illustrating outdoor lighting located at all building entryways.



Example photo illustrating outdoor lighting provided at regular intervals.

3.6.5 SPECIAL TREATMENTS: TRANSIT STATION AREAS & GATEWAYS

Key intersections and gateways require greater attention to detail due to their prominent locations and sensitive relationship to the public realm. Special attention to the treatment of buildings and the public realm at key locations can greatly enhance the character of the area and establish a unique sense of identity.

Key opportunity areas exist along major corridors and entrance points in the Specific Plan area—Vermont Avenue, Carson Street, Torrance Boulevard, 223rd Street, and Normandie Avenue.

- Orient and design buildings located at key gateways and intersections to emphasize the corner as a node of activity and architectural prominence. Solutions for developing projects that are of an exemplary quality at gateways or prominent intersections include:
 - »» Tower elements as a prominent massing feature.
 - »» Entry plazas on corner sites.
 - »» Distinct changes in the building volume at the primary entry.
 - »» Prominent landscape features, such as tall trees.
 - »» Unique building lighting for nighttime effect.
 - »» Public art installations that reinforce a theme reflective of the community.
- Buildings should serve as iconic representations of the community character.
- Trademark buildings (franchise architecture) shall be prohibited if they are not consistent with other design principles established in the standards and guidelines herein.

04

MOBILITY AND PUBLIC REALM

INTRODUCTION

The West Carson Mobility and Public Realm Strategy describes the circulation improvements needed to support transit oriented development within the Specific Plan area. A key component of the Specific Plan is the transformation of the current circulation network, which largely supports vehicular travel, to a network that places a higher priority on the principles of complete streets and multi-modal design. The strategies set forth in this document are intended to provide a framework for establishing and maintaining a sustainable circulation network that supports both motorized and non-motorized modes of transportation together in an integrated system.

GOALS AND POLICIES

The following goals and policies set the framework for the mobility and public realm strategy of West Carson. They serve as guidelines and provide direction for future decision-making and development activities. The goals and policies identified in this section were derived from input received from community members, stakeholders, and County staff during the community engagement process and County Task Force meetings. The following lists the major mobility goals and policies for the Specific Plan.

- **Goal 1:** Provide and maintain a comprehensive circulation system that improves accessibility to transit, connections within the community, and the safe and efficient movement of all users of the roadway.
 - »» Policy 1.1: Implement complete streets designs that contribute to a multi-modal transportation system.
 - »» Policy 1.2: Ensure that roadway improvements allow for easier, safer, and more efficient transit operations, as well as improved passenger safety and accessibility.
 - »» Policy 1.3: Consult with local jurisdictions to provide attractive and convenient bus stops, including shade/ weather protection, seats, transit information, and bus shelters where appropriate.
 - »» Policy 1.4: Consider the interactions between bus and bicyclists and design bus stops that will help minimize conflicts.
- **Goal 2:** Provide safe, connected, and accessible bikeway and pedestrian network.
 - »» Policy 2.1: Establish a connected pedestrian and bicycle network that links Metro's Silver Line Station, Harbor-UCLA Medical Center, residential neighborhoods, local schools, and retail corridors.

- »» Policy 2.2: Complete bicycle infrastructure improvements that close gaps in the County's Bicycle Master Plan and those providing connections to adjacent communities to enhance regional connectivity.
- »» Policy 2.3: Identify opportunities to create dedicated bicycle lanes and pedestrian sidewalks that connect the neighborhood and commercial areas to community services.
- »» Policy 2.4: Establish and maintain attractive and functional sidewalks that maximize accessibility, enhance the pedestrian environment, and foster social interaction.
- »» Policy 2.5: Design bicycle and pedestrian infrastructure in accordance with federal, State, and local design standards, including ADA accessibility standards.
- **Goal 3:** Provide and maintain attractive mobility corridors that promote livability and sustainability.
 - »» Policy 3.1: Implement streetscape features such as street lighting, street trees, landscaping, and wayfinding to create safer and attractive corridors.
 - »» Policy 3.2: Integrate pedestrian amenities, such as benches and public art to transform the streetscape and create public space.
 - »» Policy 3.3: Identify new opportunities to incorporate public park and open space improvements within the area that provide small-scale, but well-designed outdoor areas for unstructured play and socializing.
- **Goal 4:** Promote efficient use of parking resources and support programs that attempt to induce mode shifts from single auto occupancy travel to transit, rideshare, bicycle, or pedestrian travel.
 - »» Policy 4.1: Utilize shared parking where possible and establish guidelines and standards to optimize parking supply.
 - »» Policy 4.2: Encourage and allow shared parking for new development in lieu of the provision of off-street parking spaces.

STREET NETWORK

The Specific Plan provides guidance for the design of a comprehensive and context sensitive street network as shown in Figure 4.1 Street Network, to connect the West Carson community. Much of the street network within the Specific Plan area will remain the same to support new development and growth within the area, however some streetscape improvements are proposed along key arterials within the Specific Plan area. These improvements are intended to transform the existing auto-oriented streetscape into a more sustainable and multi-modal design. The Specific Plan's roadway and circulation network plans are described below.

TORRANCE BOULEVARD

Existing Conditions

Torrance Boulevard is classified as a Secondary Highway on the County's Highway Plan and runs east and west at the northern edge of the Specific Plan boundary. The corridor currently meets the minimum width right-of-way standards for a Secondary Highway classification as set forth in the General Plan, which is 80-feet. The corridor is surrounded mostly by residential land use with some light industrial and general commercial use. The posted speed limit is 35 miles per hour. Within the project area, the roadway consists of two travel lanes in each direction with a dedicated auxiliary lane in the center. On-street parking is not permitted along the corridor within the project area. Torrance Transit operates a local bus line along a short segment of the corridor.

Vision

Torrance Boulevard's role as a key corridor connecting neighborhoods and communities within West Carson should be reinforced and enhanced through the provision of a well-connected network of high quality pedestrian and bicycle infrastructure. Wider sidewalks with pedestrian amenities, such as street trees, landscaping, and lighting should be considered to enhance safety and the overall pedestrian environment. Bicycle facilities and amenities should be installed to improve connections to the greater bikeway network, promote active modes of transportation, and to provide first-and-last mile strategies to transit.

Plan Strategy

Consistent with the County's Bicycle Master Plan, the Specific Plan proposes the addition of Class II bicycle facilities along Torrance Boulevard to improve connections to the regional bikeway

network, which includes the proposed 208th Street multi-use path and the Dominguez Channel located in the neighboring City of Carson. The Specific Plan also encourages the provision of community facilities, such as community centers, community gardens, and libraries, as well as enhancements to the pedestrian environment such as landscaping, street trees, and lighting to encourage more pedestrian activity and social interactions.

VERMONT AVENUE

Existing Conditions

Vermont Avenue is classified as a Major Highway on the County Highway Plan and runs north and south within the Specific Plan boundary. The corridor currently meets the minimum width right-of-way standards for a Major Highway classification as set forth in the General Plan, which is 100-feet. The corridor is surrounded by a variety of land uses including residential, mixed use, light industrial, and public space. The posted speed limit is 40 miles per hour. Within the project area, the roadway consists of two travel lanes in each direction with a dedicated auxiliary lane in the center. Class II striped bike lanes also exist in each direction within the project area. On-street parking is permitted along much, but not all of the corridor within the project area. Torrance Transit and Metro operate bus lines along the corridor.

Vision

Vermont Avenue serves as a primary transit corridor within West Carson, with multiple bus routes from various local transit agencies traversing through the corridor. Vermont Avenue's role as a key transit corridor should be reinforced and enhanced through the provision of high quality transit stop amenities and pedestrian infrastructure, landscaping, lighting, as well as streetscape improvements that will support high levels of pedestrian activity and improve accessibility to the area's rich transit network.

Plan Strategy

The Specific Plan introduces mixed-use and higher density residential development along Vermont Avenue to activate the corridor and encourage more pedestrian activity. The Specific Plan will also introduce streetscape improvements including a striped buffer between existing Class II bicycle facilities and on-street parking to improve bicycling safety and landscaped medians to improve overall aesthetics along the corridor.

NORMANDIE AVENUE

Existing Conditions

Normandie Avenue is classified as a Secondary Highway on the County Highway Plan and runs north and south within the Specific Plan boundary. The corridor currently does not meet the minimum width right-of-way standards for a Secondary Highway classification as set forth in the General Plan, which is 80-feet. The roadway is located entirely within the City of Los Angeles and is not maintained by Public Works. The corridor is surrounded by a variety of land uses including residential, mixed use, and public space. The posted speed limit is 35 miles per hour. Within the project area, the roadway consists of two travel lanes in each direction with a dedicated auxiliary lane in the center. On-street parking is permitted along much, but not all of the corridor within the project area. Gardena Municipal and Torrance Transit operate bus lines along the corridor.

Vision

Normandie Avenue is a major roadway connector throughout the Specific Plan area as well as throughout the local region. Normandie Avenue is a key point of access to the West Carson area and the Harbor-UCLA Medical Center, with several bus routes from multiple transit agencies servicing the corridor. Its role as a key corridor should be reinforced and enhanced through the provision of streetscape improvements, transit amenities, and a well-connected network of high quality pedestrian and bicycle infrastructure.

Plan Strategy

The Specific Plan proposes wider sidewalks along Normandie Avenue to accommodate high levels of pedestrian activity generated from the Harbor-UCLA Medical Center and the adjacent proposed mixed-use land use. The Specific Plan also introduces a Class II bicycle facility along Normandie Avenue to improve connections to the greater regional bikeway network.

Figure 4.1 Street Network

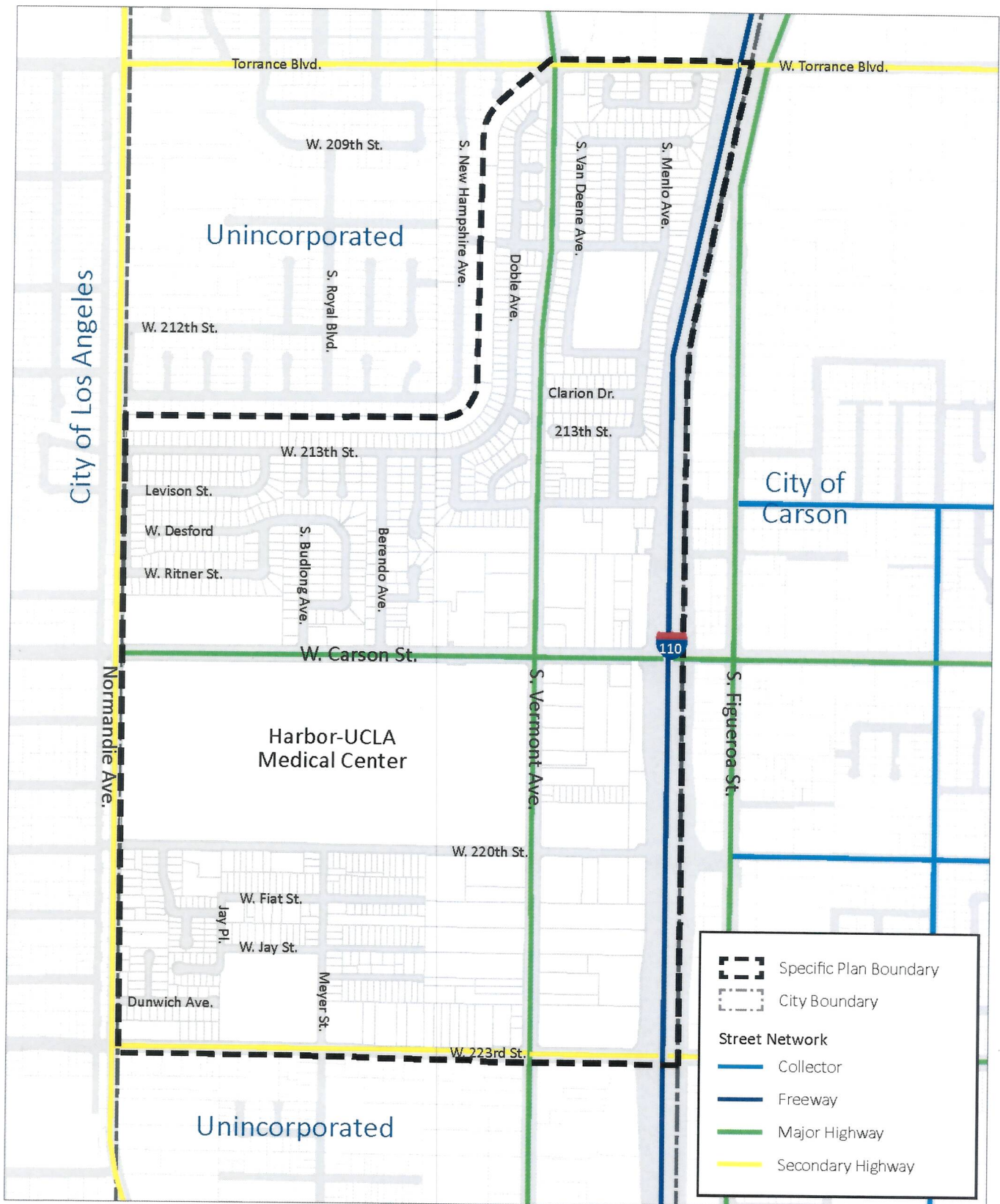




Photo showing the existing condition of Carson Street.

CARSON STREET

Existing Conditions

Carson Street is a Major Highway that runs east and west within the Specific Plan boundary. The corridor currently meets the minimum width right-of-way standards for a Major Highway classification as set forth in the General Plan, which is 100-feet. The posted speed limit is 35 miles per hour. Within the Specific Plan area, the roadway consists of two travel lanes in each direction with a dedicated auxiliary lane in the center. On-street parking is permitted along much of the corridor. Torrance Transit and Metro operate bus lines along the corridor.

Vision

Carson Street is a gateway to the Specific Plan area and serves as a primary roadway connector to the Harbor-UCLA Medical Center. The Specific Plan emphasizes a more livable and sustainable Carson Street with more mixed-use development opportunities, enhanced pedestrian and bicycle infrastructure, and a focus on a multi-modal approach to circulation for the area.

Plan Strategy

The Specific Plan proposes mixed-use and higher density development along and adjacent to Carson Street to lay the foundation for a more livable and sustainable corridor that works to improve air quality, traffic congestion, and mobility. The Specific Plan introduces wider sidewalks, landscaping, street trees, reduced on-street parking, striped buffers between existing bicycle facilities and vehicular traffic, and a multi-use pathway to support active modes of transportation. The Specific Plan also encourages the provision of transit amenities, such as shelters, benches, lighting, wayfinding, service route maps and information, and streetscape improvements that focus on facilitating the safe and efficient movement of transit.

223RD STREET

Existing Conditions

223rd Street is a Secondary Highway that runs east and west within the Specific Plan boundary. The corridor meets the minimum width right-of-way standards set forth in the County Code, which requires various right-of-way widths ranging from 80-feet to 100-feet depending on the location. The corridor is surrounded by a variety of land uses including residential, general commercial, light industrial, and public space. The posted speed limit is 25 miles per hour between Normandie Avenue and Vermont Avenue and 35 miles per hour east of Vermont Avenue. Within the project area, the roadway consists of two travel lanes in each direction. On-street parking is permitted along much of the corridor.

Vision

The Specific Plan transforms the existing auto-oriented design of 223rd Street to a more multi-modal streetscape, incorporating the principles of complete streets into its design with the provision of bicycle facilities, pedestrian amenities, and traffic calming methods to improve safety.

Plan Strategy

Consistent with the County's Bicycle Master Plan and recent efforts to secure grant funding for active transportation projects, the Specific Plan introduces both a Class II and a Class III bicycle facility along various segments of the corridor to improve connectivity to the regional bikeway network. The proposed bicycle facilities are also intended to provide first-last mile solutions to transit within the West Carson area.



Photo showing the existing condition of 223rd Street.

TRANSIT CIRCULATION

A key component of the Specific Plan is to improve accessibility to the existing transit system and the overall transit experience, which encompasses more than the transit ride itself, but also includes getting to and from a transit stop and the rider's experience waiting for transit. The following section describes transit circulation within the study area and identifies strategies to improve transit access and the overall transit experience.

LOCAL BUS SERVICES

The Specific Plan area encompasses a rich transit network that is serviced by three local transit agencies which includes Metro, Torrance Transit, and Gardena Municipal. As illustrated in Figure 4.2, a total of eight local bus routes traverse throughout the Specific Plan area on primary transit corridors that include Normandie Avenue, Vermont Avenue, Carson Street, and 220th Street. Of these streets, Normandie Avenue, Vermont Avenue, Carson Street, as well as 223rd Street have been identified as streets with completed Traffic Signal Synchronization Programs (TSSP), which help to prioritize the movement of transit vehicles.

The local bus routes identified in Figure 4.2 not only provide service within the West Carson community, but also serve as feeder routes to the Metro Silver Line, located along the Interstate 110 freeway. The Silver Line provides key connections between Downtown Los Angeles and the South Bay communities.

The Specific Plan recommends coordinating operating schedules between local feeder bus routes and the Silver Line to improve overall transit service. Schedule improvements such as minimizing passenger wait times between transfers can help improve efficiency and consequently encourage more transit ridership. Recommended improvements include improvements to bus route arrival/departure times, frequency, and the relocation of bus stops to allow for better connection timing to the Silver Line stop. Local transit agencies should collaborate to ensure service hours for routes improve transit connectivity and the efficiency of multi-operator transit trips.

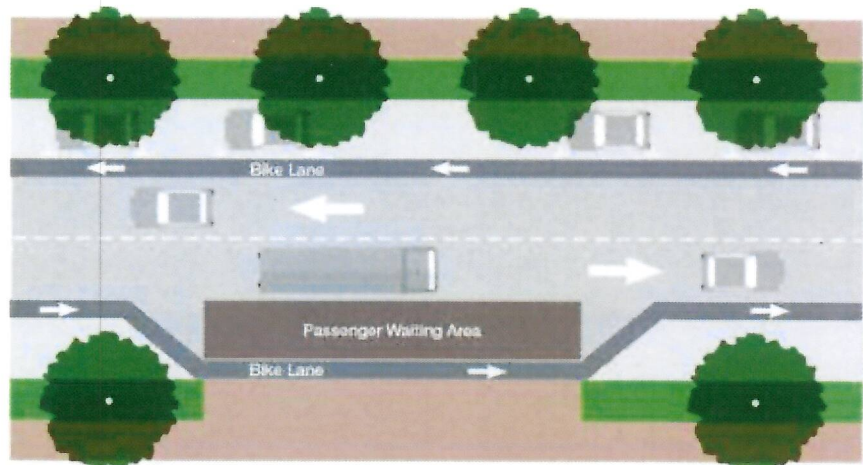
Figure 4.2 Existing Transit Network



BUS AND BIKE INTERFACE

The Specific Plan also acknowledges that alternative modes of transportation, such as transit and bicycling are complementary modes and must often interact with one another on urban and suburban streets. The coexistence between buses and bikes on roadways, however, can present significant challenges due to differences in size, average speed, and stopping patterns. Conflicts often arise as bicyclists must share the right-hand lane and curb with stopping buses. To minimize these types of conflicts, the Specific Plan encourages the exploration of alternative bus stop designs outside of the conventional curbside stops. One such design includes creating a short bike channel that diverts bicycle traffic behind transit stops as depicted in Figure 4.3. Additional analysis should be conducted to determine feasibility and to evaluate design effectiveness.

Figure 4.3 Floating Bus Stop And Bike Channel Design



TRANSIT ACCESS, COMFORT AND SAFETY

- A key element impacting the overall transit experience includes the safety and comfort of the transit stop environment. An individuals' perception of safety and comfort walking to and from a transit station as well as waiting for transit will impact their decision to use public transit. The Specific Plan proposes the following improvements to safety and comfort for the Carson Street stop.

Transit Stop Relocation Along the Interstate 110

The existing Interstate 110 Carson Street transit stop, which services Metro's Silver Line, is accessed by patrons via stairways and elevators from the Carson Street overpass. The stop's location directly below the Carson Street overpass and along the Interstate 110 freeway contributes to its low levels of perceived safety. Although lighting exists at the transit stop, the stop lacks a sense of transparency, or the degree to which an individual can see or perceive what lies beyond the edge of a street or public space to feel safe. Additionally, high travel speeds along the freeway also impact perceived safety as the existing stop fronts the Interstate 110 freeway. These factors likely contribute to the low utilization of the existing station.

To improve transit access and safety, the Specific Plan proposes to move the existing transit stop from below the Carson Street overpass to a new location along the Interstate 110 freeway. Relocating the stop from underneath the overpass would enhance visibility of waiting transit patrons and improve safety. An example of this configuration is illustrated in Figure 4.4.

Figure 4.4 Proposed Transit Stop Relocation



Freeway Underpass and Overpass Enhancements

Because the Interstate-110 freeway forms the eastern boundary of the Specific Plan area, freeway underpass and overpass enhancements are key in improving safety and comfort for the residents and visitors of West Carson. The Specific Plan recommends incorporating visually-engaging elements at freeway crossings to make a friendlier street and pull active transportation users along the pathway. Incorporating public art along the Interstate-110 overpass or underpass can also help engage pedestrians and bicyclists by giving them compelling things to look at.

The pedestrian experience and safety along the overpass can also be improved by installing special paving and bollards along curb edges. These features help to improve safety for pedestrians with visual impairments and provide a valuable cue separating the sidewalk from the roadway and vehicular traffic.

Transit Amenities

Transit stop amenities work to improve operations, ridership levels, and the overall transit experience. Amenities can include shelters, improved plaza areas, benches, lighting, transit information, bicycle racks, and public art. Well-designed transit stops can improve patron comfort and convenience and attract new riders. Installation of transit stop amenities should be done in consultation with the local transit agencies servicing the area, which includes Metro, Torrance Transit, and Gardena Municipal. Figure 4.26 Pocket Park Concept at the Caltrans Park-n-Ride lot at Carson Street and Interstate-110, is another recommendation for improving transit amenities and safety near the Carson Street Station. These improvements would require coordination with Caltrans, the owner of the parcel.

Bus shelters play an important role in transit operations. They provide patrons shelter from varying weather conditions and provide a place to rest and wait. Bus shelters should provide other amenities such as benches, stop ID, route information, and lighting. Additionally, shelter placement should not obstruct the loading and unloading of passengers or the pedestrian pathway.

Transit information is also an important amenity at transit stops. Whenever possible, transit stops should include information on service routes and schedules, as well as local area maps and wayfinding information. Providing transit information at stops allow patrons to determine whether they are waiting at the correct stop and where they need to go once they arrive at their stop. Ideally, real-time arrival information should also be included whenever possible to improve transit reliability and encourage transit usage.

FIRST-LAST MILE STRATEGIES

In 2014, Metro approved its First Last Mile Strategic Plan, which identifies design strategies to improve active transportation access and connections to public transit. The Specific Plan recognizes that station access is a key element in successful TOD station area planning and identifies strategies that focuses on improving accessibility during the first and last miles of a transit rider's journey. These strategies include streetscape improvements, bicycle and pedestrian infrastructure improvements, as well as signage and wayfinding improvements.

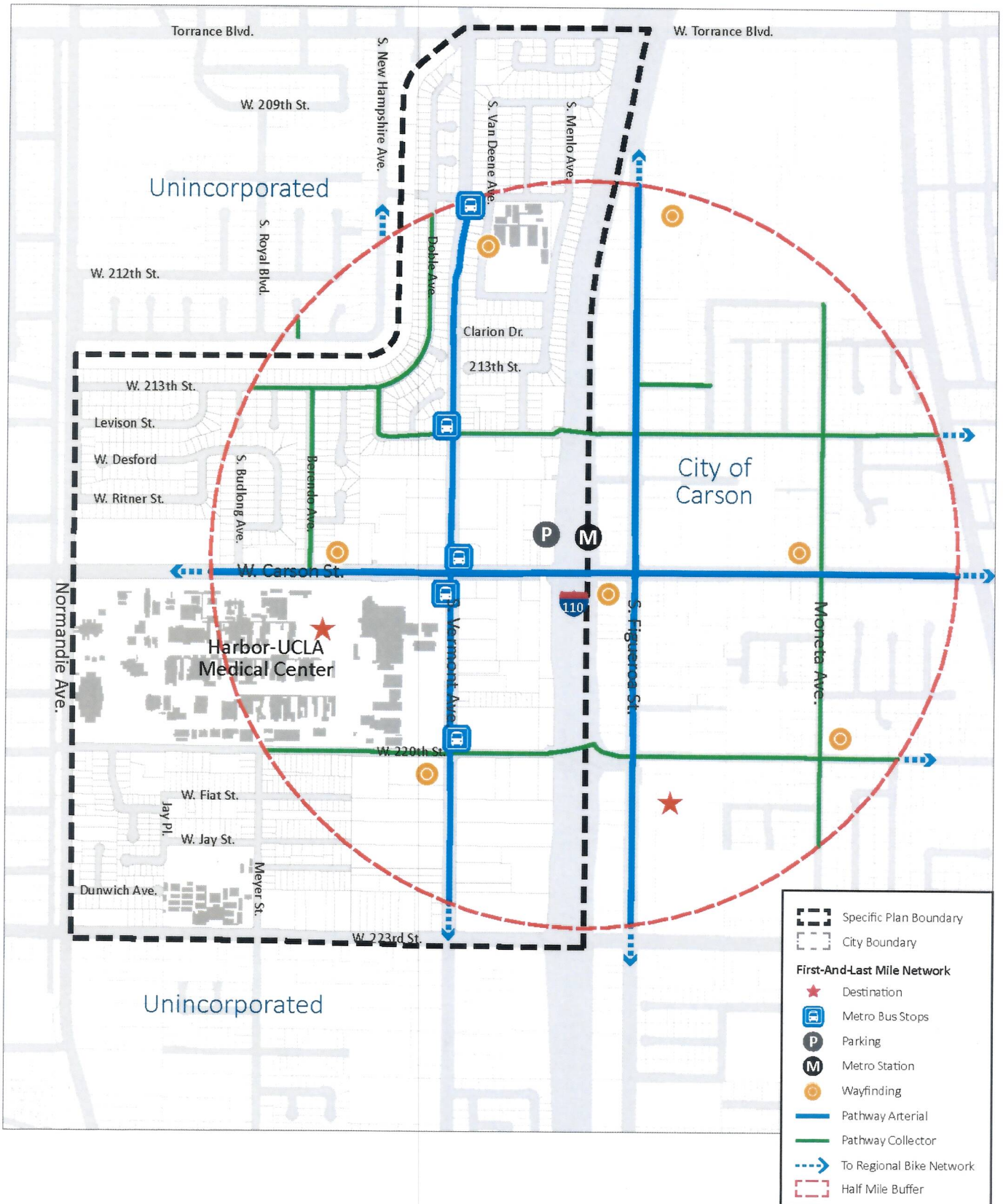
PATHWAYS

The Metro Pathways concept established in Metro's First-Last Mile Strategic Plan includes a hierarchy of pathways that extend to and from a transit station and is designed to support active modes of transportation. These pathways take into consideration the existing street network, key destinations, feeder transit services, existing and planned infrastructure, existing bike and pedestrian volumes, and surrounding land uses to design a physical active transportation network that improves transit station access. The Metro Pathways concept is comprised of two types of pathways, pathway arterials and pathway collectors. Pathway arterials serve as the main branches of the network, while pathway collectors serve as feeder routes. Figure 4.5 illustrates the proposed Metro Pathway network and includes pathway arterials, pathway collectors, existing Metro bus stops, key destinations, and suggested areas for wayfinding signage.

Pathway Arterials

Pathway arterials are primary routes that extend from stations and support maximum throughput activity for active transportation users. They are designed to accommodate high levels of active transportation and a broad range of users. They typically include design treatments such as separated active transportation lanes, signal and crossing improvements, wayfinding, and plug-in components (i.e. bike share). Pathway arterials should typically radiate out from a station portal in at least four directions, and extend out at a minimum of one-half mile from the station to an upper limit of three miles from the station. Pathway arterials should also integrate the regional bikeway network at opportune points beyond the one-half mile access shed. As illustrated in Figure 4.5, pathway arterials surrounding the Carson Street station include Carson Street, Vermont Avenue, and Figueroa Street.

Figure 4.5 First And Last Mile Pathway Network Map



Pathway Collectors

Pathway collectors are routes within the station area that both feed into pathway arterials and support crossing movements and general station area permeability. They work to reduce travel distances for non-motorized users by focusing on the provision of efficient access to pathway arterials. Pathway collectors include streets and routes located within the one-half mile access shed of a transit station and streets that feed into the main branch lines or pathway arterials. They typically include design considerations that improve intersection and mid-block crossings. As illustrated in Figure 4.5, pathway collectors surrounding the Carson Street station include 213th Street, 214th Street, 220th Street, Moneta Avenue, and Berendo Avenue.

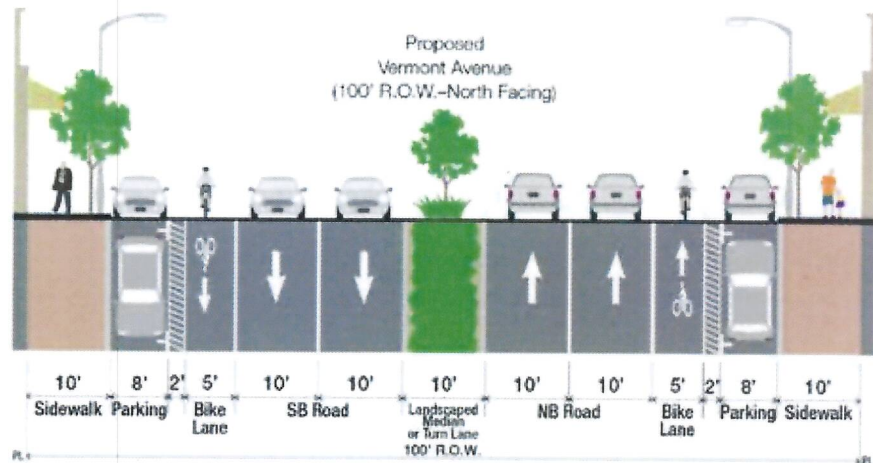
STREETSCAPE IMPROVEMENTS

The Specific Plan recognizes the role that the built environment and street design plays on a community's health and wellness. Well-designed streets can provide safer and more attractive settings for people to walk and bike, as well as to encourage more transit use. The Specific Plan focuses on laying the foundation for a more unified network of streets that promote multi-modal circulation as well as the safe and efficient movement of motorized and non-motorized modes of transportation. The Specific Plan proposes the following streetscape improvements to promote more livable and sustainable streets. More detailed landscape and hardscape design recommendations are included in the Public Realm Design and Park Design section below.

Vermont Avenue

Vermont Avenue serves as a pathway arterial connecting the Carson Street transit stop. Proposed streetscape improvements for Vermont Avenue are illustrated in Figure 4.6. The improvements are intended to enhance and promote multi-modal activity along the corridor as well as to support its role as a pathway arterial with high levels of non-motorized activity. Streetscape improvements include a Class II bicycle facility with striped buffer between on-street parking to enhance safety, street trees, lighting, reduced travel lane widths, and a landscaped median to improve visual aesthetics along the corridor. The Specific Plan also recommends the installation of wayfinding signage along the corridor to improve navigation to the transit stop.

Figure 4.6 Proposed Vermont Avenue Streetscape Improvements



Carson Street

Similar to Vermont Avenue, Carson Street also serves as a pathway arterial connecting the Carson Street transit stop and serves as a primary transit corridor with several local bus routes traversing the corridor. Carson Street's role as a pathway arterial and transit corridor solidifies the need for a multi-modal streetscape design that prioritizes the safe and efficient movement of multiple modes of transportation. Streetscape improvements to Carson Street are illustrated in Figure 4.7 and include wider sidewalks, Class II bicycle facilities with striped buffers, reduced travel lane widths, landscaped medians street trees, lighting, and a multi-use path along the south side of Carson Street. The Class II bicycle facilities proposed in this Specific Plan are consistent with existing County street improvement plans. The County was recently awarded grant funding from the California Caltrans Active Transportation Program (ATP) to install a 0.5 mile Class II bicycle facility along Carson Street between Normandie Avenue and Vermont Avenue. The new bikeway is intended to improve connections to existing facilities and the regional bikeway network and support Metro's First-Last Mile Strategic Plan.

Figure 4.7 Proposed Carson Street Streetscape Improvements

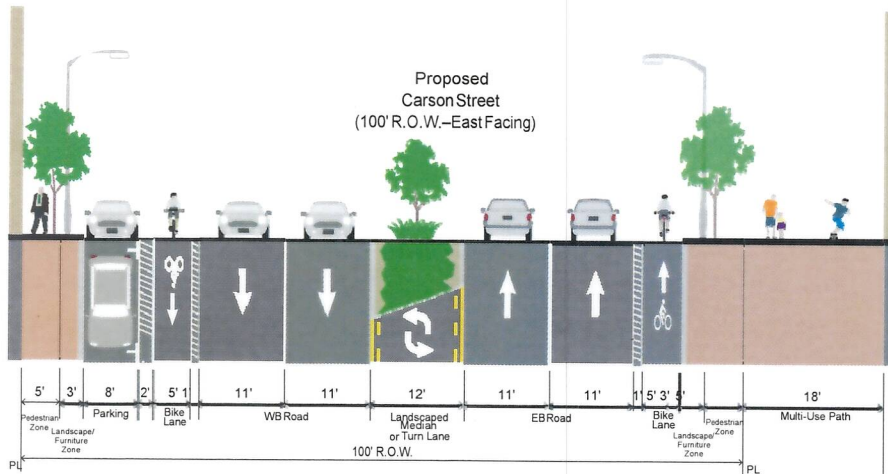
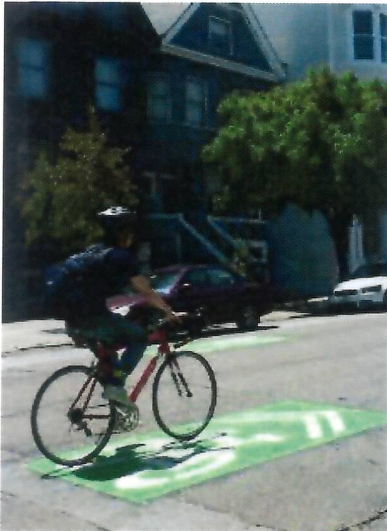


Figure 4.8 Green-backed Sharrows



Possible treatment options for green-backed sharrows.

223rd Street

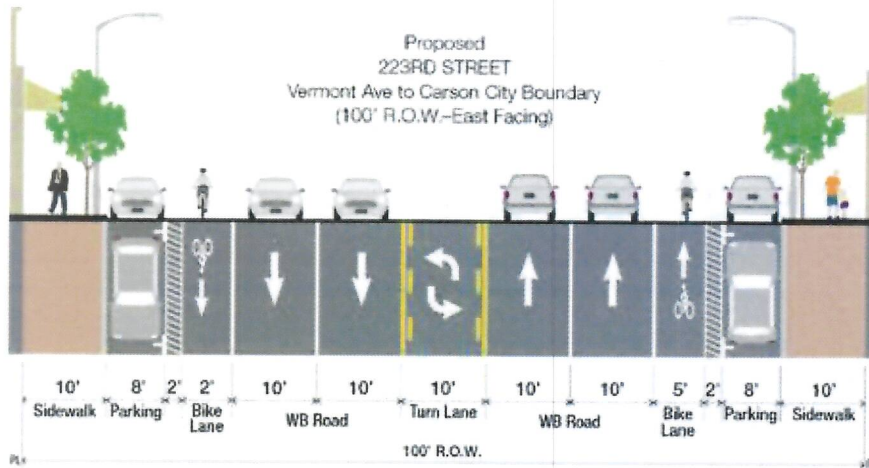
223rd Street is classified as a Secondary Highway under the County's Highway Master Plan. Proposed streetscape improvements for the 82-foot right-of-way segment of the corridor between Normandie Avenue and Vermont Avenue are illustrated in Figure 4.9. These improvements include incorporating a Class III bicycle facility along the roadway to improve connectivity with the greater regional bikeway network. The bicycle facility will incorporate green-backed sharrows to provide high visibility and send a strong signal to bicyclists as to where they should ride as well as to help alert motorists of their presence. Examples of green-backed sharrows are provided in Figure 4.8. Streetscape improvements also include improved landscaping and street trees.

Streetscape improvements for the 100-foot right-of-way segment of the corridor between Vermont Avenue and the city boundary for the City of Carson are illustrated in Figure 4.10. These improvements include reduced travel lane widths, the provision of on-street parking along the westbound portion of the corridor, and the introduction of a Class II bicycle facility in each direction with striped buffers. These proposed improvements are intended to support the Specific Plan's vision of transforming the existing auto-oriented design of 223rd Street to a more multi-modal streetscape.

Figure 4.9 Proposed 223Rd Street 82'-Row Streetscape Improvements



Figure 4.10 Proposed 223rd Street 100'-Row Streetscape Improvements



220th Street

220th Street serves as a pathway collector and provides connections to the Vermont Avenue and Figueroa Street pathway arterials. As a pathway collector, 220th Street is intended to support crossing movements and moderate levels of activity for active transportation users. Streetscape improvements to the segment of 220th Street adjacent to the Harbor-UCLA Medical Center are illustrated in Figure 4.11. Proposed improvements include relocating the exterior fence surrounding the medical center behind the existing culvert and covering the culvert to support an expanded pedestrian pathway and landscaping. Improvements to the remaining segment of 220th Street are illustrated in Figure 4.12 and include Class III bicycle facilities, street trees, lighting, and wider sidewalks.

In addition to the proposed streetscape improvements, the Specific Plan recommends improvements to the existing 220th Street pedestrian bridge. The pedestrian bridge crosses over the Interstate 110 freeway and is accessed via a walkway at 220th Street. The existing bridge has low levels of visibility and poor lighting conditions, which adversely impacts safety and contributes to the bridge's low levels of utility. The Specific Plan proposes to improve safety at the bridge by incorporating overhead lighting as well as lighting along the proposed handrails. The Plan also focuses on enhancing the visual aesthetics of the bridge by introducing a painted walkway, painted support poles, and a mural to help deter and camouflage graffiti. These proposed improvements are illustrated in Figure 4.13.

Figure 4.11: Proposed 220th Street Streetscape Improvements (Harbor-UCLA Medical Center)

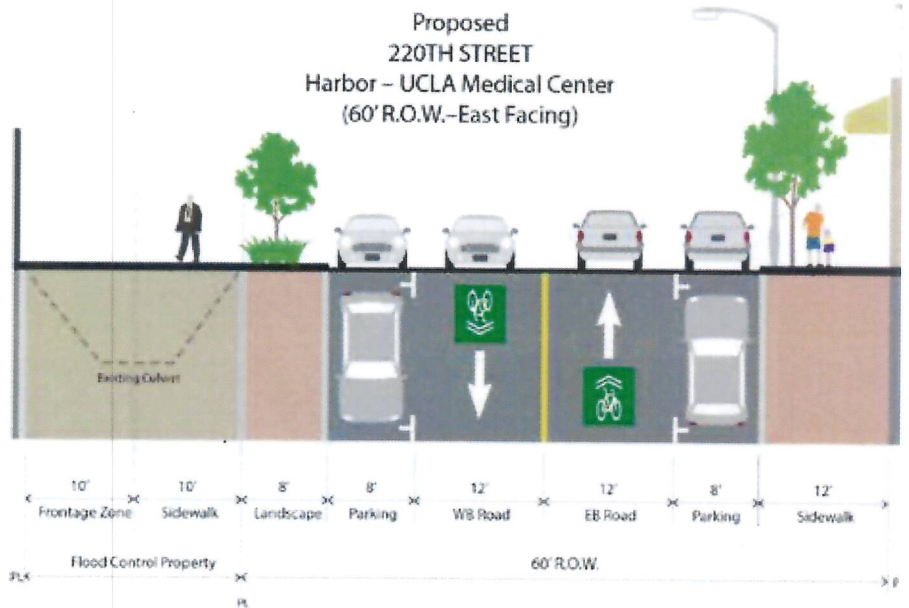


Figure 4.12: Proposed 220th Street Streetscape Improvements

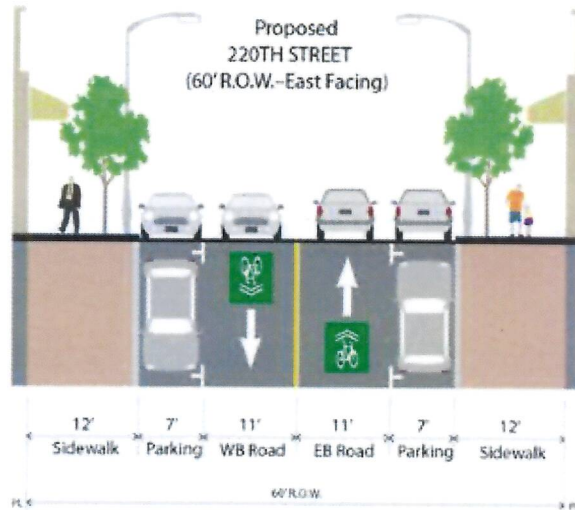


Figure 4.13: Proposed 220th Street Pedestrian Bridge Improvements

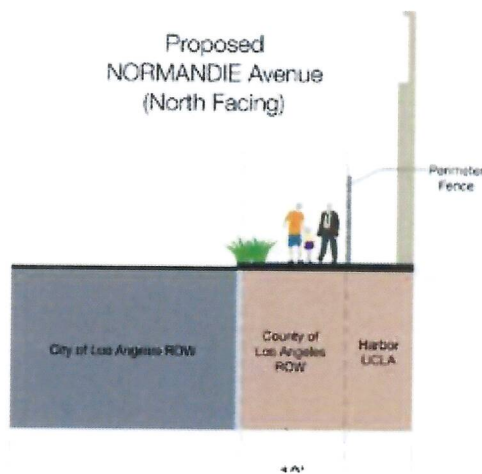


Before and after renderings of the 220th Street pedestrian bridge.

Normandie Avenue

Similar to Vermont Avenue and Carson Street, Normandie Avenue also serves as a pathway arterial connecting the Carson Street transit stop. Due to right-of-way constraints, only the pedestrian sidewalk along the east side of Normandie Avenue lies within the County's jurisdiction. The remaining section lies within the City of Los Angeles' right-of-way. Streetscape improvements to Normandie Avenue include improved landscaping to enhance the pedestrian environment as illustrated in Figure 4.14.

Figure 4.14: Proposed Normandie Avenue Streetscape Improvements



PUBLIC REALM DESIGN AND PARK STRATEGIES

One of the most important components of place-making is thoughtful urban design that employs buildings, open space, and landscaping to create engaging and comfortable outdoor places for residents, visitors, and workers of an area. Places such as sidewalks and parkways, multi-purpose trails, parks, plazas, and squares should be linked to each other and to the larger community. This interconnected pattern creates a range of valuable venues that accommodate a full spectrum of urban, commercial, and family-oriented activities.

This section identifies recommendations for change in the public realm, including pedestrian circulation, new small park opportunities, a landscape plan for major roads, and the creation of a transit plaza for riders using Metro's Carson Station. It is important to note that as the Harbor-UCLA Medical Center Master Plan gets implemented overtime, there will be significant improvements in terms of landscaping, fencing, pedestrian paths, lighting, seating, and public art that will significantly upgrade the public realm along the edges of the campus. The landscape design plan for this Specific Plan is intended to be integrated with and complement the medical campus.

The recommended strategies can improve the public realm and park network by utilizing public land/streets and pedestrian connections to bring park and open space amenities within reasonable walking distance for residents. Key components of this strategy include:

- The creation of new open space amenities in tandem with new development. The development regulations for this Specific Plan include detailed requirements for the provision and design of usable openspace.
- Requiring new development to have an engaging relationship to public streets, with requirements for how building entrances are designed, front setbacks are designed, and requiring parking to be located to the rear or side of buildings.
- Maximizing visibility and promoting safety of existing and new plaza areas, transit stops, pedestrian bridges, and park areas.
- Transforming vacant or publicly-owned lots, drainage channels, and cul-de-sacs into pocket parks and pathways.
- Improving the sidewalk and parkway systems on the arterial roadways, with the addition of street trees and parkway plantings, street furniture, enhanced street crossings, bus stop improvements, and pedestrian lighting where needed.

- Enhancing pedestrian crossings with improved marked crosswalks, providing pedestrian safety islands and curb extensions where feasible, and other techniques.

PEDESTRIAN ENVIRONMENT

Sidewalk Hierarchy

The quality of the pedestrian environment often impacts a person's decision to walk or drive to an area. Adequate pedestrian infrastructure, together with appropriate land use designations, can help to activate corridors and promote pedestrian activity. This section discusses some of the existing opportunities to improve the pedestrian environment within West Carson and to reinforce the area as a TOD district.

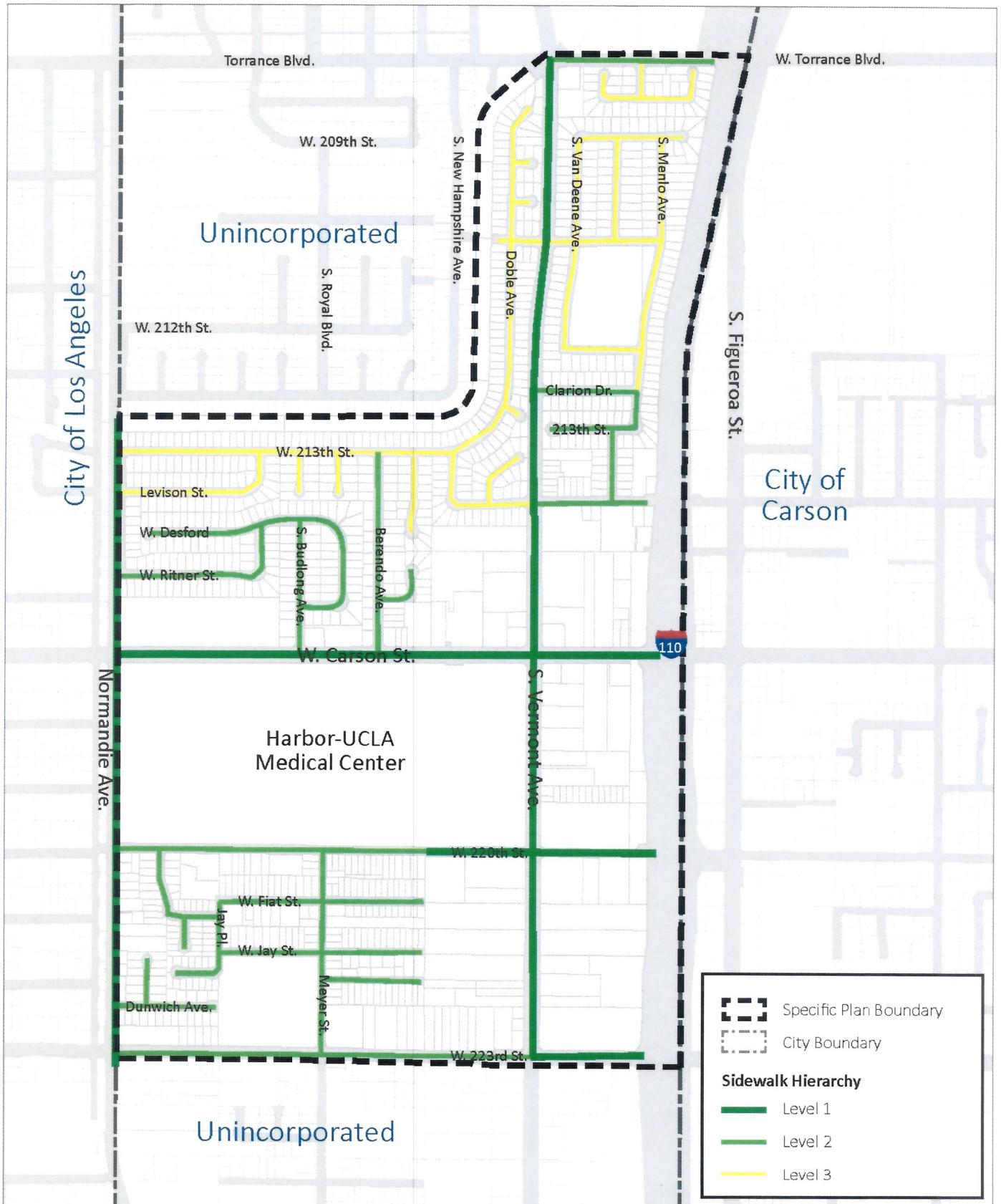
Sidewalks shall be recognized not as a pedestrian amenity, but as the back bone of West Carson's pedestrian network. Sidewalks present inventive opportunities to transform streets into public spaces.

Although sidewalks exist along major streets within the Specific Plan area, most sidewalks are narrow in width and do not support high levels of pedestrian activity. The Specific Plan proposes a sidewalk hierarchy to establish a physical framework for sidewalk design. The sidewalk hierarchy is intended to facilitate the most appropriate allocation of space that encourages people to walk as a part of their everyday routine.

The sidewalk hierarchy is composed of three levels: Level 1, Level 2, and Level 3 as shown in Figure 4.15 Pedestrian Network Map. Each level varies in its allocation of space amongst the frontage zone, pedestrian zone, furniture zone, and curb zone, which are defined as follows:

- **Frontage Zone:** The frontage zone encompasses the area of the sidewalk that separates pedestrians from the property line or building/store fronts. The frontages zone typically provides space for outdoor seating, store entrances, street vendors, and provides a buffer for pedestrians from opening doors and other architectural elements. There are a variety of frontage types that define how buildings are accessed and how private property interfaces with the sidewalk. Requirements for frontage types on key streets within the Specific Plan area are included in Chapter 3.

Figure 4.15 Pedestrian Network Map



- **Pedestrian Zone:** The pedestrian zone is the area of the sidewalk that is specifically reserved for pedestrian travel. It should be free of obstacles, well-lit, and functional in all weather conditions. Street furniture, plantings, outdoor seating, utility boxes, and other elements should not protrude into the pedestrian zone. Additionally, the surface quality of the pedestrian zone is of key importance and should be smooth, stable, and slip resistant, with minimal gaps and rough surfaces.
- **Furniture Zone:** The furniture zone is the area of the sidewalk between the pedestrian zone and the street curb. The furniture zone provides space for utilities, such as traffic poles and fire hydrants, as well as amenities, such as benches, bus shelters, and street trees. Items placed in this zone shall be strategically located so not to obstruct sight lines, prevent damage from vehicles on the street, and to allow for access to and from parked cars.
- **Curb Zone:** The curb zone is the first six inches of sidewalk area immediately adjacent to the roadway. The curb zone discourages motor vehicles from driving onto the sidewalk, prevents excess water from collecting onto the sidewalk, and provides a valuable cue that separates the sidewalk from the roadway and vehicular traffic.

Each level within the hierarchy is designed to support varying levels of pedestrian activity. Locations of each level corresponds with the locations of the various pathways in the Metro Pathways hierarchy. For instance, Level 1 sidewalks, which are the widest sidewalks, should be located along pathway arterials to accommodate high levels of pedestrian activity. Level 2 sidewalks should be located along pathway collectors to accommodate moderate levels of pedestrian activity, while Level 3 sidewalks should be reserved for low-density residential streets. The various levels of the sidewalk hierarchy are defined as follows:

- **Level 1:** Level 1 sidewalks are the widest sidewalks within the hierarchy and should have a minimum width of 10-feet to support high pedestrian volumes and to accommodate the street trees, benches, outdoor seating, and other amenities. Level 1 sidewalks should be located along pathway arterials and areas with higher density, mixed-use, or commercial land use development. Space allocation for Level 1 sidewalks should favor the pedestrian and frontage zone to provide adequate passing space between pedestrians and to accommodate for store-front amenities such as outdoor seating within commercial or mixed-use zones. Examples of a Level 1 sidewalk can be seen in the Vermont Avenue and Carson Street streetscape improvements illustrated in Figures 4.6 and 4.7.

- **Level 2:** Level 2 sidewalks are slightly narrower in width than Level 1 sidewalks and should have a minimum width of seven-feet to accommodate moderate levels of pedestrian activity and to accommodate some pedestrian amenities. Level 2 sidewalks should be located along pathway collectors. Space allocation for Level 2 sidewalks should favor the pedestrian and furniture zone. Examples of a Level 2 sidewalk can be seen in the 82-foot right-of-way streetscape improvement for 223rd Street as illustrated in Figure 4.9.
- **Level 3:** Level 3 sidewalks are the narrowest sidewalks within the hierarchy and should be located along low-density residential streets that do not carry high volumes of traffic or pedestrian activity. They should have a minimum width of five-feet to meet ADA standards and should favor the pedestrian zone.

Figure 4.15 illustrates the suggested locations of the various sidewalk levels. The Specific Plan, however, recognizes that sidewalk design and construction often occurs under constrained conditions, such as narrow rights-of-way, utilities, grading, and topography. These are all key factors to consider when designing and constructing accessible sidewalks.

Figure 4.16 Pedestrian Crossing Map



Pedestrian Crossings

A safe and comprehensive pedestrian network requires the ability to complete two important functions: walking along streets and crossing streets safely. Several tools exist to enhance the overall pedestrian experience and to help make crossing streets easier and safer for pedestrians. They include the installation of crosswalks at intersections and mid-block, curb ramps, pedestrian crossing signs, signals, and more. Figure 4.16 illustrates existing and proposed locations for pedestrian crosswalks. In addition, the Specific Plan recommends the following design guidelines to facilitate safe pedestrian crossing:

Marked Crosswalks: Marked crosswalks are a key element in providing safe pedestrian crossings. Crosswalks help to guide pedestrians and identify locations where it is safe to cross, as well as inform drivers of pedestrian movements. Crosswalks can be located at intersections, mid-block crossings, or uncontrolled crossings. Crosswalks should meet basic requirements for visibility and should follow guidelines set forth in the Federal Highway Administration (FHWA) Manual on Uniform Traffic Control Devices (MUTCD). The Specific Plan recommends that crosswalks be in place at the following locations whenever possible:

1. All signalized intersections
2. Near key transit stops and stations
3. Locations with heavy pedestrian volumes (such as along Vermont Avenue)
4. Along school walking routes (such as along Meyler Street)
5. Trail crossings

Pedestrian Safety Islands: Pedestrian safety islands provide pedestrians refuge and reduce the exposure time experienced at crossing wide intersections and should be installed at locations where pedestrians are required to cross three or more lanes of traffic in one direction, such as along Carson Street and Vermont Avenue.

Curb Extensions: Curb extensions are traffic calming treatments that narrow the roadway to create safer and shorter crossing distance for pedestrians. They also help to improve the overall visibility of pedestrians by placing them in alignment with on-street parking. They should be placed on streets with high pedestrian volumes or along wide streets that are difficult to cross.

Curb Ramps: Curb ramps are critical features to improve accessibility pedestrians with mobility limitations and visual impairments. They should be installed at all crosswalks to comply with ADA requirements.



Example photo illustrating signage used to indicate a pedestrian crossing.

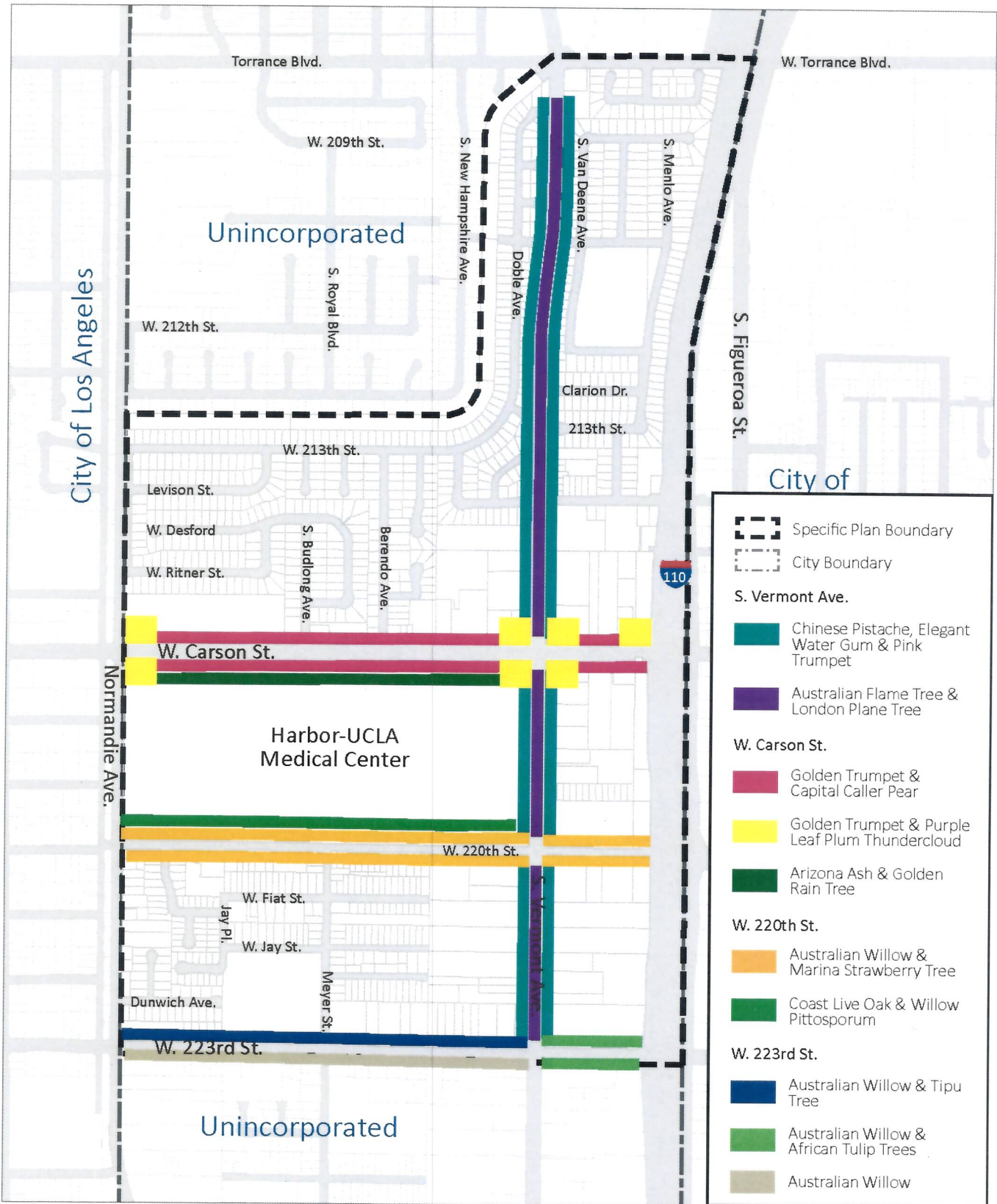
Pedestrian Signage: Pedestrian crossing signage is used to alert motorists of the presence of pedestrians along roadways. They should be installed at uncontrolled crossings and near key destinations, such as Meyler Elementary School and Van Deene Avenue Elementary School to alert motorists in advance.

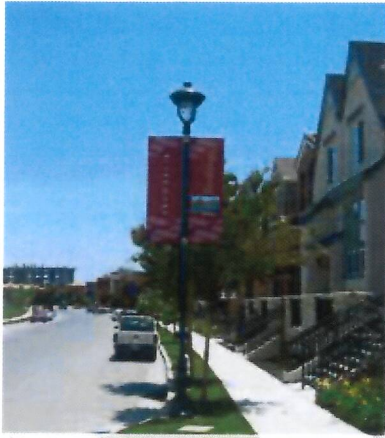
Streetscape Design

Streets with comfortable sidewalks and planted parkways provide the unifying structure of the plan area. Pedestrian friendly sidewalks should incorporate amenities that are attractive, pleasant, offer visual stimulation, promote activity. The Specific Plan recommends the following design guidelines to guide the installation of streetscape amenities:

- 1. Street Trees:** Street trees serve a variety of urban design functions such as acting as a pedestrian buffer, accentuating spaces, creating a sense of enclosure, improving air quality, reduction of heat island effect by providing shade and filtered light, and improving visual aesthetics along a corridor. Street trees should be incorporated whenever possible, especially along pathway arterials. All street trees should be planted in accordance with established County planting standards.
- 2. Seating:** Seating provides reprieve for pedestrians and provides a place to rest or wait. Providing comfortable places to sit can transform a sidewalk into a gathering place for social activity. Pedestrian seating and benches should be installed when feasible and should adhere to ADA standards and should not obstruct pedestrian pathways.
- 3. Street Lights:** Street lights provide a sense of safety and security for pedestrians and waiting transit patrons, as well as facilitate the safe movement of vehicular traffic. Appropriate levels of street lighting should be installed to provide safe, consistent lighting along a corridor, while reducing energy consumption and costs. The addition of pedestrian lighting on Carson Street and segments of Vermont Avenue within the Specific Plan area is highly recommended to increase safety.
- 4. Public Art:** Public art features help provide visual stimulation and improve visual aesthetics along a corridor. They can help unify an area or district or help to identify a neighborhood gateway. Care should be given to the installation of public art to ensure that they do not obstruct pedestrian pathways and they adhere to ADA standards. The introduction of public art would be appropriate within the

Figure 4.17 Street Tree Plan





Example photo of public art used to brand a district.

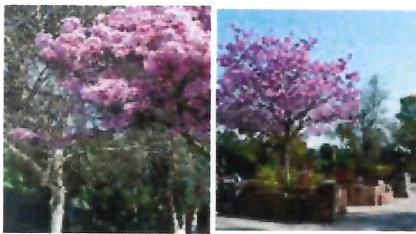
median at the intersection of Carson Street and the Interstate 110 off/on ramps. The art could be an extension of the art improvements planned for Harbor-UCLA Medical Center.

5. **Furnishings:** Furnishings such as seating and bike racks encourage pedestrian and bicycle activity by providing a sense of comfort and convenience. Pedestrian-oriented streets and hubs of high pedestrian activity should provide appropriate seating at regular intervals. Seating should be located in areas with shelter, shade, trees, and lighting and may be enhanced with seat walls, landscaping, fountains, and trellises, and other features. Bike racks should be placed in areas with ample space per bike and an adequate pedestrian clearance. Encourage bike rack design to enhance the identity of the community.

The streetscape design recommendations as shown in Figures 4.17 - 4.24 are focused on the major roadways: Carson Street, South Vermont Avenue, West 220th Street, and West 223rd street. There are currently very few street trees or planted parkways within the public rights-of-way. There are some trees that have been planted along front property lines that help to provide shade for pedestrians. In general, a consistent, well-designed landscape plan for the streets can provide shade, introduce seasonal color, define the street edge, and invite pedestrian activity. The following landscape and planting design recommendations are based on Public Works' Tree Selection Catalog, Los Angeles County's Drought Tolerant Plant List, and Harbor UCLA's Master Plan Landscape Design Guidelines. The landscape recommendations in this chapter are conceptual only. When the County considers such improvements, these recommendations will be further evaluated and supplemented on a case-by-case basis.

- Major streets will be composed signature plantings from the plant palette to create clearly defined identities unique to each street to improve wayfinding and announce arrival into West Carson.
- Formal plantings will be arranged along parkways and landscape easements at intervals appropriate with street scale and canopy cover to provide a sense of rhythm and movement within the streetscape.
- Flowering trees and trees with seasonal color have been selected to provide a dynamic color palette, the use of color is key for creating an active landscape which denotes the passing of time and creates visual interest.

- The plant palette incorporates deciduous plant material to provide shade canopies during the warm season while allowing penetration of sunlight during the cooler months to provide a more comfortable and inviting atmosphere.
- Incorporate suspended pavement system(s) improving soil volume to support larger tree species development and improve plant health, promoting higher plant success rates. Additionally, these systems provide stormwater quality management through capture, evapotranspiration and storage.
- The placement of trees and portions of planted parkways will provide greater separation for pedestrians from the vehicle traffic, promoting the use of sidewalks by creating a more comfortable experience for pedestrians.



Pink Trumpet Tree



Australian Flame Tree



London Plane Tree



Tipu Tree



Golden Rain Tree



Golden Trumpet Tree



Purple Leaf Plum Thundercloud



New Zealand Christmas Tree



Arizona Ash



Elegant Water Gum



Coast Live Oak



Marina Strawberry Tree



African Tulip Tree



Australian Willow



Desert Willow



Chinese Pistache

Figure 4.18 Street Trees: Carson Street

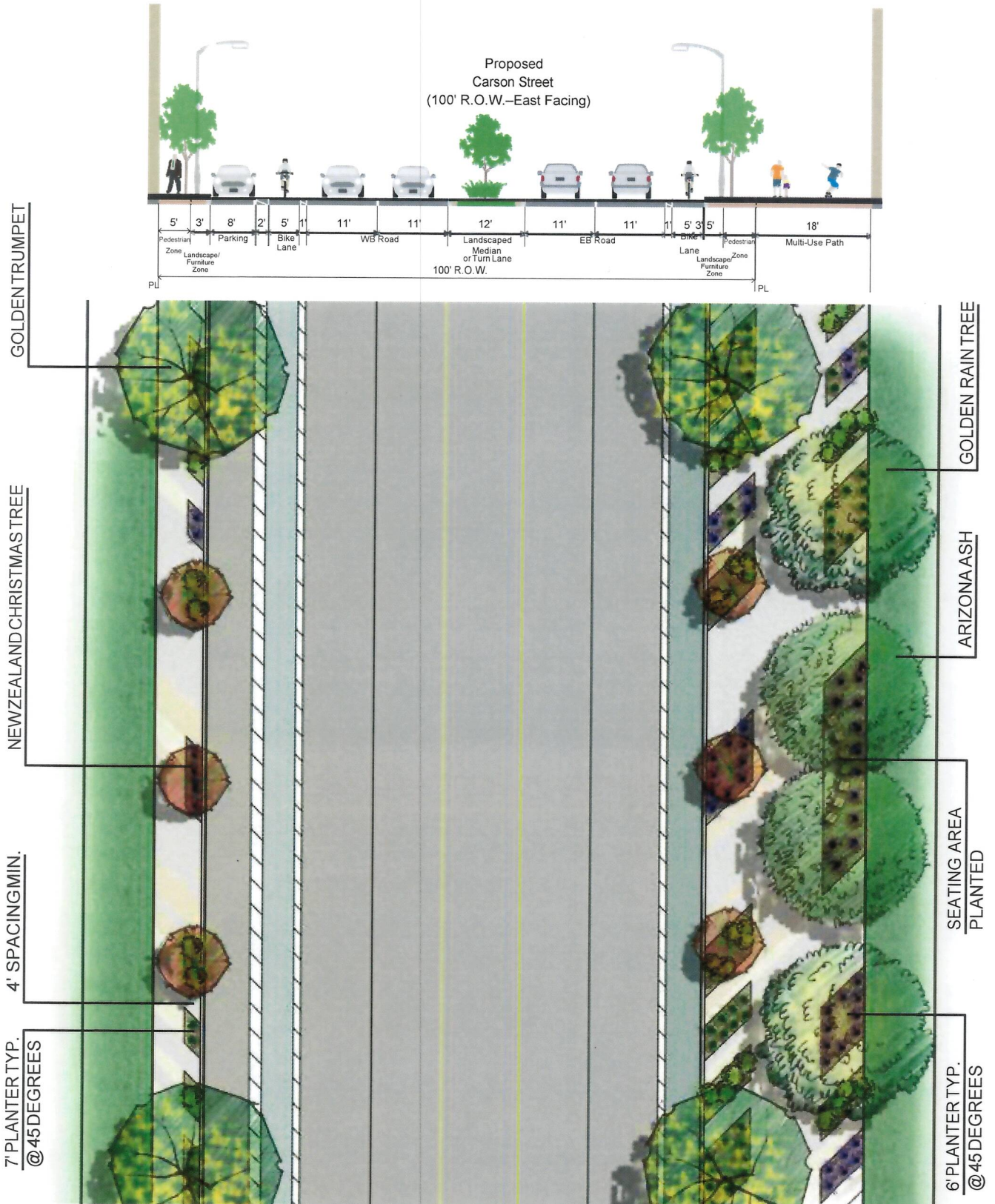


Figure 4.19 Street Trees: Vermont Avenue

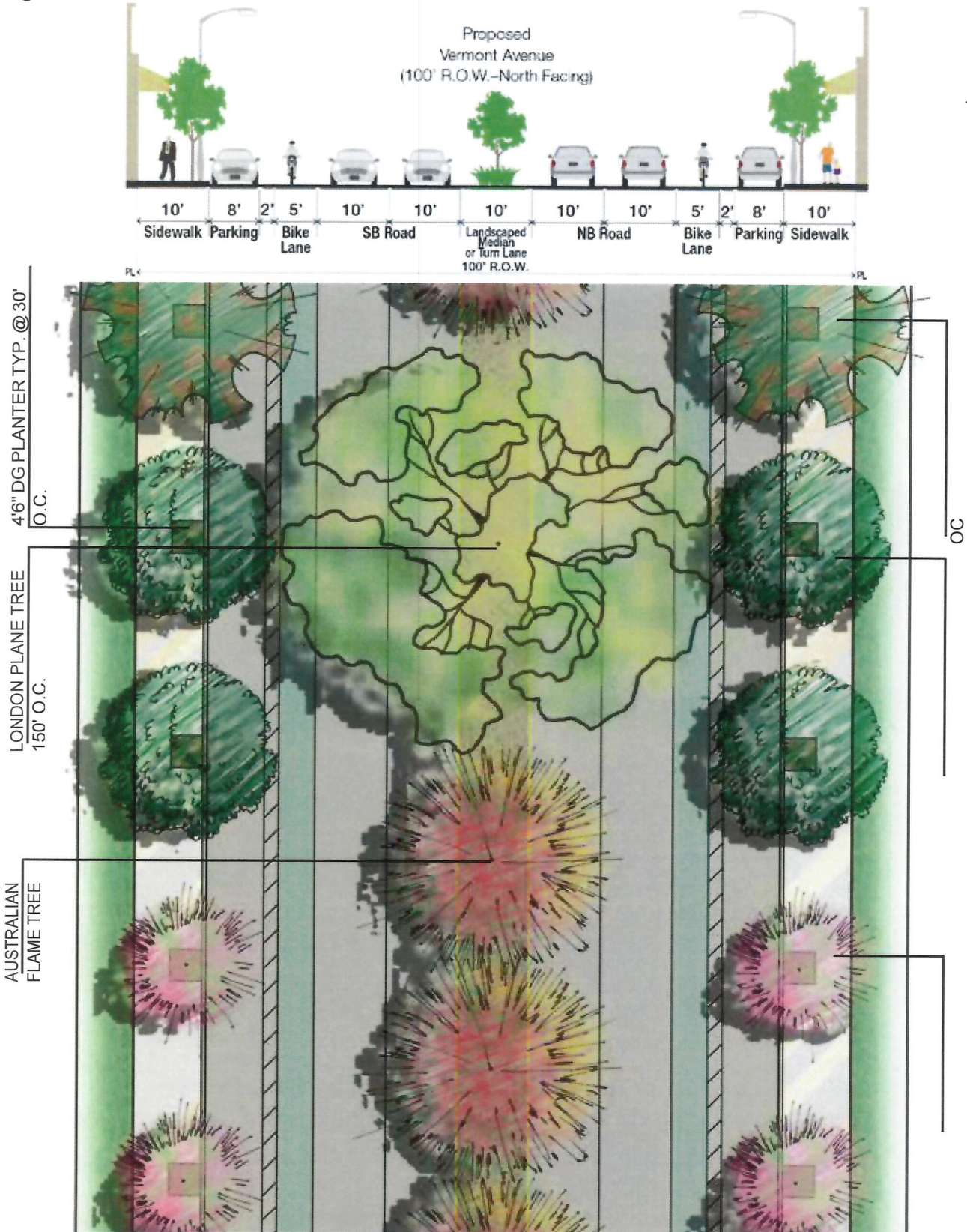


Figure 4.20 Street Trees: 220th Street (West of Vermont Avenue)

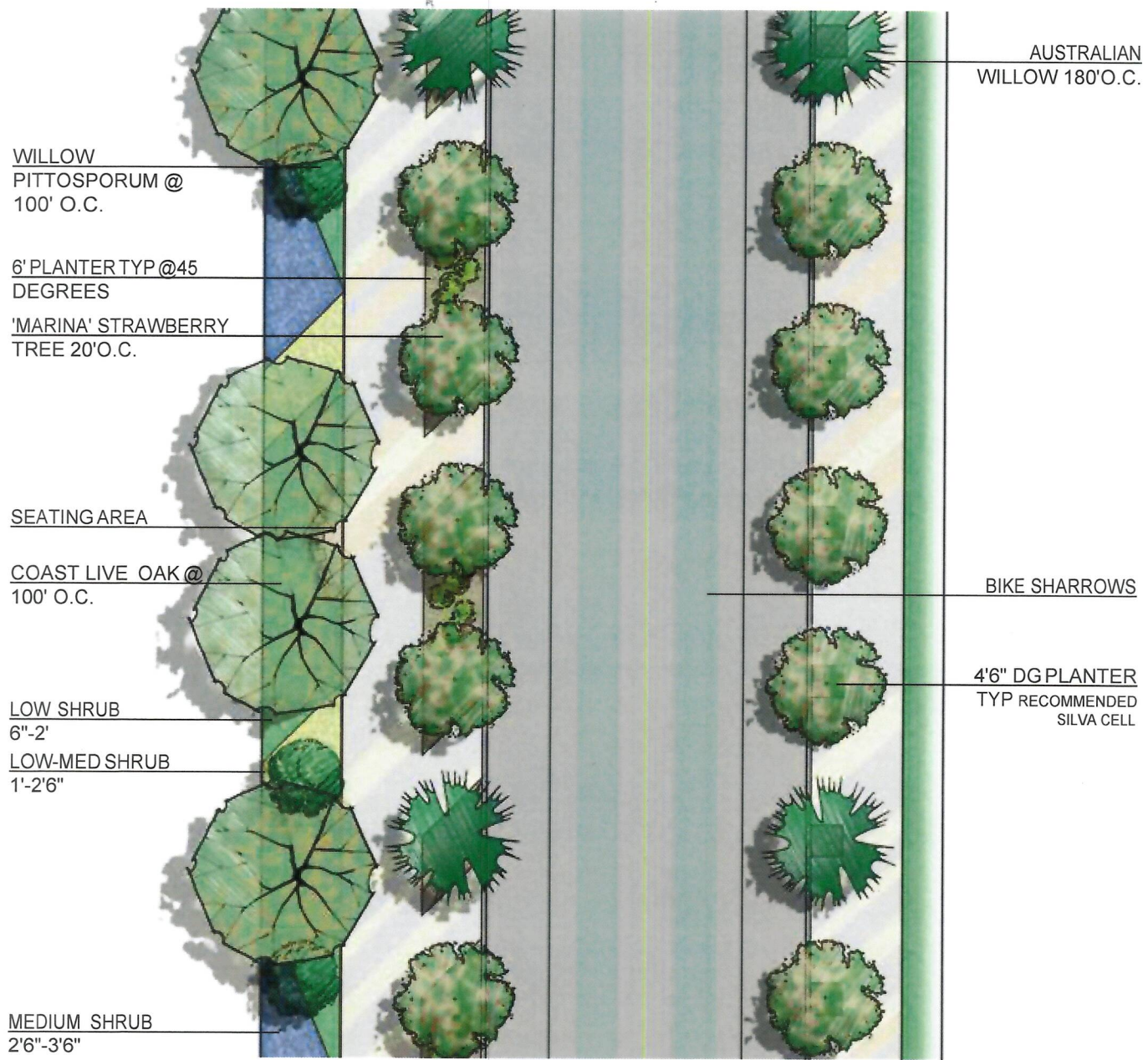
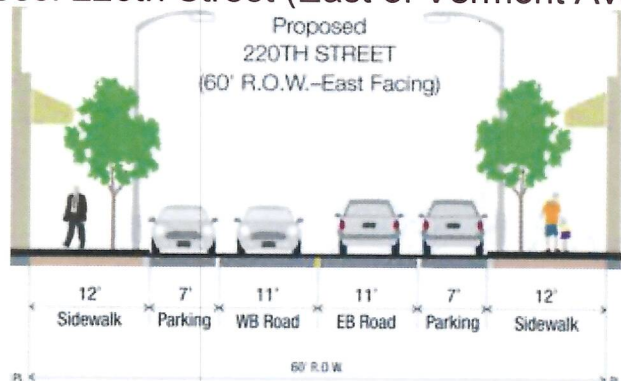
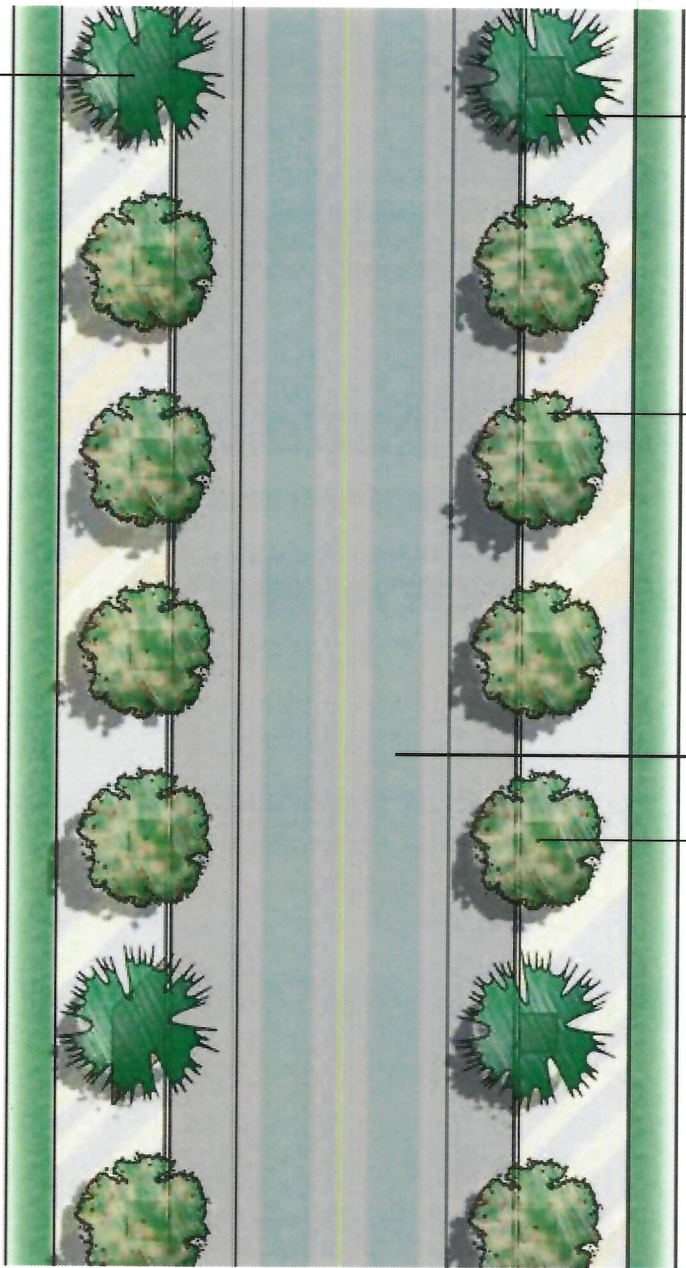


Figure 4.21 Street Trees: 220th Street (East of Vermont Avenue)



6' PLANTER TYP @ 5 DEGREES



AUSTRALIAN WILLOW
180'O.C.

'MARINA' STRAWBERRY
TREE 40'O.C.

BIKE SHARROWS

4'6" DG PLANTER TYP
RECOMMENDED SILVA CELL



Figure 4.22 Street Trees: 223rd Street (West of Vermont Avenue)

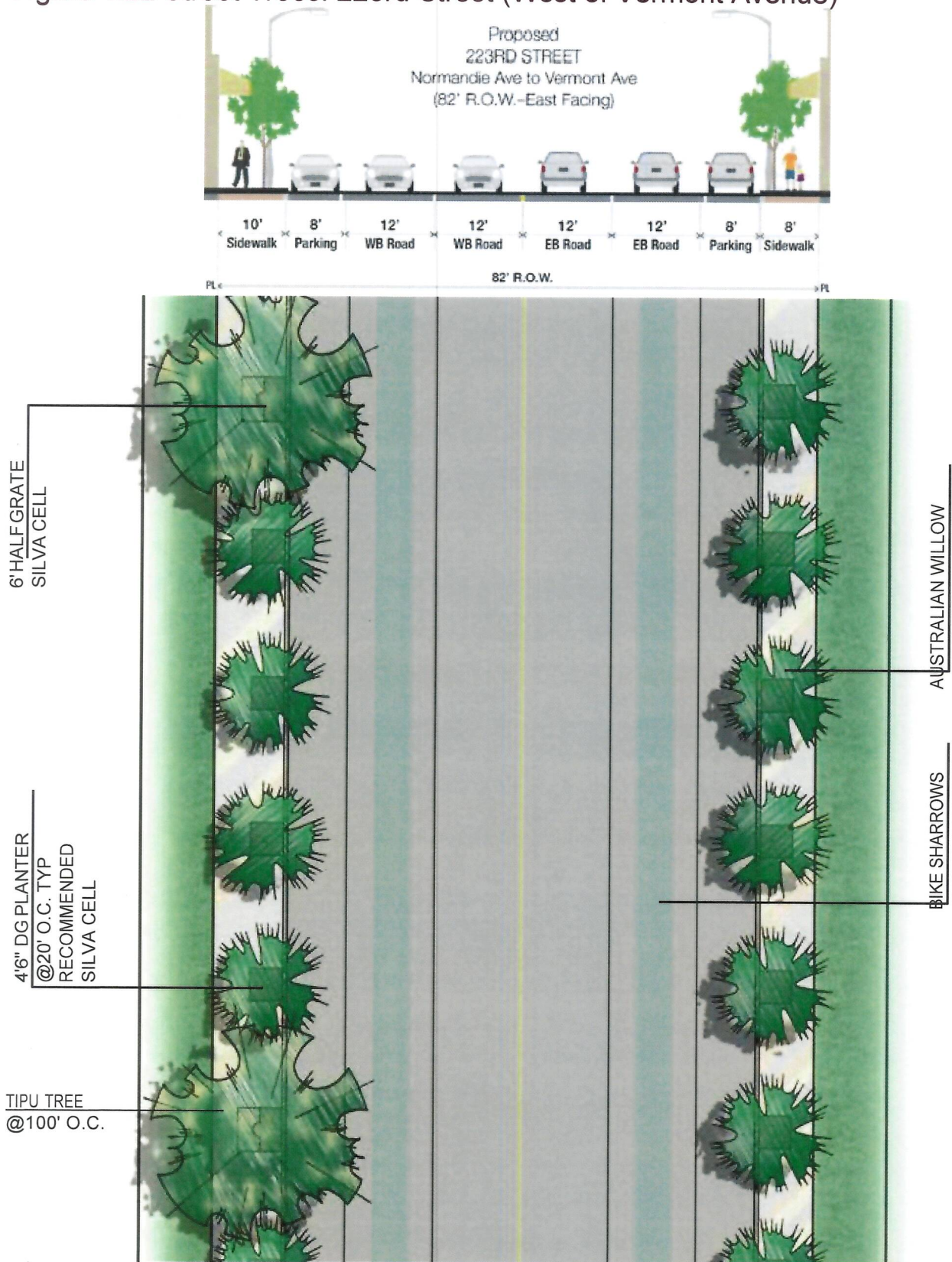


Figure 4.23 Street Trees: 223rd Street (East of Vermont Avenue)

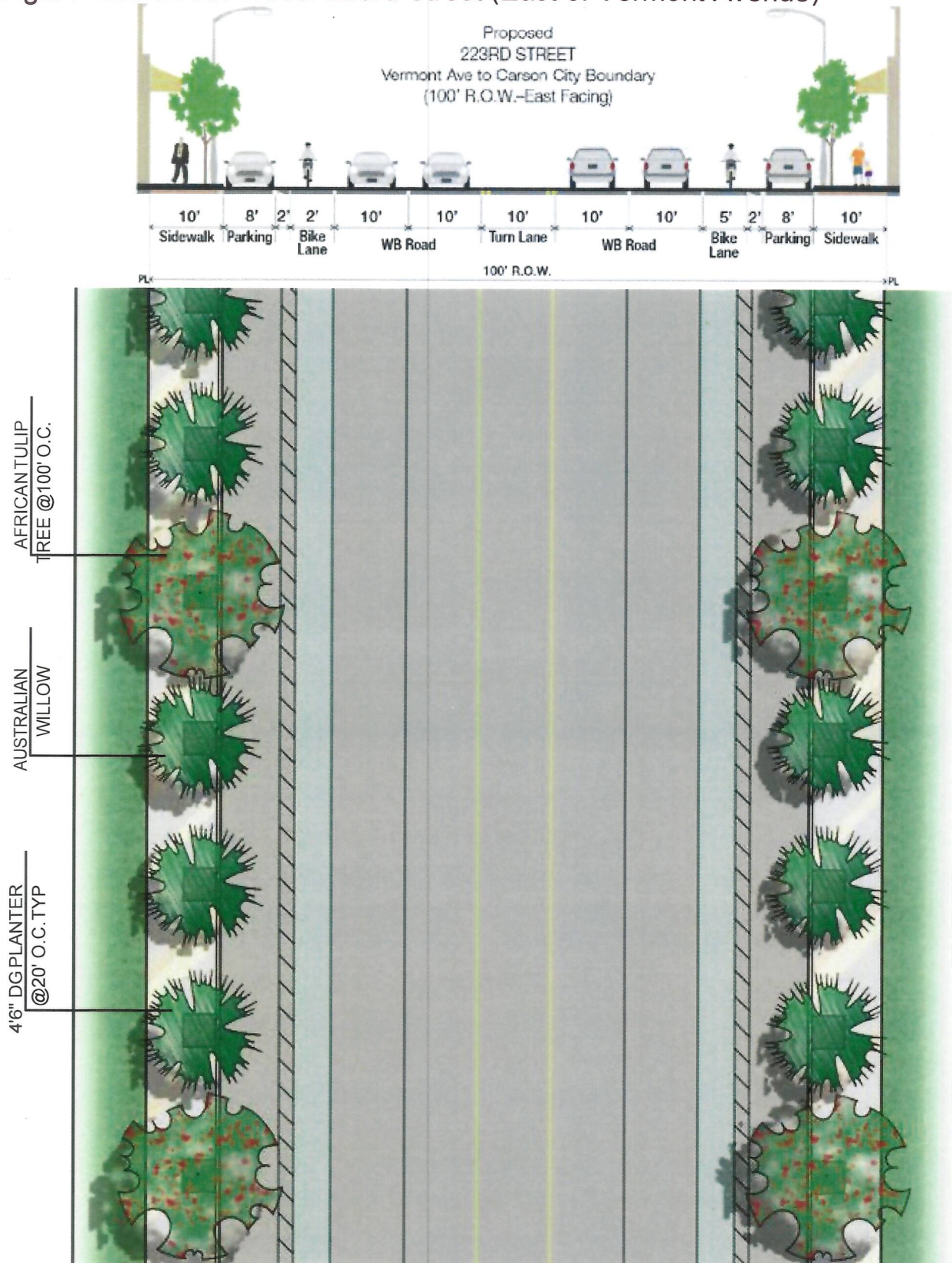
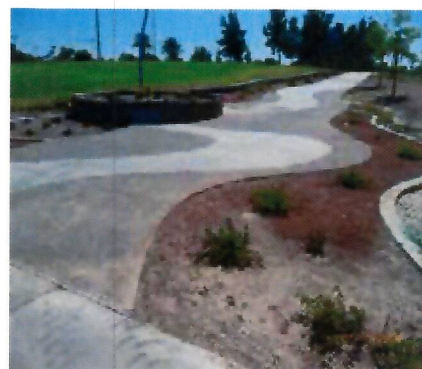
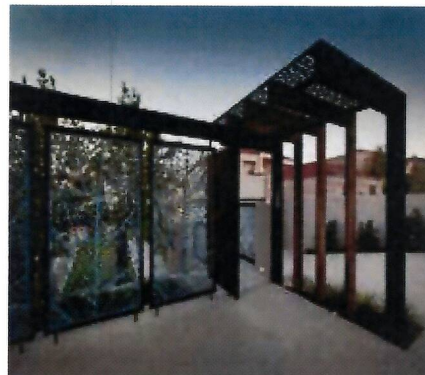
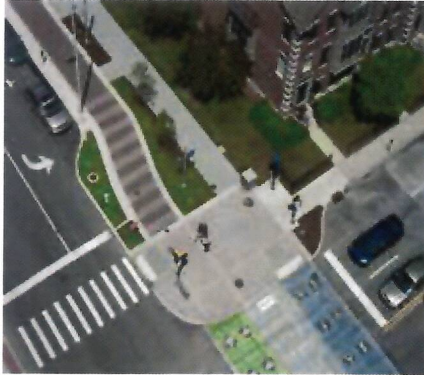


Figure 4.24 Design Recommendations



NEW PARK OPPORTUNITIES

Within the entire unincorporated West Carson area, there is an average of about 0.4 park acres per 1,000 residents, which is well below the County average of 3.3 park acres per 1,000 people. In addition, only 24 percent of people in West Carson live within walking distance (half-mile) of a park, as opposed to the County's average of 49 percent. While the majority of Specific Plan area residents have a high need for parkland and park amenities, there is almost no vacant publicly-owned land available to develop new parks.

Within the Specific Plan area, half an acre of parkland is available for recreational and public use at the Park Learning Grove County Park. This park is a small public green space with no amenities, and is used as a joint-use facility with Meyler Street Elementary School. Park amenities, including basketball courts and playground equipment, are available at another joint use park facility located in the Specific Plan area at Van Deene Avenue Elementary School, but does not include any green or open space. Beyond the Specific Plan area, the nearest park facilities can be found at the Normandale Recreation Center in the City of Los Angeles, the Veterans Sports Complex in Carson, Carson Park in Carson, and joint-use facilities at White Middle School.

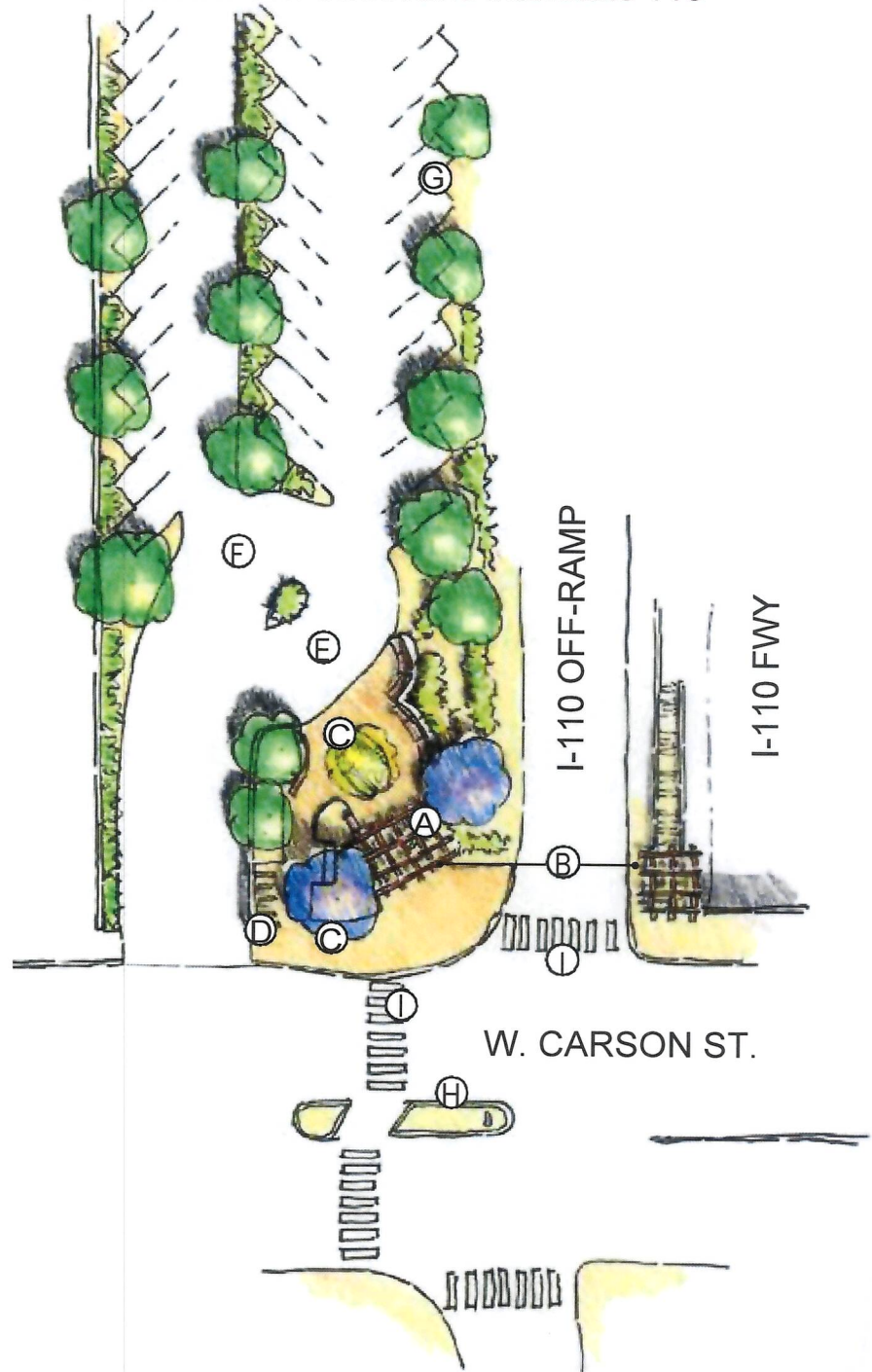
Figures 4.25 - 4.31 identify potential locations for the creation of pocket parks by converting cul-de-sacs, partially covering a drainage channel, and ultimately reclaiming property that will no longer be needed by Harbor-UCLA Medical Center. Each of these concepts shows the potential for passive and active recreation. The ultimate design and programming of these spaces should be done in conjunction with the neighborhood in which they are located. Pocket parks can provide socialization opportunities for a wide variety of age groups.

Figure 4.25 New Park Opportunities



Figure 4.26 Pocket Park Concept at the Caltrans Park-n-Ride lot at Carson Street and Interstate 110

- Ⓐ Pergola or other type of overhead structure to anchor corners and provide shade
- Ⓑ Real-time bus tracking kiosk
- Ⓒ Introduction of new W. Carson St. landscape palette
- Ⓓ Bike parking/sharing
- Ⓔ Benches against planterwalls
- Ⓕ Modified entry to parking lot and restriping
- Ⓖ Parking lot landscaping
- Ⓗ Entry monument or public art
- Ⓘ Enhanced cross-walk for safety



Opportunity

- Redesign Caltrans park-n-ride to include transit waiting plaza.
- Create a gateway to West Carson with landscape/hardscape elements that also bring visibility to the transit station.

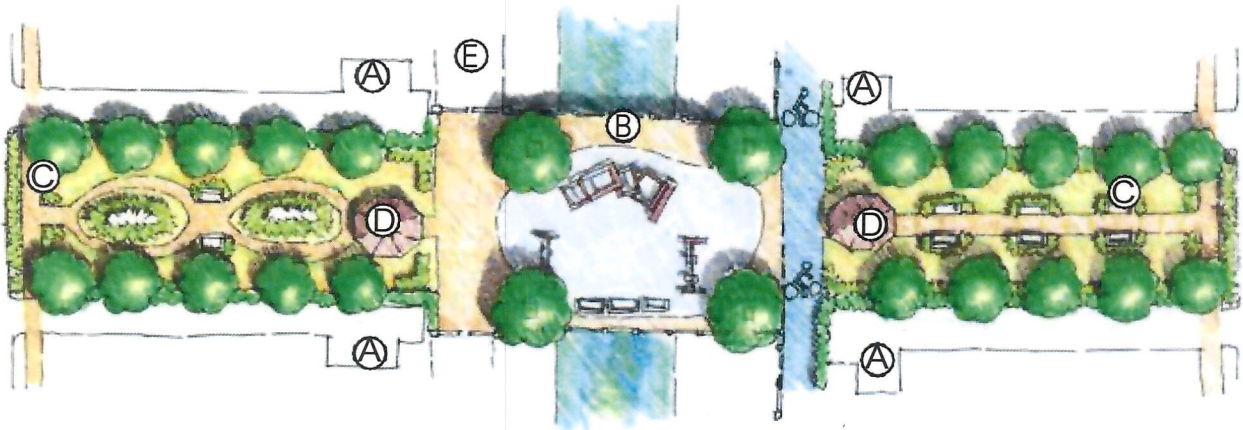
Figure 4.27 Pocket Park Concept at the East End of 220th Street



Opportunity

- To provide children and parents with a safer and inviting walking experience to White Middle School via a pedestrian bridge over the Interstate 110.
- To create a small pocket park within the cul-de-sac at 220th Street.
- To improve the pedestrian bridge with new fencing and lighting.

Figure 4.28 Pocket Park Concept at 208th Street and Javelin Street

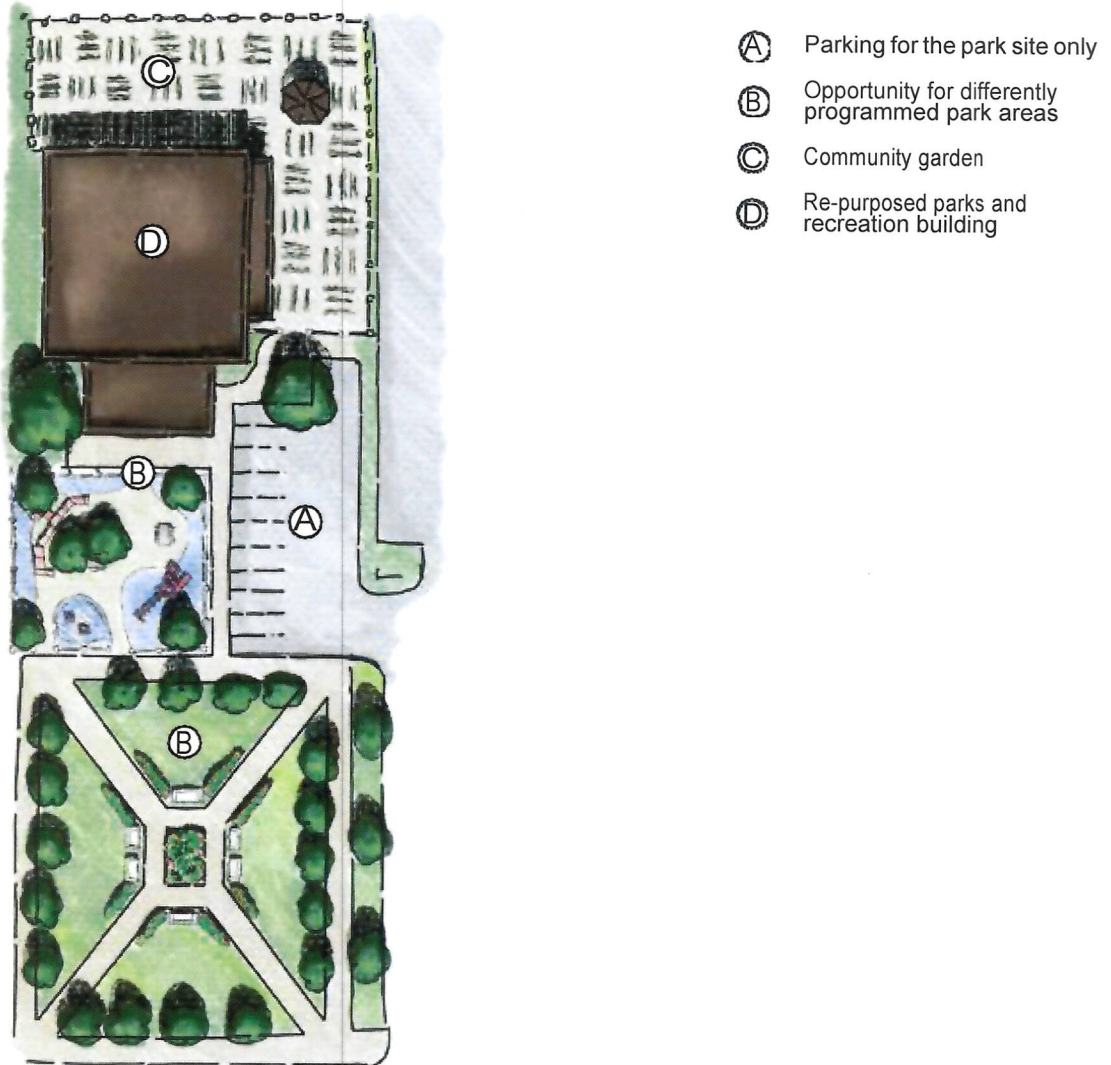


Opportunity

- To provide new park space to serve the single-family neighborhood north of Carson Street.
- Converts the two cul-de-sacs along Javelin St., connected by a channel cap for additional playground space.
- Includes bike path on one side of the channel and access road on other side.

- (A) New 10' drive to garage
- (B) Channel cap with hardscape, playground equipment and boxed trees for shade
- (C) Areas for unstructured play, seating, picnics
- (D) Shade structure with table
- (E) Existing maintenance road with gates at edge of park

Figure 4.29 Pocket Park Concept for the Harbor-UCLA
Kindercare Site (Option 1)



Opportunity

- To repurpose the Harbor-UCLA Medical Center Kindercare site, if and when this use relocates to the main hospital campus.
- To provide new park and recreation opportunities accessible from Carson Street and the residential neighborhoods to the north.
- To repurpose the Kindercare building as the West Carson Youth Center, with structured programs and events.
- Create a combined access drive to both the multi-family apartment project and the park at the signalized intersection.

Figure 4.30 Pocket Park Concept for the Harbor-UCLA
Kindercare Site (Option 2)

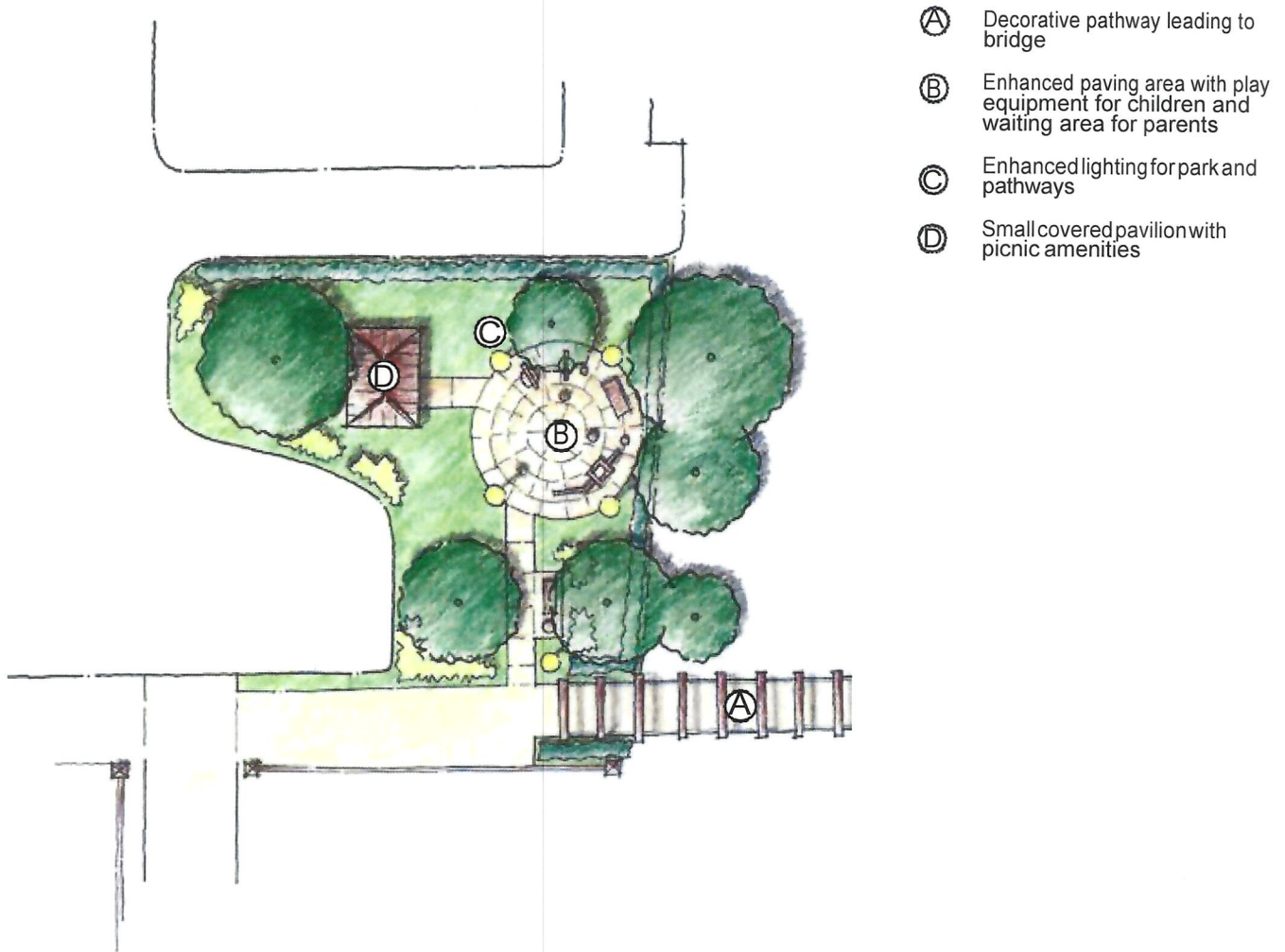
- Ⓐ Combined parking for the park and guests of the residential project
- Ⓑ Opportunity for differently programmed park areas
- Ⓒ Community garden
- Ⓓ Re-purposed parks and recreation building



Opportunity

- To repurpose the Harbor-UCLA Medical Center Kindercare site, if and when this use relocates to the main hospital campus.
- To provide new park and recreation opportunities accessible from Carson Street and the residential neighborhoods to the north.
- To repurpose the Kindercare building as the West Carson Youth Center, with structured programs and events.
- Create a combined access drive to both the multi-family apartment project and the park at the signalized intersection.

Figure 4.31 Pocket Park Concept for the East End of 214th Street



Opportunity

- To provide children and parents with a safer and inviting walking experience via a pedestrian bridge over the Interstate 110.
- To create a small pocket park within the cul-de-sac at 214th Street.
- To improve the pedestrian bridge with new fencing and lighting.

BICYCLE CIRCULATION

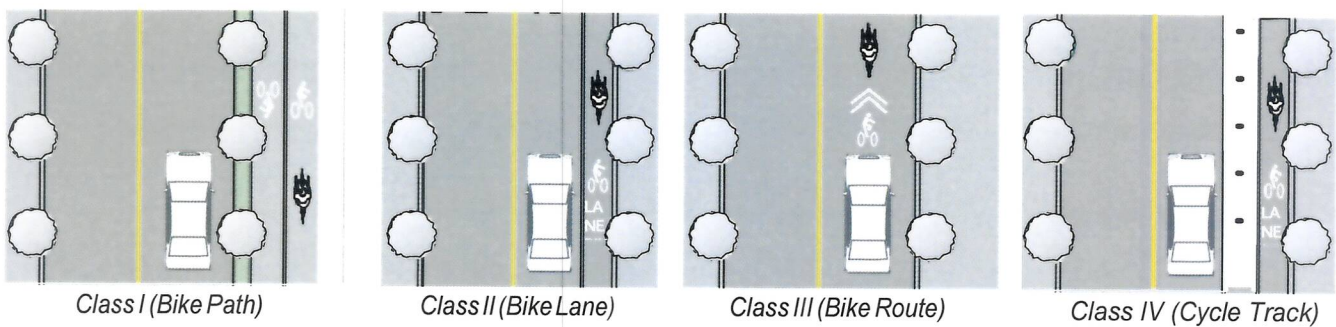
Bicycling provides a sustainable solution for traveling the first and last mile to and from a transit station. While transit and bicycling are complementary modes, issues of infrastructure connectivity often impacts a person's decision to bike the first and last mile of their journey from transit. The following section describes improvements to bicycle network to promote bicycling within the Specific Plan area.

Bicycle Facility Types

Bikeways are facilities that are designated primarily for bicycle travel. They are generally divided into three types: Class I, Class II, Class III, and Class IV.

- »» **Class I (Bike Path):** Provides a completely separated right-of-way (off-street) designated for the exclusive use of bicycles and pedestrians with crossflow traffic minimized.
- »» **Class II (Bike Lane):** Provides a restricted right-of-way (on-street) designated for the exclusive or semi-exclusive use of bicycles with through travel by motor vehicles or pedestrians prohibited, but with crossflows by pedestrians and motorists permitted. Vehicle parking can be allowed to the right of bike lane if sufficient right-of-way width exists.
- »» **Class III (Bike Route):** Provides for shared use with pedestrians or motor vehicles and is (on-street) designated by signs or permanent markings.
- »» **Class IV (Cycle Track):** Provides bikeways (cycle tracks) located in or adjacent to a roadway and separated from traffic by a physical barrier such as bollards, on-street parking, or planters. This design allows an exclusive right-of-way for bicycle travel.

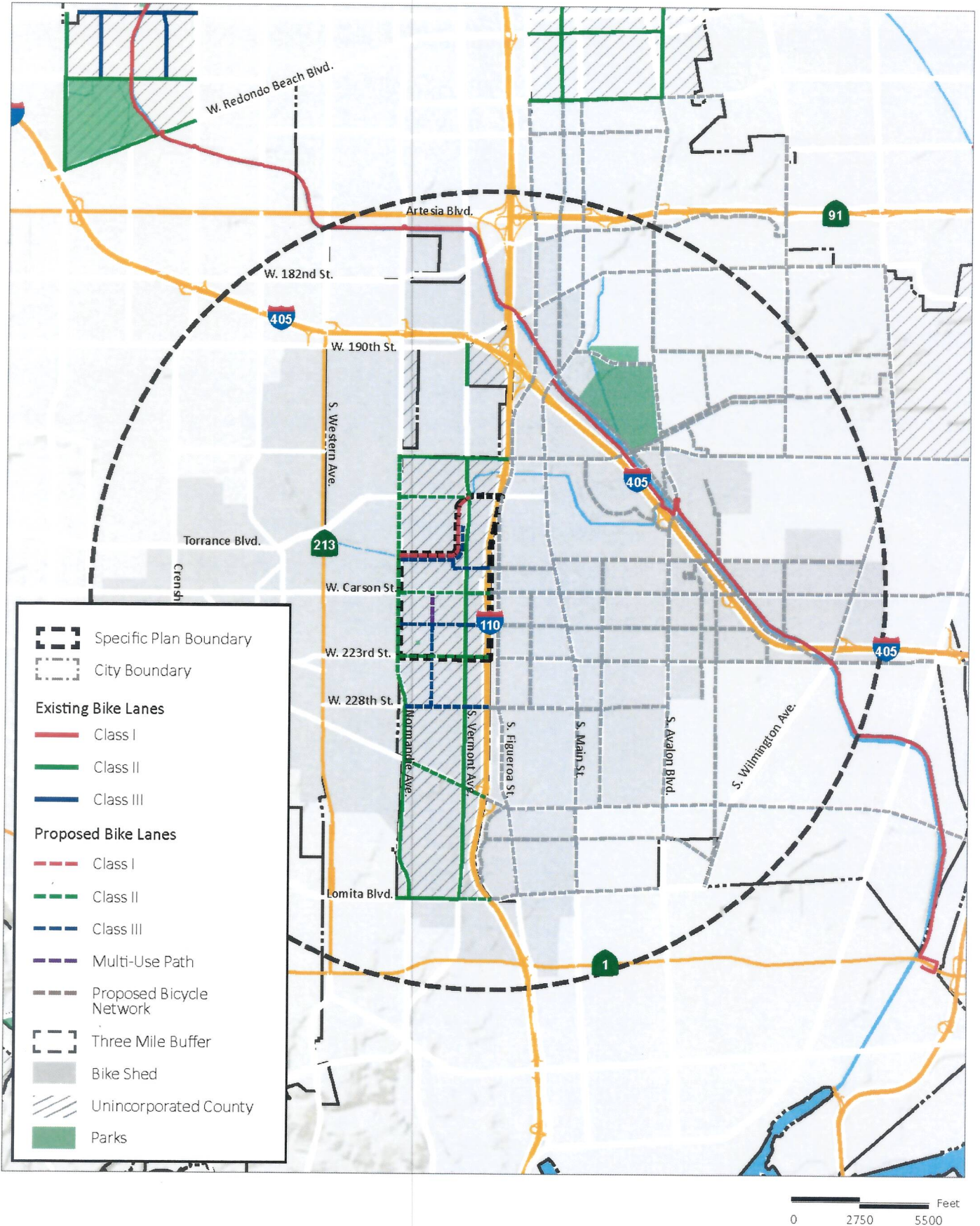
Figure 4.32 Bike Facility Types



Bicycle Network

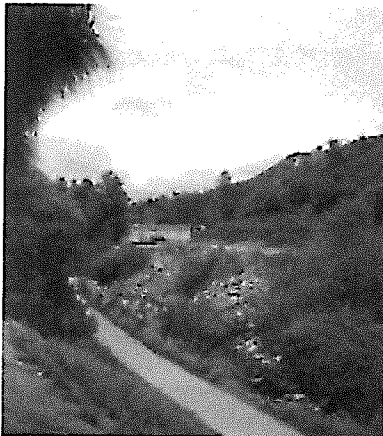
1. Figure 4.33 illustrates the level of bicycle accessibility within a three mile radius of the Carson/Harbor Freeway transit station. Using the station as a starting point, all possible bicycle routes were mapped based on the street grid and then consolidated into a larger catchment shape or bike shed. The existing bicycle network within the Specific Plan area provides limited accessibility and connectivity, with approximately five miles of existing bikeway infrastructure. Of the five miles, four and one half miles are Class II bicycle facilities and one half miles are Class III. One of the main constraints of the existing bicycle network is the limited north and south access, with only two north-south bike paths that traverse through West Carson.
2. The Specific Plan proposes to add approximately 7.50 miles of bikeways to the existing network. These streets were selected based on the opportunities they presented to improve connectivity within West Carson and the regional bikeway network of neighboring cities. The Specific Plan proposes to add the following bicycle facilities:
 - 208th Street Drainage Channel Multi-Use Path or Class I bicycle facility
 - Meyler Avenue Multi-Use Path (between 220th Street and Carson Street)
 - Normandie Avenue Class II bicycle facility
 - Carson Street Class II bicycle facility with striped buffer
 - 223rd Street Class II bicycle facility with striped buffer (between Vermont Avenue and City of Carson boundary)
 - 223rd Street Class III bicycle facility (between Normandie Avenue and Vermont Avenue)
 - 220th Street Class III bicycle facility
 - Meyler Avenue Class III bicycle facility
 - Doble Avenue Class III bicycle facility
 - 214th Street Class III bicycle facility
3. Buffered Class II bicycle lanes are proposed along Carson Street, and the segment of 223rd Street between Vermont Avenue and the City of Carson boundary. Buffered bicycle lanes are created by painting a striped buffer between the bicycle facility and on- street parking. These striped buffers help to improve safety and provide additional space between parked cars and bicyclists.

Figure 4.33 Bicycle Network



Striped buffers are recommended along streets with average speeds of 40 mph or more.

4. Additionally, the County, on behalf of West Carson, applied and received funding assistance for its West Carson Community Bikeways project under the Caltrans ATP Cycle 2 program. The West Carson Community Bikeways project includes the design and construction of 0.5 mile Class II bicycle facilities on both Lomita Boulevard and Carson Street, as well as 0.65 miles of Class III bicycle facilities on 220th Street, between Normandie Avenue and Vermont Avenue.

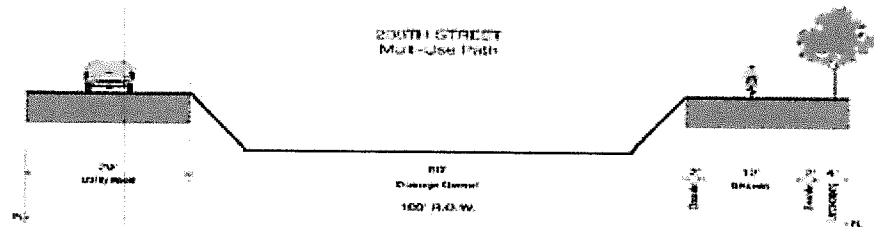


Example photo of multi-use path in a drainage channel.

208th Street Multi-Use Path

The 208th Street drainage channel presents an opportunity to implement a multi-use path to encourage and promote active transportation. The 208th Street drainage channel located along the northern boundary of the Specific Plan area provides opportunities to connect to the larger regional bikeway network, which includes the existing Class I bicycle facility along the Dominguez Channel. Figure 4.33 illustrates where the proposed multi-use pathway would be located, while Figure 4.34 illustrates its proposed design.

Figure 4.34 Proposed 208th Street Drain Channel Bikeway



Bicycle Infrastructure Amenities

In addition to provision of bikeways, the Specific Plan recommends the following design guidelines to guide installation of bicycle infrastructure amenities:

1. **Bicycle Parking:** Providing ample, well-designed bicycle parking is a key component to increasing bicycling within an area. Bicycle parking consists of racks that supports the bicycle upright and provides a secure place to lock. Care should be given to their installation to ensure that they do not obstruct pedestrian pathways and they adhere to ADA standards. Consideration should be given to installing bicycle parking near key transit stops and key destinations. The required amount of bicycle parking to be provided per land use is discussed in detail in the Bicycle Parking Standards section.
2. **Bicycle Crossing Signals:** Bicycle signals facilitate safe intersection crossing by instructing bicyclists when they are able to enter an intersection and by restricting conflicting vehicular movement.
3. **Bicycle Wayfinding Signage:** Bicycle wayfinding systems typically consists of signs or pavement markings that indicate

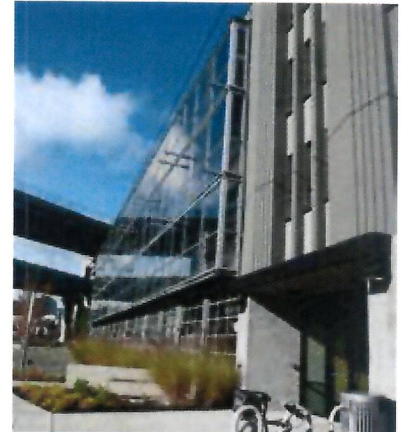
information regarding route, destinations, and directions. They help familiarize bicyclists with the bikeway network, identify best routes to destinations, and alert motorists of the presence of bicyclists. They should be placed along all streets that are part of the bikeway network.

Bicycle Parking Standards

The provision of bicycle parking provides a number of benefits that can enhance the overall bicycling experience in West Carson. These benefits include reducing bicycle damage from improperly parked bicycles, as well as reducing sidewalk hazards for pedestrians caused by improperly parked bicycles on sidewalks, reducing bicycle thefts, preventing damage to trees and other street furniture, and improving access to local shops, jobs, and transit.

The West Carson TOD Specific Plan provides modifications to the existing bicycle parking requirements contained in Chapter 22.112. Bicycle parking requirements are provided for both short-term and long-term parking. Per the County Code, short-term bicycle parking is defined as bicycle parking intended for a period of two hours or less, while long-term bicycle parking is defined as bicycle parking intended for a period of two hours or longer. Short-term bicycle parking is appropriate for individuals making short visits to commercial establishments, offices, or other uses, while long-term bicycle parking is appropriate for residents, employees, transit users, and visitors to hotels nearby.

The proposed modifications to the existing bicycle parking requirements are intended to provide a bicycle parking supply that supports TOD districts and encourages the use of bicycling as an alternative mode of transportation. The Specific Plan proposes the following bicycle parking requirements:



Example photo illustrating bicycle parking.

Table 4.1 Minimum Bicycle Parking Requirements

USE	SHORT-TERM	LONG-TERM
Multi-Family Residential	1 per 10 DU (2 minimum)	1 per 10 DU
General Retail	1 per 4,000 gsf (2 minimum)	1 per 6,000 gsf (2 minimum)
Hotel	1 per 30 guest rooms (2 minimum)	1 per 20 guest rooms (2 minimum)
Office	1 per 13,000 gsf (2 minimum)	1 per 6,000 gsf (2 minimum)
Theaters/Auditoriums/ Other Entertainment	1 per 50 intended visitors (2 minimum)	1 per 100 intended visitors (2 minimum)
Industrial	1 per 20,000 gsf (2 minimum)	1 per 10,000 gsf (2 minimum)
Hospitals	1 per 15,000 gsf (2 minimum)	1 per 10,000 gsf (2 minimum)
Schools	4 per classroom (4 minimum)	1 per 10 classrooms (2 minimum)
Churches	1 per 50 intended visitors (2 minimum)	1 per 100 intended visitors (2 minimum)

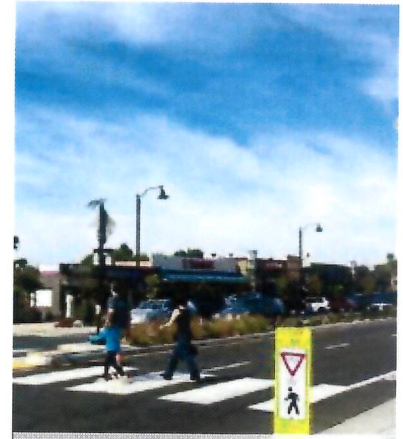
DU denotes dwelling units, gsf denotes gross square feet.

SAFETY AND COMFORT

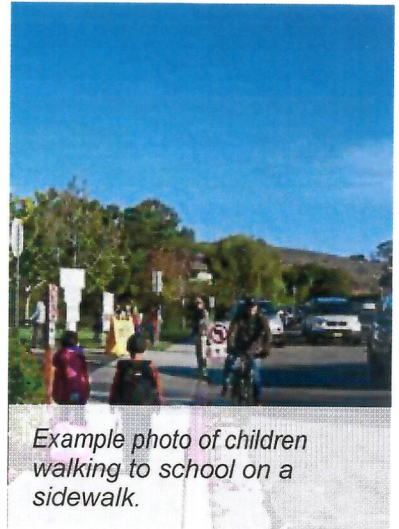
SAFE ROUTES TO SCHOOL

Safe Routes to School (SRTS) is a program that focuses on helping children get to school safely by walking and bicycling. Meyler Street Elementary School and Van Deene Avenue Elementary School both lie within the Specific Plan area, creating the need to develop a safe network of pedestrian and bicycle infrastructure for children to utilize. Public Works identified suggested crossings to be used by children for various elementary schools within the unincorporated areas of the County. Figures 4.35 and 4.36 illustrates the County's suggested routes for both schools. Although some controlled intersections exist within the area, there is a general lack of pedestrian crossing signage and protected crosswalks surrounding the area. The Specific Plan proposes the following design guidelines to promote safer routes to school:

1. Appropriate levels of street lighting should be installed on both sides of wide streets.
2. Appropriate traffic controls, such as marked crosswalks, traffic signals, and warning signs or flashers should be utilized at pedestrian crossing locations.
3. Curb ramps with warning strips, such as truncated domes, should be provided at pedestrian street crossings to facilitate the safe crossings of pedestrians with mobility or vision impairments.

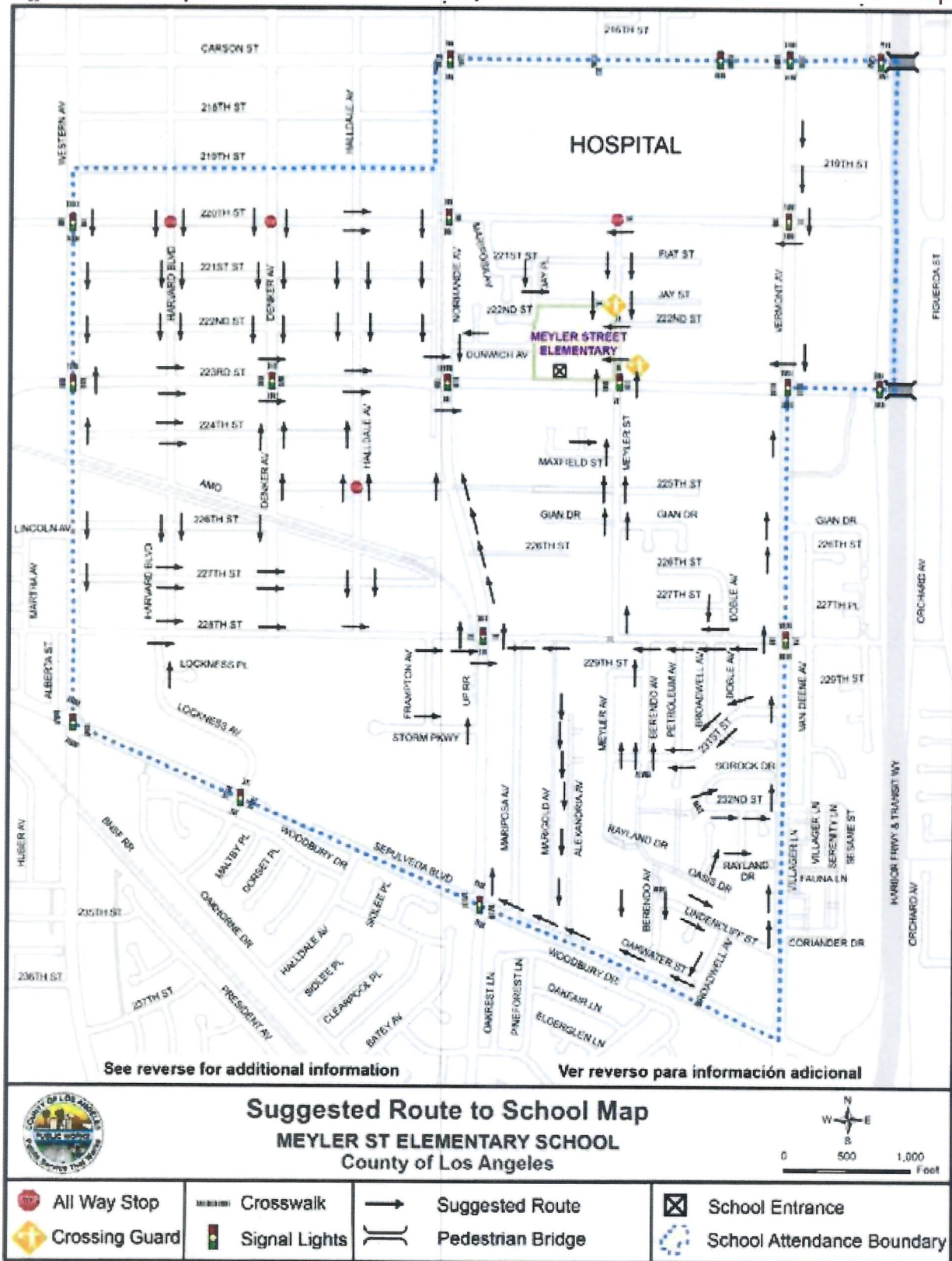


Example photo illustrating a marked crosswalk.



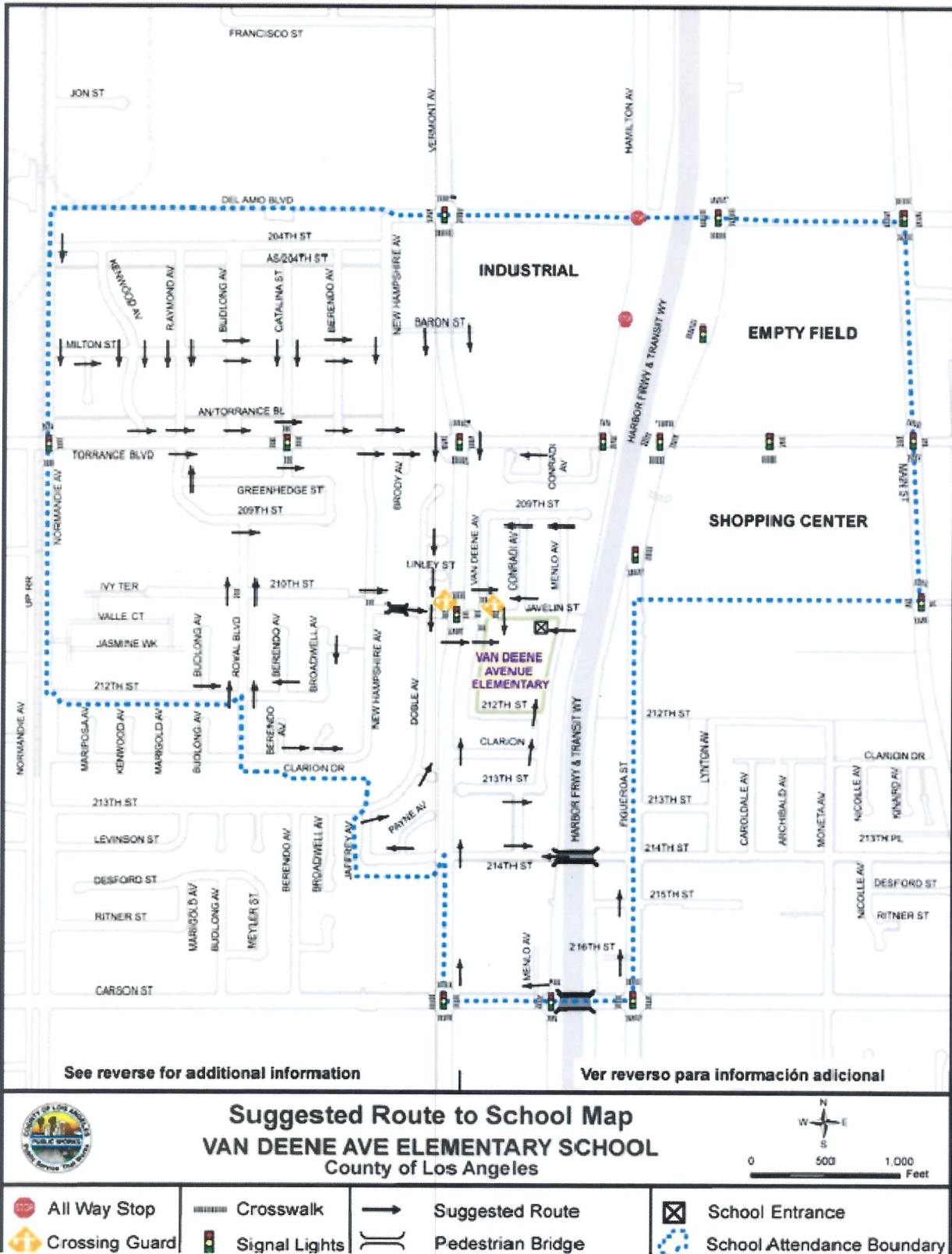
Example photo of children walking to school on a sidewalk.

Figure 4.35 Meyler Street Elementary School Safe Routes To School Map



Source: Los Angeles County Department of Public Works

Figure 4.36 Van Deene Avenue Elementary School Safe Routes To School Map



Source: Los Angeles County Department of Public Works

PARKING STANDARDS

Parking policies play a significant role in the viability and success of TOD projects and districts. These policies can help to shape an area's travel behavior, community design, and local economic growth. The existing parking supply within the Specific Plan area is comprised of a combination of on-street and off-street parking resources as shown in Figure 4.36. The Specific Plan provides modifications to the parking requirements contained in Chapter 22.112 of Title 22 of the County Code. These modifications are intended to provide a parking supply that supports TOD districts and allows for greater flexibility in the provision of minimum parking spaces.

The Specific Plan proposes the following off-street parking requirements:

1. Required parking shall be provided on site; and is not permitted within the required front yard or corner side yard. Required parking for new single-family uses may be uncovered. A Ministerial Site Plan Review (SPR) application is required for the conversion of covered parking to uncovered parking in existing single-family uses.
2. Tandem parking is allowed for single family uses. A parking space for an accessory dwelling unit may be located in tandem with a space serving the existing single-family residence if the design is necessary to provide the required number of spaces for both units.
3. The provisions regarding shared parking contained in Chapter 22.180 (Parking Permits) of Title 22 shall apply.

Figure 4.37 Parking

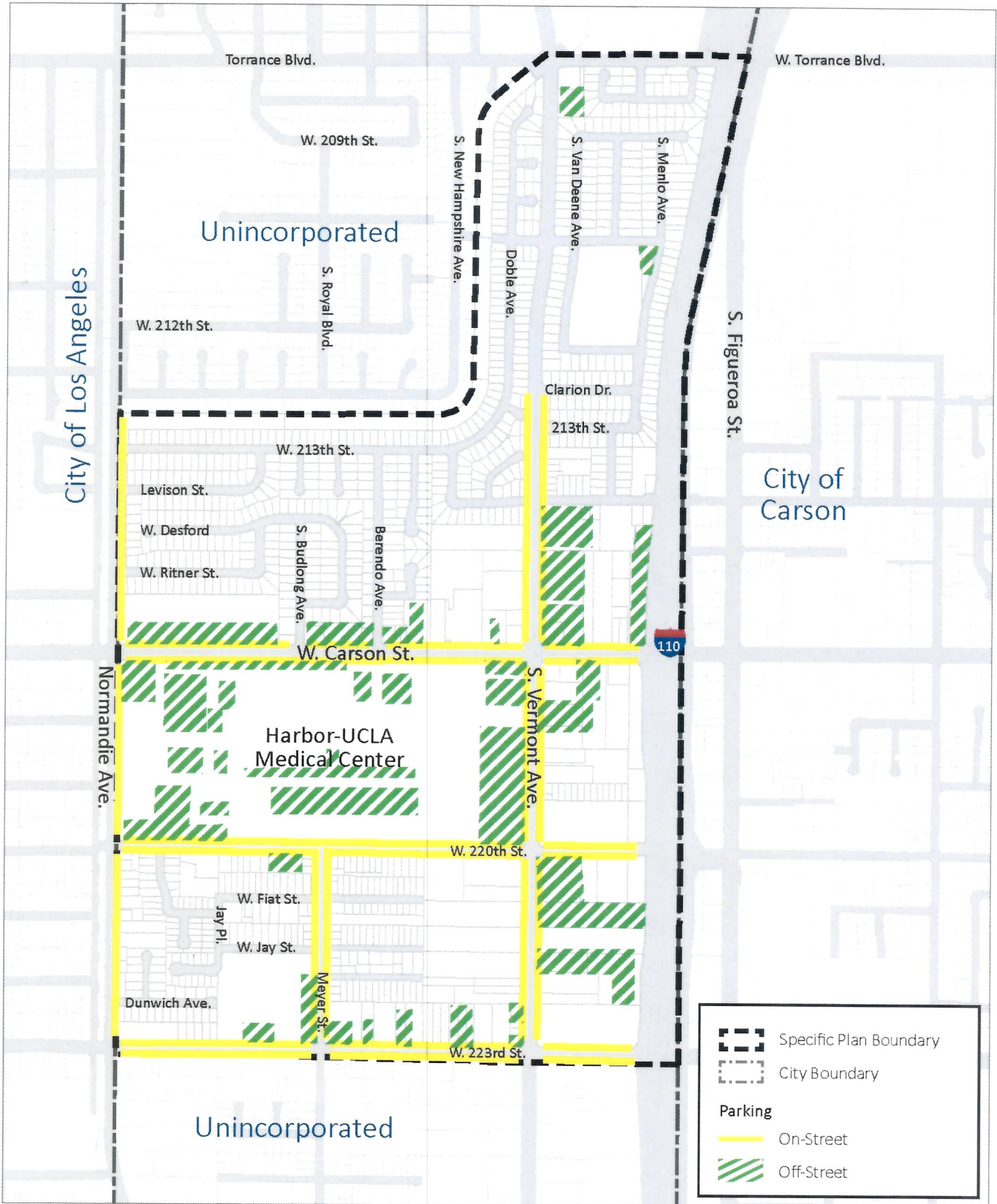


Table 4.2 Parking Requirements

USE	MINIMUM	MAXIMUM
Residential Zone 1		
Single-Family Residential	2.00 spaces/DU	2.00 spaces/DU
Residential Zone 3		
Bachelor Apartment	0.60 space/DU	1.00 space/DU
1 Bedroom	0.90 space/DU	1.25 spaces/DU
2+ Bedroom	1.20 spaces/DU	2.00 spaces/DU
Guest	0.15 space/DU	0.25 space/DU
Residential Zone 4		
Bachelor Apartment	0.60 space/DU	1.00 space/DU
1 Bedroom	0.90 space/DU	1.25 spaces/DU
2+ Bedroom	1.20 spaces/DU	2.00 spaces/DU
Guest	0.15 space/DU	0.25 space/DU
Mixed-Use Zone 1		
Bachelor Apartment	0.60 space/DU	1.00 space/DU
1 Bedroom	0.90 space/DU	1.25 spaces/DU
2+ Bedroom	1.20 spaces/DU	2.00 spaces/DU
Guest	0.15 space/DU	0.25 space/DU
Office	1.50 spaces/ 1,000 sf	2.50 spaces/ 1,000 sf
Retail	2.50 spaces/ 1,000 sf	3.50 spaces/ 1,000 sf
Restaurant	1.00 space/200 seats	1.00 space/ 100 seats

Mixed-Use Zone 2		
Bachelor Apartment	0.60 space/DU	1.00 space/DU
1 Bedroom	0.90 space/DU	1.00 space/DU
2+ Bedroom	1.20 spaces/DU	2.00 spaces/DU
Guest	0.15 space/DU	0.25 space/DU
Office	1.50 spaces/ 1,000 sf	2.00 spaces/ 1,000 sf
Retail	2.40 spaces/ 1,000 sf	3.00 spaces/ 1,000 sf
Restaurant	1.00 space/200 seats	1.00 space/ 100 seats
Harbor-UCLA Medical Zone		
Medical Office	2.50 spaces/ 1,000 sf	3.50 spaces/ 1,000 sf
Clinic	2.50 spaces/ 1,000 sf	3.50 spaces/ 1,000 sf
Industrial Flex Zone		
Light Industrial	1.20 spaces/ 1,000 sf	2.00 spaces/ 1,000 sf

DU denotes dwelling units, sf denotes square feet. Guest parking is only required when calculated to greater than one (1). Guest parking spots are rounded down to the nearest whole number.

05

INFRASTRUCTURE

INTRODUCTION

Utility service infrastructure such as water, wastewater, gas and electric facilities, and stormwater drainage facilities are critical services for ensuring orderly growth and community investment. The infrastructure discussion provides an overview of existing and future conditions for water, sewer, and storm drain systems serving the West Carson Specific Plan area. This section identifies the current conditions for these infrastructure systems, along with recommended upgrades to accommodate the levels of new development proposed as part of the Specific Plan land use concepts.

WATER SERVICES

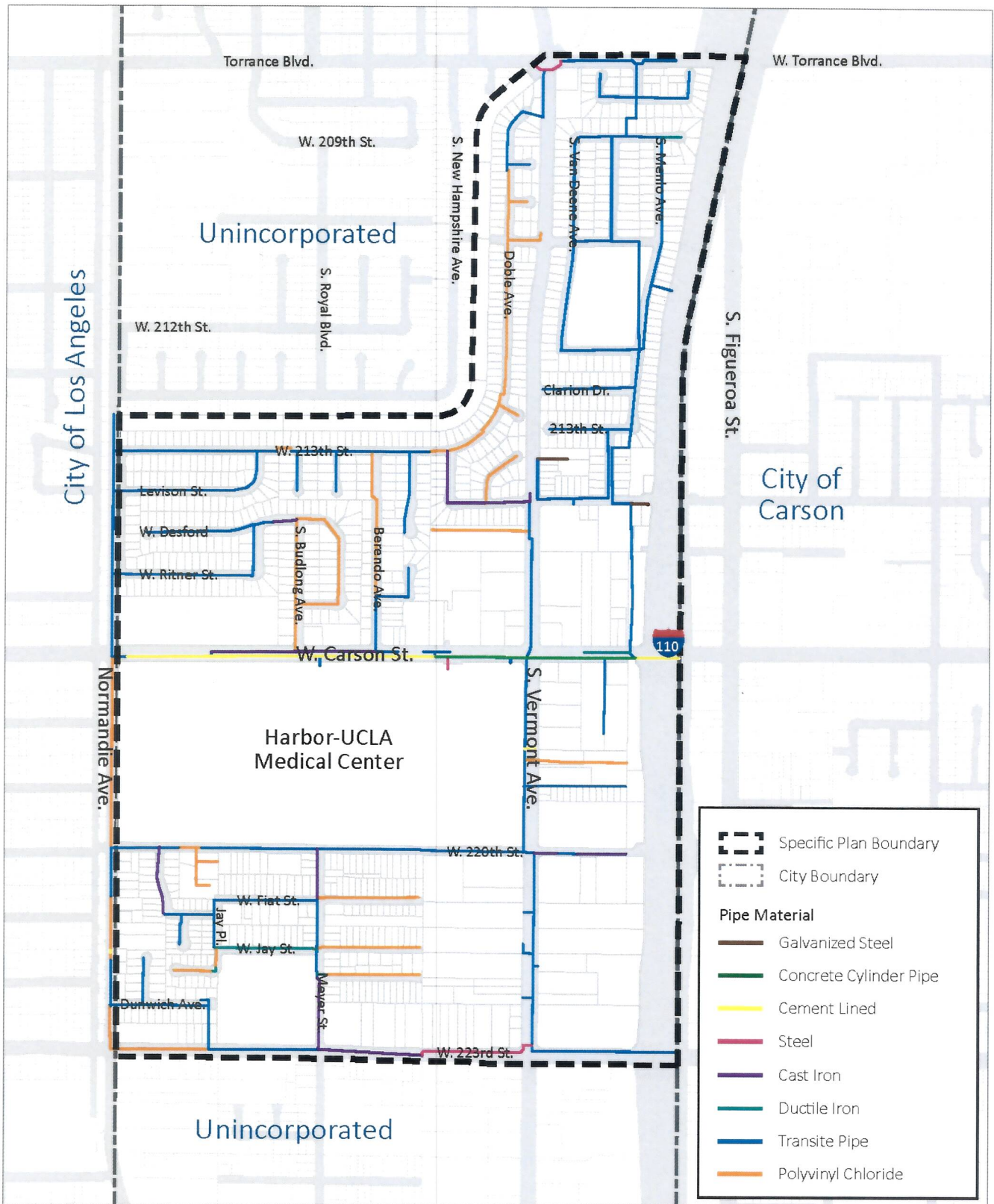
The Rancho Dominguez District of the California Water Service is the provider of potable water within the boundary of the West Carson TOD Specific Plan. The customer needs are met by a combination of local groundwater and surface water purchased from the Metropolitan Water District of Southern California.

WATER SERVICES EXISTING CONDITIONS

The Specific Plan area is serviced by pipe sizes varying from two inch connectors to 33 inch main lines. The vast majority of pipe is composed of one of two materials – transite and PVC. The largest pipe connects the plan area to the east side of Interstate 110 via a 33 inch water main. This decreases to a 16 inch main before connecting to the 10 inch distribution pipe on the west boundary of the plan area on Normandie Avenue. The majority of distribution pipes off the main lines are six inch and eight inch water lines. Figures 5.1 and 5.2 illustrate water pipe materials and size for the Specific Plan area.

Water supply requirements and flows were estimated using industry standards to determine capacities. The Harbor-UCLA Medical Center located along the 33 inch to 24 inch main is the largest estimated consumer in the area. The large water main located next to the medical center campus is capable of providing both water supply and fire flow protection with minimal head loss. The distribution pipe size is large enough to sufficiently supply the Specific Plan area with water.

Figure 5.1 Water System- Pipe Material



California Water Service has a waterline replacement plan in place with the goal to replace water mains every 50 years. The pipes in West Carson are included in this plan. The district no longer uses transite pipes for water mains, and so existing transite pipes will be phased out and replaced over time.

WATER SERVICES FUTURE CONDITIONS

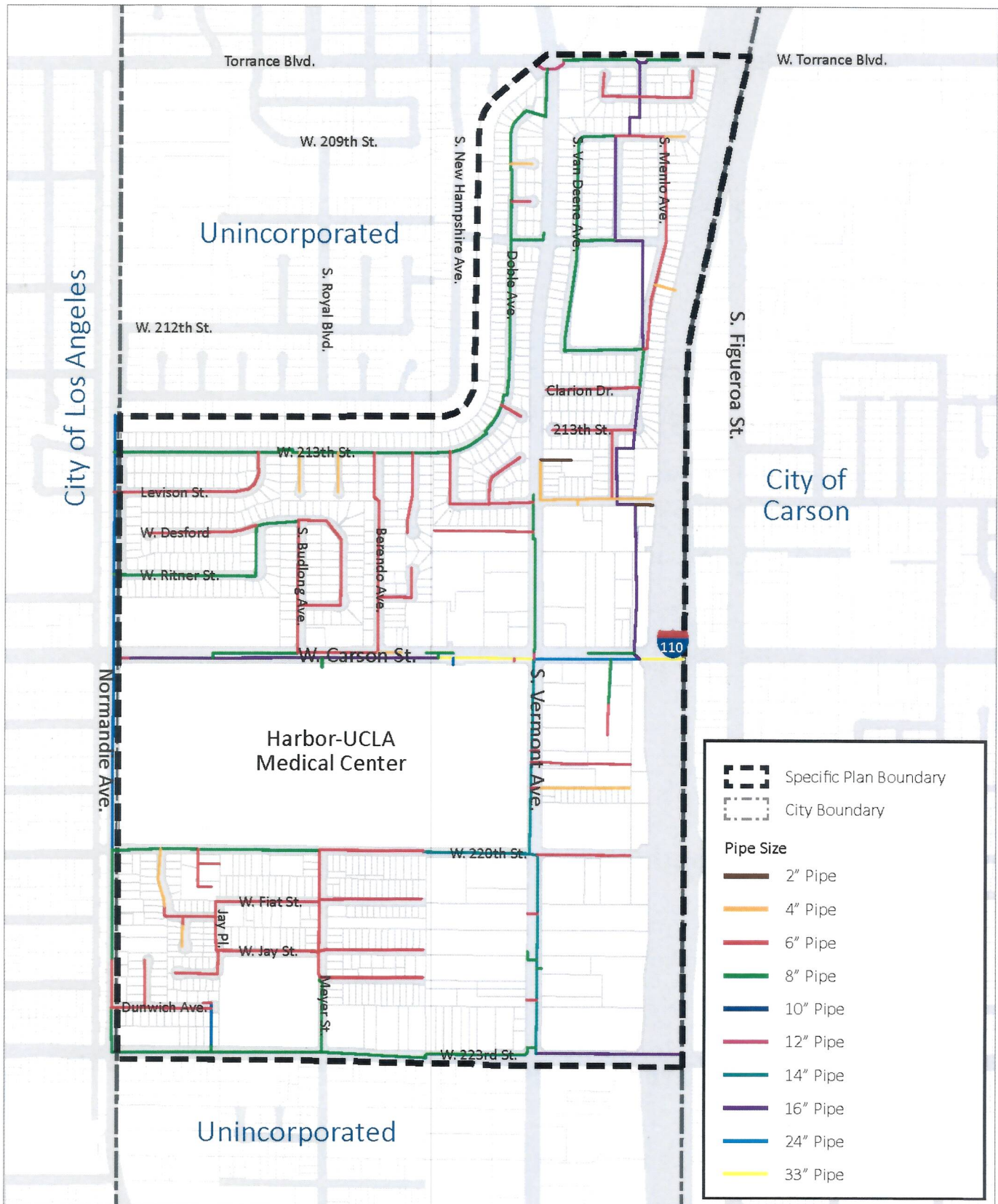
Buildout of the Specific Plan includes a substantial influx of households which in turn results in an increase in water demand and water flow on the north end of Carson Street and the east side of Vermont Avenue around the Harbor-UCLA Medical Center. South from West 220th Street along Vermont Avenue, this increase in flow is a result of additional proposed development on both sides of Vermont Avenue. The water main service line in the area is the 33 inch and 24 inch pipe along Carson Street coming from the east.

The two largest areas associated with planning zone West Carson Residential 1 have no need to increase capacity of service lines because the decrease in square footage will decrease flow to the areas.

To meet the demand of water consumption and flows with buildout of the Specific Plan land uses, the following upgrades in water line pipe sizes are recommended:

- The 14 inch Pipe from Carson Street to 220th Street along Vermont Avenue has such a dramatic increase in flow that it will require resizing to a minimum of a 20 inch pipe.
- South from 220th Street to 223rd Street, the 14 inch pipeline is near the headloss threshold. The flow rate north from Carson Street to 214th Street is split between two lines: an eight inch pipe along Vermont Avenue, and a 16 inch along Menlo Avenue. Depending on the locations of the planned households and which distribution line supplies the water, one or both of these lines will need to be replaced. Without knowing flow into each pipeline, exact sizing cannot be recommended at this time. Figure 5.3 highlights the general location of these water system impacts.
- A fact sheet from California Water Boards titled "August 2015 Statewide Conservation Data" shows that the average flow in the South Coast Hydrologic Region has decreased significantly to roughly 100 gallons per capita per day. Using this flow, with an assumption that water conservation measures such as low water use toilets, low flow showerheads, watering lawns and gardens in morning or evening hours, improved leak

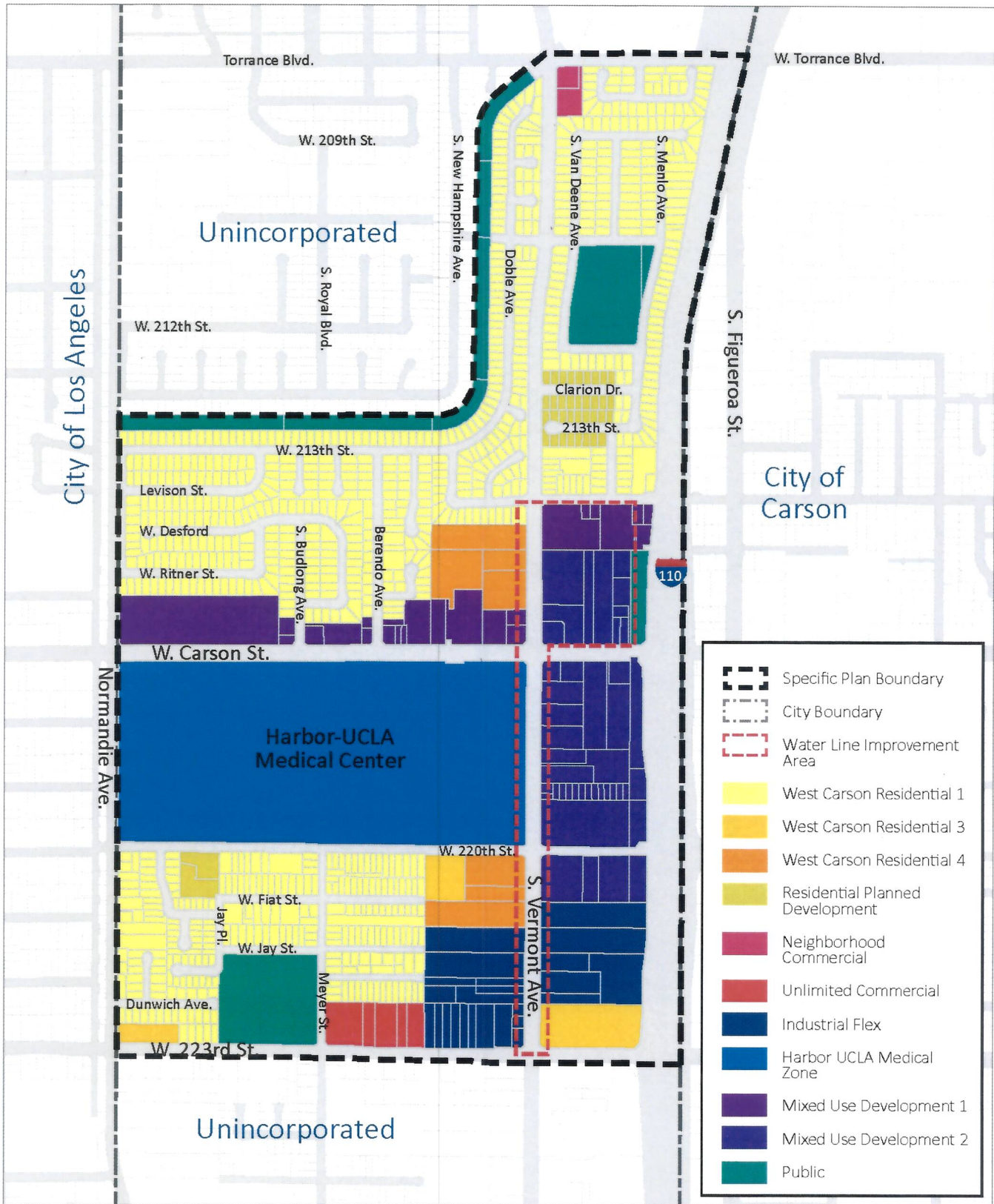
Figure 5.2 Water System- Pipe Size



detection and repair, etc., are in place to maintain a low average daily demand, there is no pipe that exceeds the threshold of headloss per 1,000 feet.

These recommendations are based on an assumption of flow into the area that is independent of factors in the surrounding area. The water provider, California Water Service, will evaluate all development that will occur in the future and analyze and decide whether water facilities require upgrades.

Figure 5.3 Water Services Area of Concern



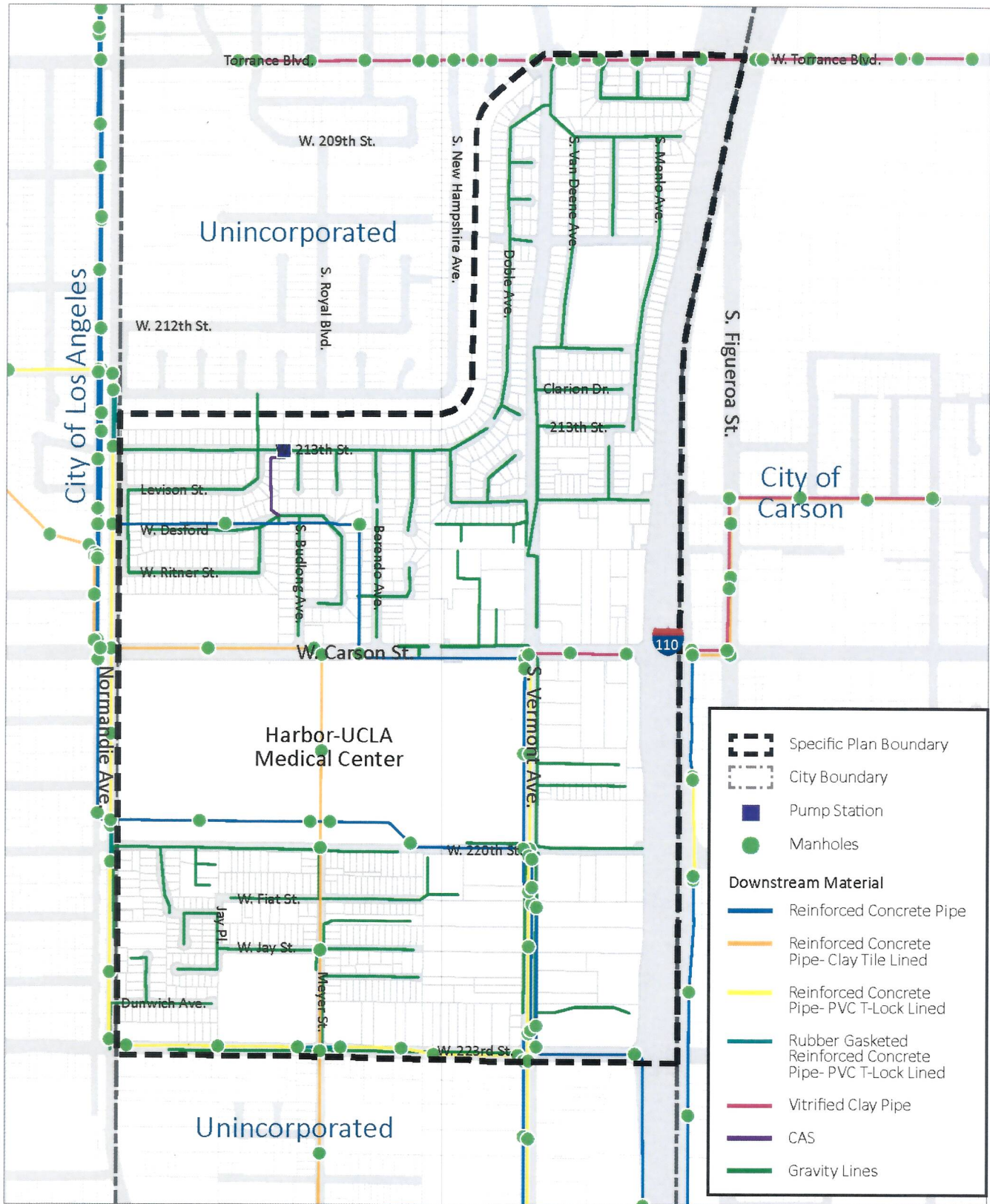
SEWER SERVICES

SEWER SERVICES EXISTING CONDITIONS

Two sanitary systems exist in the Specific Plan area – local lines and trunk sewers. The local lines are a series of primarily eight inch gravity mains with laterals connecting to existing buildings. All of these sewers are composed of Vitrified Clay Pipe (VCP). The northwest section of the Specific Plan area collects into a pump of the main trunk. All local sewer lines are owned and operated by Public Works.

The trunk sewer lines are owned and operated by the Carson District of the Los Angeles County Sanitation Districts (LACSAN). There are four main segments of these trunk lines collecting the sewage from the Specific Plan area. The northeast corner is served by a 12 inch VCP Trunk. Another trunk line runs east on Desford Street, south on Berendo Avenue, and east on Carson Street before connecting up with the large trunk that runs south on Vermont Avenue. A third trunk, 54 inch in size, runs east on Carson Street at the Specific Plan boundary, cutting south through the Harbor-UCLA Medical Center campus along the same center line as Meyler Street, continuing south past the Specific Plan boundary. The final trunk line runs east on 223rd Street, connecting with the second trunk and continuing south on Vermont Avenue. There are three segments of trunk line that are out of service: 1) 63 inch trunk running east along the southern Harbor-UCLA access road (adjacent to 220th Street) connecting at Vermont Avenue, 2) 66 inch to 78 inch trunk running south along Vermont Avenue from Carson Street to 223rd Street, and 3). The continuation of the first unused trunk running south from 220th Street to 223rd Street. The majority of these lines are reinforced concrete with linings as shown in Figure 5.4.

Figure 5.4 Sanitary Utilities



The eight inch sanitary collection lines are of sufficient size to collect sanitary waste from houses and shops in the area and transport them to the main collection trunks. The medical center is located in close proximity to one of the larger trunks. Because of this, the higher volume of waste produced from the medical facilities is sufficiently captured. In the event the status of the plan area would change with the addition of industrial sites, the collection lines would require evaluation.

SEWER SERVICES FUTURE CONDITIONS

Sewer services in the Specific Plan area would require updating under buildout of the Specific Plan. The connection laterals of the largest zone: West Carson Residential 1, are large enough to accommodate the changes of the area. The large increase in flows under buildout result from a large anticipated increase of population located along a sanitary trunk main that runs along Carson Street north of Harbor-UCLA Medical Center and then along Vermont Avenue east of Harbor-UCLA; and along West 223rd Street, between Normandie Avenue and Vermont Avenue.

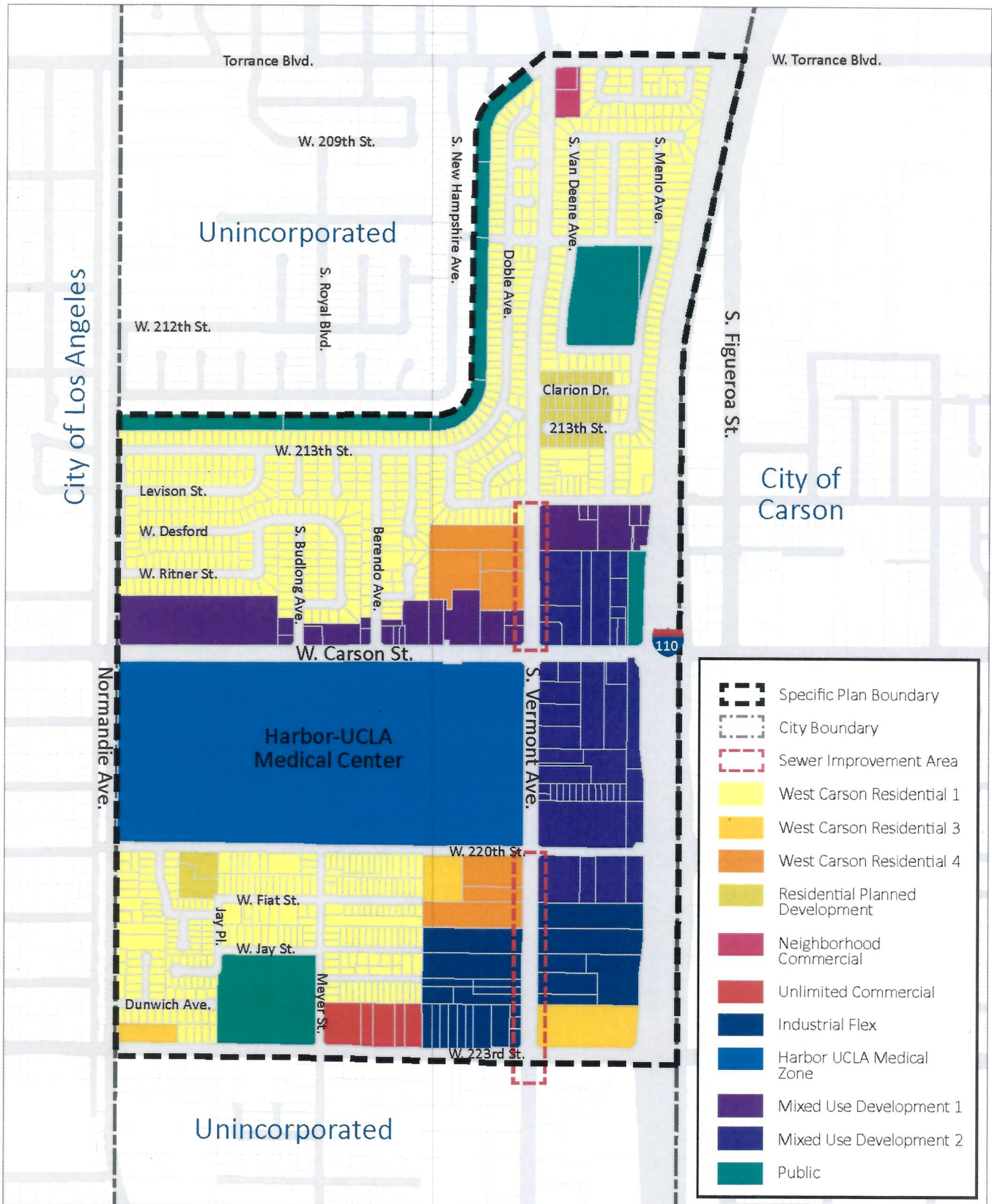
Due to the location of increased development in the Specific Plan, substantial amounts of new development will occur near existing collection trunks. Therefore, collection laterals are not anticipated to need to be resized. However, with buildout of the Specific Plan, area north of Carson Street and east of Vermont Avenue would require connection to the trunk line south of Carson Street. The increased flow in that area cannot be handled by the existing eight inch collection laterals. With the large increase in flow along the east side of Vermont Avenue, more collection laterals may need to be installed to adequately capture the flow and distribute it to the trunk line servicing the area. The total increased flow to the area will have an increase of peak flow from 4.23 cubic feet per second to 9.90 cubic feet per second. Areas of potential impact are shown in Figure 5.5.

Trunk sewer capacity is highly dependent on upstream and downstream flows. The trunk line south of 220th Street past the Specific Plan area is undersized for flow in the area. With an increase of 5.6 cubic feet per second, this trunk line would need to be addressed. Strategies that could address the flow need include:

- Installation of a pumping station which could provide pressure to the line
- Allowing more sewage flow through it;
- Increasing the size of the trunk; or
- Increasing the slope at which the trunk carries the sewage.

Using the same low flow data from the water service, the trunk line would still be undersized and would need to be addressed. The peak factor would increase the flow by 2.8 cubic feet per second. As these collection trunks span the entire collection system of LACSAN, it will evaluate all development that will occur and conduct its own analysis of changes to the service trunk and necessary sewer infrastructure upgrades.

Figure 5.5 Sewer Services Area of Concern



STORMWATER SERVICE

STORM DRAINAGE EXISTING CONDITIONS

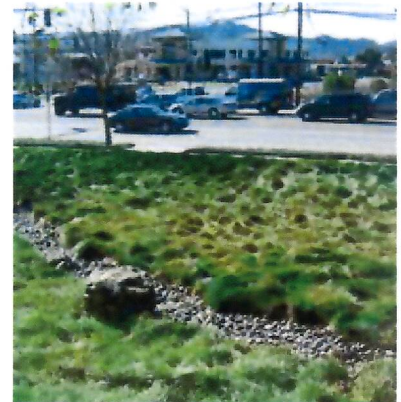
Stormwater runoff in the Specific Plan area is managed by closed and open drainage channels. The primary source for drainage is a 10-foot reinforced cement concrete channel, which is also the north boundary of the Specific Plan Area. All but two sections of pipe drain into this channel, including the five-foot reinforced cement concrete channel that is located on the north side of West 220th Street. All drainage basins in the area connected to this channel connect via reinforced concrete pipe ranging from 18 inches to 84 inches in size. This drainage channel drains east and connects to the Dominguez Channel located to the northeast in the City of Carson. The storm drains on the southern part of the Specific Plan area drain south, eventually connecting with the Wilmington Drain. Figure 5.6 illustrates the storm drain system within the Specific Plan area.

STORM DRAINAGE FUTURE CONDITIONS

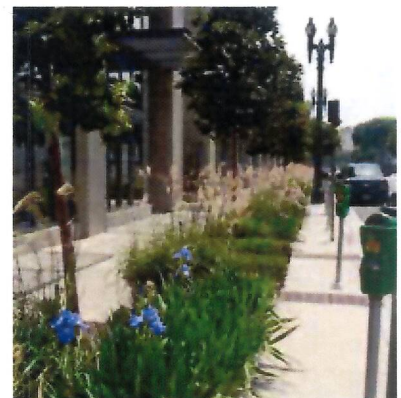
Stormwater services in the Specific Plan area are connected to the large network of open channel drains which are tied to a larger collection basin. Stormwater flow in these channels is greatly dependent on upstream and downstream flow. Buildout of the Specific Plan will generate little increase in runoff to the existing drainage system, since the area is completely developed.

New development will primarily affect the existing impervious surfaces surrounding Harbor-UCLA Medical Center and Vermont Avenue. The volume of stormwater runoff is expected to not significantly change due to the minimal disturbance of areas having existing pervious surfaces. Areas designated as West Carson Residential 1 are the primary pervious surface locations in the Plan Area, which will remain primarily undisturbed.

Recent trends from the National Oceanic and Atmospheric Administration (NOAA) indicate rainfall events increasing in intensity, but decreasing in duration. This increased intensity does not allow as much stormwater to be captured by pervious surfaces and increases instantaneous flow on impervious surfaces. This trend should be monitored by the County's stormwater management team for future development.



Example photo of a pervious surfaces used to manage stormwater.



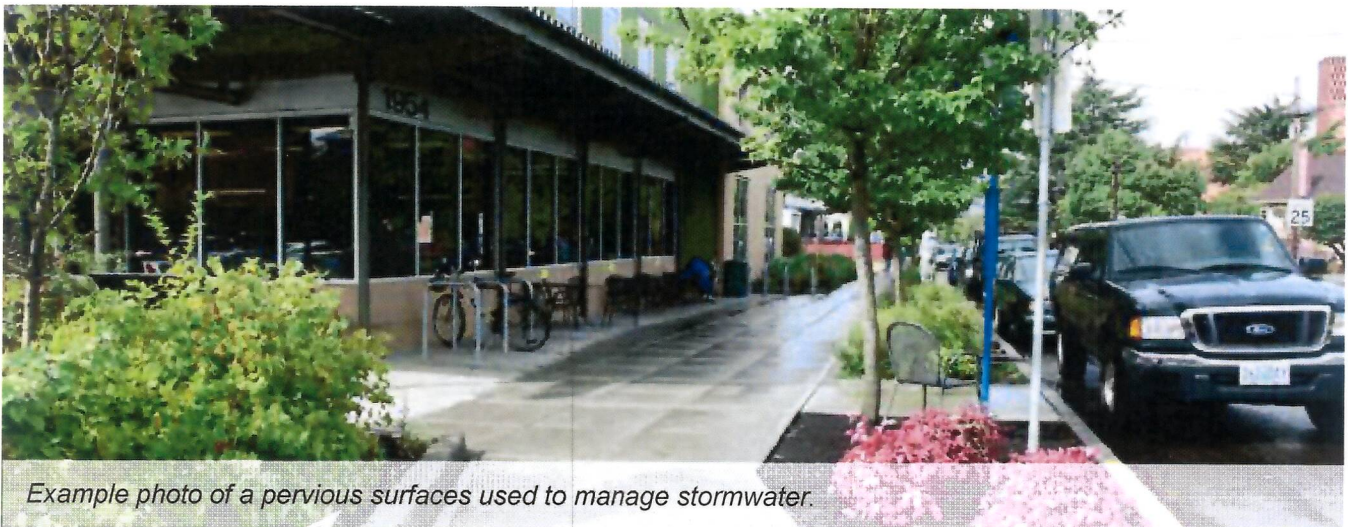
Example photo of a pervious surfaces used to manage stormwater.

GREEN INFRASTRUCTURE RECOMMENDATIONS

Impervious roadways frequently wash stormwater runoff containing harmful pollutants into nearby water bodies, such as rivers and flood control channels, which lead directly to the ocean. These harmful pollutants include substances commonly present on roadway surfaces, such as dirt, oil, grease, toxic chemicals, and trash, and can harm wildlife in local water bodies as well as nearby plants and vegetation. Impervious roadways can also contribute to problems with stormwater quantity. When stormwater falls onto impervious surfaces, it cannot seep into the ground and therefore can cause flooding on roadways.

Public Works has developed Green Infrastructure Guidelines to guide new construction and reconstruction of road and flood projects. The goal of the guidelines is to incorporate sustainable practices into the design, construction, and operation of Public Works' infrastructure. The guidelines provide low-impact development (LID) design options to consider during planning or designing of road and flood projects intended to manage stormwater runoff.

The Specific Plan recommends that all new development projects that involve the construction of new roadway projects shall conform to the Green Infrastructure Guidelines as set forth by Public Works. The guidelines define roadway projects to include the new and reconstruction of public roads, maintenance access roads, road widening, medians, bike paths, sidewalks, parking lots, grade separation, etc. Further, all new development projects shall incorporate the following best management practices as identified by the guidelines:



Example photo of a pervious surfaces used to manage stormwater.

Permeable Surfaces

Permeable surfaces should be incorporated whenever feasible to allow infiltration of rainfall and to reduce the total volume of runoff, replenish groundwater, and improve water quality. The following lists some of the guidelines for the application of permeable surfaces from Public Works' Green Infrastructure Guidelines:

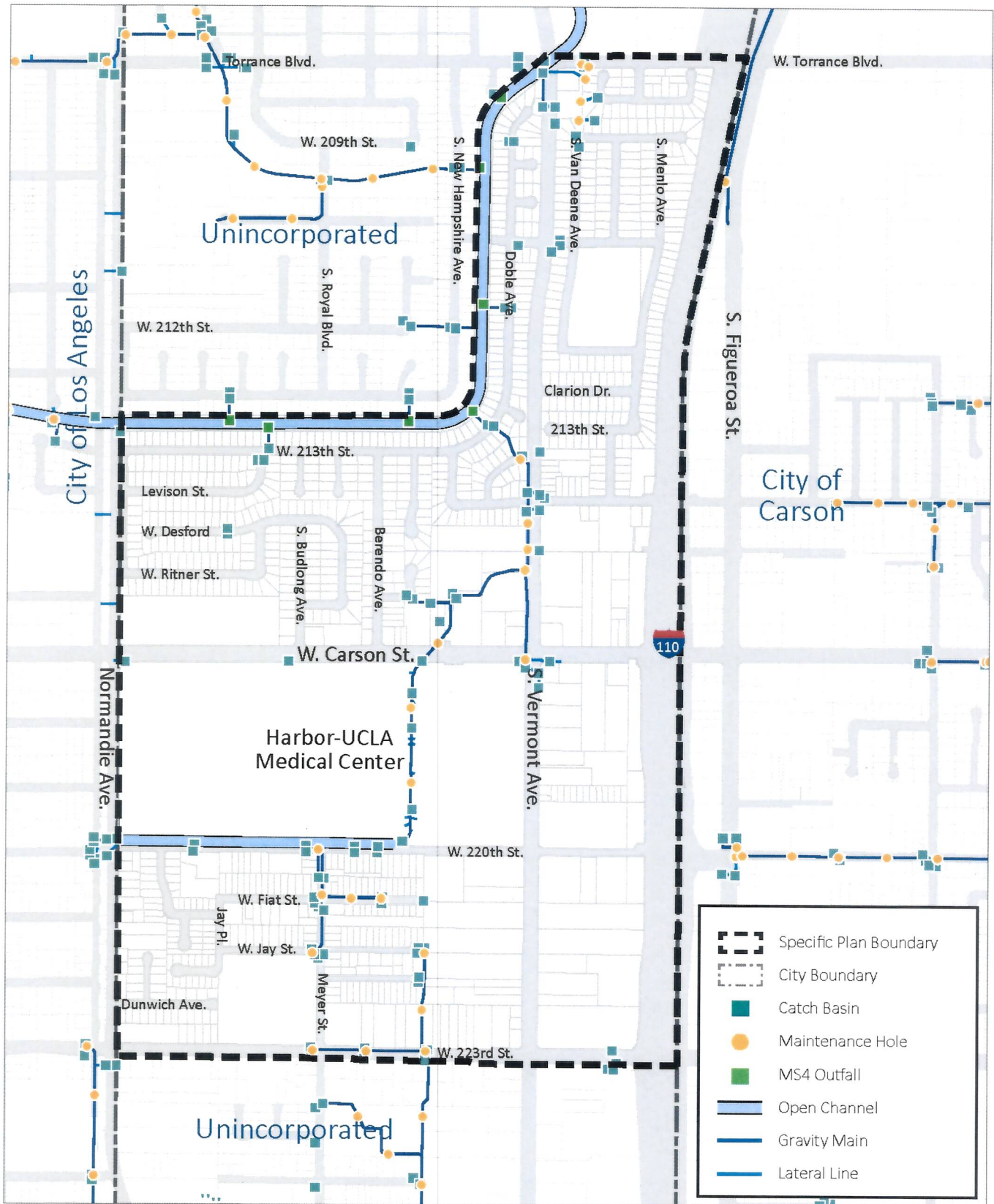
- »» Permeable sidewalks shall adhere to existing Public Works standards for sidewalk design.
- »» Permeable access roads are not recommended for roadways with high volume of equipment trucks, as they can cause damage to permeable surfaces.
- »» Permeable pavement and underdrain systems for parking lots shall be directed toward LID-type best management practices if needed to achieve the required volume reduction.
- »» Permeable alleys are recommended for alleys that are less than eight-feet wide since they prevent access from heavy vehicles.

Vegetation and Landscaping

Vegetation and landscaping elements such as vegetated swales, vegetated buffers, planter/tree box filters, bioretention, and filter strips are intended to maximize available permeable space in an area to reduce pollutant concentrations in stormwater runoff and reduce runoff rates. The following lists some of the guidelines for the application of vegetation and landscaping from Public Works' Green Infrastructure Guidelines.

1. Vegetated swales shall be designed in accordance with Chapter 3 of Public Works' Best Management Practices Design and Maintenance Manual.
2. Vegetated swales are recommended in areas where slope is between one and six percent.
3. Plant species for vegetated swales shall be tolerant to both extreme wet and dry conditions. Refer to the vegetated swale plant list of Public Works' Best Management Practices Design and Maintenance Manual.
4. Vegetated swales should be greater than 100 linear feet in length and at least 12 inches in depth from the top of the sidewalk to the swale bottom.

Figure 5.6 Storm Drain System



5. Bioretention facilities shall be designed in accordance with Chapter 5 of Public Works' Best Management Practices Design and Maintenance Manual.
6. Planting/tree box filter designs should typically incorporate a concrete vault filled with a bioretention soil mix and vegetation, and may contain an underdrain connected to an adjacent flood control conveyance.

SOLID WASTE MANAGEMENT

The Specific Plan area utilizes the residential/commercial franchise system for solid waste collection services. Currently, Calmet Services Incorporated provides trash collection and recycling services to the unincorporated residents of Oceanview-La Rambla-West Carson under an exclusive residential franchise agreement with the County.

Key issues surrounding waste management within the County include increasing volumes of waste being disposed and generated, lack of solid waste processing facilities to accommodate volumes of waste generated, and public opposition towards the construction of new solid waste management facilities. As available space for landfills becomes more limited, local jurisdictions shall implement effective waste management strategies to reduce solid waste volumes.

In 2014, the County Board of Supervisors (Board) adopted a Roadmap to a Sustainable Waste Management Future. This roadmap outlines the process by which the County can implement strategies to reduce solid waste generation in unincorporated areas and with County operations. The West Carson community is part of this program, which includes goals of reducing solid waste destined for landfills by 80 percent by 2030 and 95 percent by 2040.

ELECTRICAL SERVICES

Electricity is provided to the Specific Plan area by Southern California Edison (SCE), a private utility company. SCE sets its own service standards, with oversight from the California Public Utilities Commission (CPUC), and facility improvement strategies. Electricity is transmitted by above-ground power lines that currently supply sufficient electrical service to the Specific Plan and have adequate capacity to serve the area with buildout of the Plan.

NATURAL GAS SERVICES

The Southern California Gas Company, a subsidiary of Sempra Utilities (The Gas Company), a private utility company, is the natural gas service provider for the Specific Plan area. Natural gas pipelines exist along all major street rights-of-way within the area.

The analysis and decision on capacity to meet future demand under buildout of the Specific Plan will be conducted by The Gas Company in coordination with the County at the time development occurs and building plans are submitted.

TELECOMMUNICATIONS AND CABLE SERVICES

AT&T and Time Warner, two separate private utility companies, both provide local and long distance telecommunications services in the Specific Plan area. Time Warner Cable provides cable and high-speed internet services. Various wireless carriers provide service within the Specific Plan area. Wireless communications facilities, either freestanding or attached to a building or structure, are required to comply with the design guidelines, as well as approval of a conditional use permit. Conditional use permits for wireless communications facilities shall expire 10 years from the date of approval, unless amended or extended by the planning commission or hearing officer.

06

ECONOMIC DEVELOPMENT STRATEGY

ECONOMIC DEVELOPMENT STRATEGY

INTRODUCTION

This chapter provides the Economic Development Strategy for the Specific Plan. The Specific Plan area is expected to undergo sizable growth in employment to support uses related to the hospital such as medical offices; education, research, and development uses; and commercial offices and retail. The strategy provides economic analysis, a financing strategy, and policy recommendations to realize the full potential of the County's investments in the Harbor-UCLA Medical Center campus and Metro's investments in the Silver Line Carson Street station.

The economic development strategy includes:

- »» Implementation strategies to help local stakeholders that enhance West Carson's ongoing economic vitality.
- »» Assessment of the extent to which current strategies are relevant given the unique context of this unincorporated area.
- »» Recommendations for stimulating job growth, balancing employment and housing opportunities, and spurring new investment to help revitalize the West Carson area.

OPPORTUNITIES FOR ECONOMIC DEVELOPMENT

Three events present incredible economic opportunity drivers for the West Carson community:

- »» Redevelopment and expansion of the Harbor-UCLA Medical Center campus that will nurture emerging biomedical cluster.
- »» The extension of the Metro Silver Line to San Pedro (and potential station relocation) that could bring more commuters and increase demand for new housing.
- »» Potential synergies with new mixed-use developments underway in the City of Carson.

The intent of this Specific Plan is to leverage these public and private investments by creating employment and housing development opportunities in the plan area to serve the greater West Carson community.

The following strategies and recommendations offer a road map to help West Carson achieve its full economic development potential. This includes capitalizing on the district's existing strengths, evaluating key policies already in place, and refining the approach to meet the unique needs of West Carson.

Strategies and initiatives targeted to the Specific Plan Area include:

1. Support the health and biotechnology industries with logistical support and new funding tools.
2. Preserve employment-rich land uses in industrial flex zones while allowing for new residential where appropriate.
3. Improve perceptions of public safety to help attract new investment.

1. Support Health and Biotech Industries

The County already has a specific economic development policy to support the healthcare and biomed industries:

1.1 Economic Development Element

The General Plan Economic Development Element was designed to help the unincorporated County expand its economy, provide jobs, support businesses, and achieve revenue growth.

Policy ED 1.4: Encourage the expansion and retention of targeted industries and other growth economic sectors, such as the entertainment industry, aerospace industry, agriculture, transportation/logistics, healthcare, biomed/biotech, hospitality, and tourism.

The West Carson community has strong ties to at least two of the high-growth sectors targeted for expansion in the above policy—healthcare and biomed/biotech.

The County's active involvement in the redevelopment of Harbor-UCLA Medical Center indicates that nurturing the healthcare industry in West Carson remains a significant priority. With respect to biomed and biotechnology, the economic development element cites the lack of high-tech industrial or office space as a major impediment to growing this well-paying cluster. To this end, the inclusion of 200,000 square feet of new research space for biomedical research at the Harbor-UCLA campus should help establish momentum.

A feasibility assessment for developing a biotechnology partnership plan, released in 2011, confirmed this need. Among the findings—Enhancing the feasibility of a thriving biotech field would require seeding the initial development of multi-tenant commercial bioscience facilities "as part of an integrated medical district development approach at several of the public hospital campuses, especially those which have research partnerships in place." While LA BioMed currently has its own plans for expansion, the County seeks to amplify these efforts further by helping support a new Biotech Park at the western edge of Harbor-UCLA Medical Center through a public/private partnership to develop an additional 250,000 square feet of biotech park space.

1.2 County Economic Development Trust Fund (CEDTF)

In an effort to promote economic development and increase County household income through job attraction and retention, the Board voted to create the County Economic Development Trust Fund (CEDTF). The CEDTF uses one-time funds from the sale of redevelopment agency assets to target catalytic projects, support small businesses and manufacturers, invest in emerging sectors, and promote exports. Specifically, the CEDTF includes a revolving loan fund targeted to the bioscience industry.

The Bioscience Revolving Loan Fund would support the County's cluster-based development strategy by supporting organizations that represent the bioscience cluster, including emerging businesses in the area surrounding LA BioMed. The fund would start with a \$1 million initial balance and grow to \$4 million in fiscal year 2021–22; it would also include a grant program to fund best-practice cluster development (\$125,000 initially, increasing to \$2 million by 2021–22). Initial funds could be used to help attract and provide technical assistance to biomed firms and encourage campus success.

1.3 Other Strategies

In addition to the implementation plan that the County is currently undertaking, there are additional strategies for promoting the success of the healthcare and biomed industries in West Carson.

1.3.1 Promote Coordination with Local Universities and Colleges

A key component to the success of the biomed industry will be collaboration with local universities and colleges. Mission Bay in San Francisco was able to create a successful biotech hub due to its co- location with UCSF. The inclusion of UCLA on the campus is critical, but the County should also facilitate collaboration with CalTech and USC because its students and faculty will be critical allies.

1.3.2 *Improve Infrastructure Related to the Biomed Industry*

The County should ensure that broadband and/or other relevant infrastructure is available at competitive prices and levels of service. The County should consider making the unused capacity available to businesses at a reasonable cost.

2. Preserve High-Value Industrial with Employment-Rich Uses

Developers continue to pressure the County to convert industrial-zoned land into residential uses. With a housing affordability crisis and general lack of supply, the idea of creating more housing on sites with aging industrial uses can be tempting. However, it is critical to the economic health of the County and West Carson that any conversion of industrial land be done strategically to avoid incompatible use adjacencies and preserve job-generating uses. The County's employment district classifications (below) can help ensure a nuanced approach that supports a gradual transition to other uses where appropriate.

2.1 Economic Development Element

Policy ED 2.1: Protect industrial lands, especially within Employment Protection Districts, from conversion to non-industrial uses.

The General Plan analyzes employment districts throughout the County and organizes them into two general categories.

»» Employment Protection Districts

»» Industrial Flex Districts

Employment Protection Districts, with high employment densities and heavy industrial activity, feature policies that discourage land conversion to more profitable short-term uses. Such conversions are increasingly common as rising home prices incentivize developers to seek zone changes to pursue greater returns on cost.

Industrial Flex Districts are slightly less restrictive; they preserve existing employment uses while simultaneously allowing for non-industrial uses, where appropriate.

The General Plan does not call for any Employment Protection Districts in West Carson, but it does designate at least four Industrial Flex Districts.

The Specific Plan area itself has one Industrial Flex District along Vermont Avenue just south of Carson Street. Employment generators there vary widely and include music production and metal fabrication facilities, silk-screening and embroidered apparel, medical supply outlets, and freight-forwarding brokerages.

However, the General Plan also notes that the Specific Plan area has seen a significant amount of large-scale residential development over the past several years. The remaining Industrial Flex District represents a very small portion of the Specific Plan area. Some residents would be attracted to residential opportunities in proximity to a major employment node—allowing for shortened commute times and less spending on transportation—the Flex District therefore allows for conversion to other uses over time, depending on market demand. These districts should retain the flexibility to preserve existing uses—including light industrial and/or uses related to the medical center—but still allow for other development as the market dictates.

2.2 County Economic Development Program

The County has made significant strides toward developing a comprehensive economic development program over the past several years. The CEDTF mentioned above addresses industrial-zoned land as well as bioscience space. Articulating a holistic strategy was made especially urgent by the demise of Redevelopment and the elimination of the Enterprise Zone program. A dedicated implementation plan ("Economic Development Implementation Roadmap") provides strategies for achieving its four main goals:

- Supporting small business and manufacturers
- Investing in emerging sectors
- Targeting catalytic projects
- Attracting foreign investment and promoting exports

Specific programs and funding mechanisms that may be applicable to West Carson include the following:¹

2.3 Manufacturing Business Loan Program

The Manufacturing Business Loan Program would be targeted toward manufacturers in Employment Protection Districts and Industrial Flex Districts. To qualified applicants, it would provide \$100,000 to \$500,000 in below market loans (fixed at three percent) for land acquisition, construction, working capital, machinery and equipment, and debt refinancing.

¹ <http://file.lacounty.gov/bos/supdocs/98389.pdf>.

2.4 Catalytic Development Fund

The proposed Catalytic Development Fund would promote infill and brownfield development along designated transit corridors like West Carson. This fund would leverage new market tax credits and conventional bank financing to rehabilitate and upgrade industrial properties to house industries targeted by the County for development. The program parameters are currently under development.

2.5 Façade Improvement Program

Improving existing façades is a relatively inexpensive means of updating the local streetscape to attract new tenants and activate the streetscape. The County has an existing Façade Improvement Program that could benefit the West Carson community. Under this program, the Community Development Commission (CDC) helps revitalize older commercial corridors by way of grants and technical assistance for property owners to design, construct, and otherwise improve their storefront facades.

Although West Carson is not currently targeted under this program, the County may want to consider West Carson for this program in the future. As discussed earlier, the Study Area has seen no new retail development in at least 10 years. Meanwhile, the principal shopping center, located north of West Carson Street between Normandie Avenue and South Vermont Avenue, is showing signs of age, with a strip mall configuration dating back to 1962.

3. Address Public Safety Issues Related to Homelessness

West Carson residents and other local stakeholders cite homelessness as a significant obstacle to revitalizing the Study Area. Already at crisis levels across the County, homelessness in West Carson is exacerbated by mental health patients being discharged from Harbor-UCLA. According to law enforcement, Harbor-UCLA is an emergency center for mental health patients, who are often discharged without access to transportation or transitional housing. This results in a large mentally ill homeless population surrounding the medical center campus, and few residents feel safe enough to walk along West 220th Street at its southern border.²

² Per kick-off meeting interview with County Sheriff.

The following strategies should be implemented to address issues related to homelessness in West Carson:

3.1 New Public Health Initiatives and Homeless Strategies

The County has the second largest publicly run healthcare system in the nation. The County Department of Health Services (Health Services) currently operates four public hospitals, including Harbor-UCLA Medical Center. The County also provides mental health services through the Department of Mental Health (Mental Health), the largest municipal mental health department in the nation.

Coordination among the myriad departments and agencies that treat mentally ill homeless patients in the area has been a challenge, but may soon become easier. The Board voted in August 2015 to consolidate three of its health agencies under a single director. Some of the goals for this change are to improve information sharing between departments, lessen overcrowding in hospitals' psychiatric wards, and reduce homelessness among people with medical problems.

3.2 New Funding Sources and Strategies

New dedicated funding sources at the County level may also help alleviate some of the issues associated with homelessness. In September 2015, the Board voted to pledge \$51 million in surplus revenue to expand funding for homeless programs and services. In February 2016, the Board released a detailed strategy for combating homelessness, which brought together 25 County departments, 30 cities, and dozens of community partners

Strategies from this Initiative that might be particularly beneficial in the case of West Carson include the following.

Strategy A2: Set of Discharge Planning Guidelines. Strategy A2 articulates a set of discharge planning guidelines for at-risk patients, bringing together the Departments of Health Services, Children and Family Services (Children and Family Services), Mental Health, Public Health, the Sheriff's Department (Sheriff's), Probation (Probation), the Veterans Administration, the Los Angeles Homeless Services Authority (LAHSA), the Hospital Association of Southern California, and others. The goal of this program is to prevent individuals from becoming homeless upon discharge.

Strategy B7: Interim/Bridge Housing for Those Exiting Institutions. Strategy B7 would address a critical gap in care for patients discharged from mental health care at Harbor-UCLA. It would bring together the

Homeless Services Authority, Health Services, Mental Health, Probation, Children and Family Services, and Sheriff's to develop and implement a plan to increase the interim/bridge housing stock across the County, including identification of funding that can be used to support the increase. Proposed zoning changes—particularly in the MU2 Zone—in the Specific Plan may also help support this, allowing bridge housing to be developed near the hospital at appropriate densities.

Strategy E9: Discharge Data Tracking System. There is currently no consistent method of identifying and tracking current and potentially homeless persons in jails, hospitals, the foster care system, or other public systems that may discharge people into homelessness. To the extent permitted by law, such identification is crucial to the implementation of effective and appropriate discharge planning.

3.3 Other Revitalization Strategies

3.3.1 *Inter-jurisdictional Coordination*

West Carson's central location borders a number of jurisdictions and municipalities, each with its own set of strategies for furthering economic development. Familiarity with these should inform any strategy for West Carson, which should be effectively coordinated to amplify their positive effects.

In Carson, for example, future plans for the 157-acre development site once proposed for an NFL stadium could have a significant impact on the West Carson community. The city has completed much of the environmental cleanup needed to remediate the property and recently entered into exclusive negotiations with Macerich Real Estate company to develop a large, regional-destination retail complex with a high-end fashion outlet. Such a large project may affect the viability of new local retail and increase the value of surrounding property for potential developers.

Closer to the Study Area, Carson is currently renovating Carson Street with a \$20 million streetscape program, extending from the Harbor Freeway (Interstate-110) east to the downtown. Meanwhile, housing developers in particular have been making significant investments along the corridor, intensifying Carson Street with vertical and horizontal mixed use. Outreach to these developers could provide helpful strategies for steering appropriate development to the West Carson community.

3.3.2 *Improve Bicycle/Pedestrian Connections to Transit*

Improving connectivity for cyclists and pedestrians between the Specific Plan area and the Metro Silver Line will increase nonmotorized activity along commercial corridors. Increased bike and foot traffic generally improves public safety and provides a critical mass to support more retail uses that can serve the community and the Harbor-UCLA Medical Center campus, making the campus more desirable for new tenants. Connectivity improvements identified in the Specific Plan should be implemented to assist with revitalization and increased retail strategies.

07

IMPLEMENTATION AND
ADMINISTRATION

IMPLEMENTATION AND ADMINISTRATION

GENERAL ADMINISTRATION

Specific Plan Adoption

The Specific Plan shall be adopted by ordinance according to the procedures established in Division 9 of Title 22 of the County Code.

Amendment to the Specific Plan

A Specific Plan Amendment may be initiated by the Board, the Commission, or upon application by a property owner or their designated representative. An amendment to the Specific Plan shall be processed in accordance with Division 9 of Title 22 of the County Code.

Enforcement

The Director is responsible for the overall administration and enforcement of the provisions of this Specific Plan.

Applicability

The Specific Plan shall apply to all new development projects for which a complete application has been filed on or after of the ordinance establishing this Specific Plan. Complete applications that were filed before the effective date of this Specific Plan shall comply with the regulations and applicable Title 22 provisions that were in effect at the time that the respective complete applications were filed.

Severability

If any provision of this Specific Plan or the application thereof to any person or circumstance is held to be invalid by a court of competent jurisdiction, such invalidity shall not affect the other Specific Plan provisions, clauses, or applications thereof which can be implemented without the invalid provision, clause, or application, and to this end the provisions and clauses of this Specific Plan are declared to be severable.

Relationship to Title 22

The provisions contained in the Specific Plan shall be considered to be in combination with the other applicable provisions of Title 22. Where provisions of this Specific Plan conflict with any other provision of Title 22, the Specific Plan shall govern. Where provisions of the Regulating Code in this Plan are silent, the other applicable provisions of Title 22 (Planning and Zoning) of the County Code shall govern.

Other requirements may apply. No provision in this Specific Plan shall eliminate the need for obtaining any other permit, approval, or entitlement required by the County, or any other applicable special district or agency, or the State or federal government or their agencies, or otherwise comply with applicable State and federal regulations or laws.

Interpretation

The Director or designee has the authority to interpret the intent of this Specific Plan if ambiguity arises concerning the meaning or appropriate application of the provisions of the plan. In so doing, the Director shall consider the following factors:

- »» The case is similar to previous interpretations of similar provisions.
- »» The interpretation reflects satisfactorily the vision, intent and purpose of the Specific Plan.
- »» The resulting project is consistent with the General Plan.
- »» The decision constitutes sound precedent for other similar situations.

Such interpretations may be appealed to the Commission and ultimately the Board in accordance with the appeal procedures of Division 9 of Title 22 of the County Code.

Nonconforming Uses

The nonconforming use and structure provisions in Section 22.172 of Title 22 of the County Code shall apply to all uses and structures in the area governed by the Specific Plan that were legally established or built prior to the effective date of this Specific Plan.

Specific Plan EIR Mitigation Monitoring

Pursuant to California Public Resources Code (PRC) section 21091.6, a summary of conditions of project approval shall be prepared to mitigate or avoid significant effects on the environment. The Environmental Impact Report (EIR) for the Specific Plan includes a Mitigation Monitoring Report that was completed prior to adoption of this Specific Plan.

Environmental Clearance

The EIR is primarily a source of environmental information and disclosure for the County, the lead agency for the project. The EIR describes the potential impacts from the adoption of the Specific Plan. Subsequent development projects in the Specific Plan are anticipated as it builds out. The EIR has been prepared as a Program EIR (PEIR), as defined by section 15168 of the California Environmental Quality Act (CEQA) Guidelines, and subsequent projects that are within the scope of this EIR may be subject to a more limited environmental review process, as guided by the provisions of CEQA.

Use of a PEIR provides the County with the opportunity to consider broad policy alternatives and program-wide mitigation measures. It provides the County with greater flexibility to address project-specific and cumulative environmental impacts on a comprehensive basis. Agencies generally prepare PEIRs for programs or a series of related actions that are linked geographically; are logical parts of a chain of contemplated events, rules, regulations, or plans that govern the conduct of a continuing program; or are individual activities carried out under the same authority and having generally similar environmental effects that can be mitigated in similar ways.

Tiering for future projects consistent with the Specific Plan and EIR

2015 CEQA Guidelines § 15183 (excerpt):

(a) CEQA mandates that projects which are consistent with the development density established by existing zoning, community plan, or general plan policies for which an EIR was certified shall not require additional environmental review, except as might be necessary to examine whether there are project-specific significant effects which are peculiar to the project or its site. This streamlines the review of such projects and reduces the need to prepare repetitive environmental studies.

(b) In approving a project meeting the requirements of this section, a public agency shall limit its examination of environmental effects to those which the agency determines, in an initial study or other analysis:

(1) Are peculiar to the project or the parcel on which the project would be located,

(2) Were not analyzed as significant effects in a prior EIR on the zoning action, general plan, or community plan, with which the project is consistent,

(3) Are potentially significant off-site impacts and cumulative impacts which were not discussed in the prior EIR prepared for the general plan, community plan, or zoning action, or

(4) Are previously identified significant effects which, as a result of substantial new information which was not known at the time the EIR was certified, are determined to have a more severe adverse impact than discussed in the prior EIR.

(c) If an impact is not peculiar to the parcel or to the project, has been addressed as a significant effect in the prior EIR, or can be substantially mitigated by the imposition of uniformly applied development policies or standards, as contemplated by subdivision (e) below, then an additional EIR need not be prepared for the project solely on the basis of that impact.

This approach is consistent with the tiering provision in PRC section 21083.3 and CEQA Guidelines section 15183 for "Projects Consistent with a Community Plan, General Plan or Zoning." This tiering opportunity is only available for plans (e.g., specific plan) for which an EIR has been prepared.

Note that tiering under these provisions will require environmental review and documentation to substantiate that a subsequent project does not result in any new potentially significant impacts. Such review (under PRC section 21083.3/15083) could be documented in the form of an Initial Study to ensure "topic by topic" review and substantiation. Once consistency has been substantiated and review shows that the project would not result in new significant impacts, neither a mitigated negative declaration nor an EIR would be required.

Additionally, no formal public review would be required. Projects may also be exempt from CEQA review pursuant to other sections of CEQA (e.g., exemptions for residential infill projects, statutory exemptions, or categorical exemptions) depending on the size of the project and type of development. The type of CEQA review needed for each project will be determined by County staff during their review of the type of project or development proposed.

In addition to a more limited review process, infill and transit-oriented infill projects may qualify for streamlined environmental review. CEQA Guidelines section 15183.3 allows eligible projects to streamline the environmental review process by limiting the topics subject to review at the project level. California Public Resources Code sections 21099 and 21155.4 also limit review of environmental topics and exempt certain types of projects.

PROJECT REVIEW PROCEDURES

Applications

No new development or use shall be established under the Specific Plan, and no grading or building permits shall be issued for these uses, until an application has been approved for the required permit type listed in the Use Regulation tables in section 3.4 and pursuant to the applicable procedures set forth below.

Ministerial Site Plan Review

- 1. Review Authority.** For uses that are permitted in the respective Use Regulation table, the Director shall have the authority to review projects subject to a MSPR for compliance with the Specific Plan and other provisions of Title 22 of the County Code.
- 2. Application Requirements.** A Ministerial Site Plan Review application shall include all information required by the form provided by the Department, and payment of the required fee established in Title 22 of the County Code.
- 3. Determination.** If the project complies with the provisions of the Specific Plan and other applicable provisions of Title 22, the Director shall grant the MSPR approval. Otherwise, the Director shall deny the application for a Ministerial Site Plan approval.

Minor Modifications

Minor modifications, as defined herein, shall not require a Specific Plan Amendment, but will be subject to the following "substantial conformance" determination.

1. **Review Authority.** The Hearing Officer shall have the authority to review projects requesting a modification to the development standards identified in subsection 4 below, for substantial compliance with the applicable requirements of the Specific Plan and other provisions of Title 22 (Planning and Zoning) of the County Code.
2. **Application Requirements.** A modification application shall include all information required by the form provided by the Department, and the payment of the required fee.
3. **Procedures.** A modification request shall be subject to the public hearing procedures and requirements set forth in Title 22 (Planning and Zoning) of the County Code.
4. **Determination.** If the Hearing Officer determines that the request for a modification is consistent with the principles and standards of the County Code, the Hearing Officer may approve the modification. Notwithstanding the foregoing, only the following development standards may be modified:

Table 7.1 Modifications

REQUIREMENT	MAXIMUM MODIFICATIONS
Setback	10%
Building Height	10%
Building Size/Massing	15%
Open Space Area/Landscaping	15%
Sign Height/Width/Area	10%
Parking Spaces	10%
Loading Areas	May be modified or waived

5. **Appeals.** The decision of the Hearing Officer may be appealed or called up for review pursuant to the procedures and requirements of Division 9 of Title 22 of the County Code.
6. **Revisions to Modification.** Revisions to a modification grant may be approved by the Director if the revisions do not affect the intent of the original approval. Revisions that would deviate from the intent of the original approval shall require approval of a new modification.

Specific Plan Modification Review

1. **Review Authority.** The Hearing Officer shall have the authority to review projects subject to a Specific Plan Modification Review for substantial compliance with the applicable standards and implementing options of this Specific Plan and other applicable provisions of Title 22 (Planning and Zoning) of the County Code.
2. **Application Requirements.** A Specific Plan Modification Review application shall include all information required by the form provided by the Department, and the payment of the fee set forth in Division 9 of Title 22 of the County Code.
3. **Procedures.** A Specific Plan Modification Review shall be subject to the public hearing procedures and requirements set forth in Division 9 of Title 22 of the County Code.
4. **Burden of Proof.** The applicant shall substantiate to the satisfaction of the Hearing Officer that:
 - a. Approval of the project conforms with the applicable provisions of this Specific Plan and other applicable provisions of Title 22 (Planning and Zoning) of the County Code.
 - b. Approval of the project is in the interest of the public health, safety, and general welfare.
 - c. Site layout, open space, orientation and location of buildings, vehicular access, circulation and parking, setbacks, heights, and walls and fences are designed to provide a desirable environment within a unifying context that encourages increased pedestrian activity and promotes compatibility among neighboring land uses.
 - d. Architectural character, scale, quality of design, building materials, colors, screening of exterior appurtenances, and signs are designed to ensure compatibility of the development with the Specific Plan and the character of the neighborhood.
 - e. Project landscaping, including its location, type, size, color, texture, and coverage of plant materials at the time of planting are designed and developed to provide visual interest, complement buildings and structures, and provide an attractive environment for the public. The project landscaping shall also include measures to provide for irrigation, maintenance, and protection of landscaped areas.

- f. Parking areas are designed and developed to buffer surrounding land uses, complement pedestrian-oriented development, enhance the environmental quality of the site such as to minimize stormwater run-off and the heat-island effect, and achieve a safe, efficient, and harmonious development.
 - g. Lighting and lighting fixtures are designed to complement buildings, are of appropriate scale, avoid creating glare, and provide adequate light over walkways and parking areas to foster pedestrian safety.
5. **Appeals.** The decision of the Hearing Officer for the Substantial Conformance Review may be appealed or called up for review pursuant to the procedures and requirements of Division 9 of Title 22 of the County Code.
6. **Revisions to Specific Plan Modification Review.** Revisions to the Substantial Conformance Review may be approved by the Director if the revisions do not affect the intent of the original approval. Revisions that would deviate from the intent of the original approval shall require the approval of a new Specific Plan Modification Review.

Conditional Use Permit

When a conditional use permit is required under this Specific Plan or otherwise required under Title 22 (Planning and Zoning) of the County Code, the review procedures for a conditional use permit shall be the same as those prescribed in Division 9 of Title 22 of the County Code.

IMPLEMENTATION AND FINANCING STRATEGY

There are a number of grant, loan, and value capture funding mechanisms that could finance the infrastructure and community benefits identified in this Specific Plan. These resources are detailed in this section.

LOCAL TAX INCREMENT AND ASSESSMENT DISTRICTS

Enhanced Infrastructure Financing District

The Enhanced Infrastructure Financing District (EIFD) is a new funding mechanism that was signed into law on September 2014. Its main purpose is to finance a wide array of infrastructure projects with "communitywide significance," from parks and brownfield remediation to transit improvements and affordable housing.

An EIFD can be created by a city, county, or joint powers authority to fund specific infrastructure and economic development projects as outlined in the financing plan. EIFDs can also leverage multiple funding streams to achieve these goals—including a tax increment (if approved by voters), assessment revenues, fees, and other sources such as State and federal grants.

EIFDs share a number of similarities to Community Revitalization Investment Authorities (CRIAs)—another funding mechanism recently authorized in California to help carry out revitalization activities. However, a CRIA must operate within an investment area that meets the State's criteria of a disadvantaged community (generally, the district must consist of households making no more than 80 percent of the State's median household income). Therefore, an EIFD would be more viable in West Carson's case than a CRIA, with the census-designated place's (CDP) higher incomes and below average unemployment rates.

Unlike a CRIA, however, an EIFD can be established without voter approval and does not require an affordable housing set-aside. EIFDs may not issue debt without a 55 percent vote of the district's registered voters, nor can revenues be used to fund ongoing maintenance and operations. Because an EIFD's strength lies in the power of tax increment financing, Harbor-UCLA's tax exempt status would be a constraint in West Carson's case; none of the assessed improvements associated with the medical campus could be applied toward the increment. Nonetheless, if the Specific Plan were to jumpstart a new wave of investment along West Carson and Vermont, those revenues could be tapped for any number of

improvements, including transit station improvements, water and sewer infrastructure, pedestrian connectivity, and other streetscape amenities.

Special Assessment Districts

Special Assessment Districts can be used to fund any improvement that provides a "direct and special" benefit to the assessed property. By this definition, improvements like the recommended medians, sidewalks, lighting, art, and benches that improve connectivity, as well as safety improvements like private security, can be funded via Special Assessment, while "general" benefits like schools may not.

There are two primary challenges in establishing Special Assessment Districts, particularly for those in already developed areas. The first is that total property taxes can only increase a certain amount before new development is disadvantaged relative to properties not subject to an assessment. The second challenge is that assessment districts require a majority vote of property owners weighted by property value to pass. All the affected properties must stand to benefit from that particular improvement, and no assessment can exceed the "reasonable cost" of its special benefit to that parcel. One benefit to forming a Special Assessment District in West Carson's case, however, is that non-profit uses like Harbor-UCLA would *not* be exempt from paying dues, thereby substantially increasing the available revenue stream.

Business Improvement District

A Business Improvement District (BID) is a common type of Special Assessment District that assesses business and/or property owners to fund maintenance, marketing, and other activities, including additional public services or improvements. If such a district were to be formed in West Carson, funding could be applied toward enhanced sanitation and cleaning as well as other streetscape and pedestrian improvements.

The County would need to undertake extensive outreach to the property owners to educate them on the benefits and obtain majority support before moving forward with formation. If support can be obtained, a BID is a powerful tool for raising funds to provide enhancements to the area, but cannot be used to issue bonds.

Under the California Parking and Business Improvement Area Law of 1989 and Property and Business Improvement District Law of 1994, a district can be established via a County resolution of intent to form a BID. If a majority of property owners do not protest the resolution during a subsequent public hearing, an advisory board would be

appointed. Once formed, a special assessment can be charged to commercial property or business owners for an amount proportional to the benefits they will receive.¹

Landscape and Lighting District

Like a BID, a Landscape and Lighting Assessment District (LLAD) is another type of Special Assessment District that could be applied in West Carson to fund new street and pedestrian lights, landscaping, parkways, medians, and other amenities, and require benefits to accrue proportionately to the assessed properties. LLADs are more flexible than BIDs in that they can issue bonds and require a simple majority of property owners for formation. The Study Area is already encompassed by one such LLAD that provides funding to maintain street lights. There have been no LLADs established by the County for amenities like pedestrian lighting; forming such a district in West Carson would require creating a new Special Assessment District dedicated to that purpose.

Given the same barrier to entry as a BID, formation of an LLAD that can issue bonds for the commercial areas that are supported by and include the Harbor-UCLA Medical Center is a better approach for raising local funds than the formation of a BID.

OTHER LOCAL SOURCES OF FUNDS

Development Impact Fees

Development impact fees are another potential funding source for affordable housing, parks, and recreational open space. These fees, paid by new residential and commercial development projects, must only be used to pay for improvements that can be demonstrated to serve new residents and businesses (from new development), but these fees can be combined with other funding sources to fund a project that serves both new and existing residents or businesses. A nexus study—which calculates the new increment of development, estimates the portion of an improvement project attributable to that increment of growth, and allocates the fee among the new development projects by land use—is required by State law for implementation. Additional impact fees, such as a transportation and traffic impact fee, could be considered as a means to fund additional improvements that enhance mobility.

¹ BID assessments cannot be charged to residential properties or land zoned for agriculture.

Revenue Bonds

Public activities that are revenue generating and create sufficient cash flow to cover operating costs and debt service can potentially issue tax-free municipal debt to cover the cost of capital improvements. A common example of this is revenue bonds for parking garage construction where there is pay parking.

General Obligation Bonds and Other Public Debt

New commercial and lodging projects could generate significant new sales tax and transit occupancy (lodging) tax revenues that will flow into the County's General Fund. This new money could be used to finance debt service on tax-exempt debt obligations so that existing activities provided through the General Fund are not impacted. Such a General Obligation bond, however, requires a two-thirds vote of local residents (except for educational facilities) to approve. Alternatively, for facilities that can serve as collateral for debt, certificates of participation are a public finance technique that do not require voter approval.

REGIONAL AND STATE SOURCES OF FUNDS

Affordable Housing and Sustainable Communities

The 2006 Global Warming Solutions Acts (AB 32) established a cap and trade system in California. The system establishes quarterly auctions of carbon allowances, whose proceeds are deposited into a Greenhouse Gas Reduction Fund. Using revenue from this fund, the Strategic Growth Council administers the Affordable Housing and Sustainable Communities (AHSC) program, which funds land-use, housing, transportation, and land preservation projects to support infill and compact development that reduces greenhouse gas emissions.

Projects that can show the Strategic Growth Council that they reduce vehicle miles traveled by locating near transit are most competitive for funds. However, the market for carbon emissions has shown itself to be relatively unstable. Rather than trading emission allowances, companies are reducing emissions. While this is certainly beneficial to the environment, it means that the future of this funding source is uncertain.

Infrastructure State Revolving Loan Fund

The California Infrastructure and Economic Development Bank (I-Bank) loans money for infrastructure projects around the State. The I-Bank is the State's general purpose financing authority that finances public infrastructure and private development projects that promote economic development and revitalize communities.

Eligible project categories in West Carson could include the rehabilitation of streets and highways, water supply and flood control, new parks and recreational facilities, expanded public transit, public safety features, and power and communications facilities.

Recent loan recipients in Southern California have included the City of San Gabriel, which borrowed \$3.8 million at 3.5 percent interest to upgrade, reconstruct, and rehabilitate its public streets.

Integrated Regional Water Management Grant

Using funds from Proposition 1, the water bond passed by California voters in 2014, the California Department of Water Resources will award over \$510 million in Integrated Regional Water Management Grants for planning and implementation projects throughout the State, with \$98 million specifically allocated to the Los Angeles region starting in 2016. Projects can include stormwater capture, water reuse, and other green streets measures.

Eligible projects for West Carson could include stormwater capture, water reuse, providing new open space, and other green streets measures.

Caltrans Active Transportation Program

Caltrans's Active Transportation Program (ATP) consolidates various State and federal transportation programs, including the federal Transportation Alternatives Set-Aside (TA Set-Aside), Bicycle Transportation Account, and State SRTS. Approximately \$122 million were awarded through the fiscal year 2016–2017 ATP and distributed to three categories: statewide competition (50 percent), MPO projects for regions with 200,000 or more residents (40 percent), and small urban and rural regions with populations of less than 200,000 (10 percent). In 2016–2017, ATP funds came from the following sources.²

Source	FY 2016–2017 Funding
State Funds	\$34,200,000
Surface Transportation Block Grant Program Set-Aside for Transportation Alternatives (Federal)	\$65,455,000
Other Federal	\$19,950,000

Although some programs request only State funds, most include a combination of funding from all available sources.

The goal of ATP is to encourage increased use of active modes of transportation, including walking and biking, as well as the safety and mobility of non-motorized users. Eligible projects in West Carson could include developing new bikeways and walkways, including a pedestrian bridge, and adding new landscaping, traffic control devices, and enhanced street lighting.

Southern California Association of Governments (SCAG) administers the regional portion of the ATP and relies on the California Transportation Commission's Call for Proposals process to select the capital projects to be funded through the regional program.

Funding for the West Carson Community Bikeways plan was awarded through this program in 2015, with \$425,000 to be allocated for bike improvements through 2019.³

² http://www.dot.ca.gov/hq/transprog/ctcliaison/2016/0516/22_4.12_orig_PPT.pdf.

³ http://www.catc.ca.gov/programs/ATP/2015/2015_ATP_Staff_Recommendation-Statewide_Small_Urban-Rural.pdf.

FEDERAL SOURCES

Federal Transportation Sources

The Fixing America's Surface Transportation (FAST) Act was signed into law in December 2015, and authorizes federal funding for a wide array of transit improvements through fiscal year 2020. It includes a number of potential funding sources that could benefit the West Carson TOD Study Area, including Capital Investment Grants, Urbanized Area Formula Grants, and Surface Transportation Block Grant Programs. These funds are administered through Caltrans's ATP program, described above.

The FAST Act also established a new National Surface Transportation and Innovative Finance Bureau within the United States Department of Transportation to serve as a consolidated resource for providing local government agencies with federal funding, financing, and technical assistance.

Grants: Surface Transportation Block Grant Program

The Surface Transportation Block Grant Program (STBG) is one of the primary flexible funding sources available for transit at the local level. These funds may be used for a wide array of transit corridor capital improvements, including public transportation capital improvements, fringe and corridor parking facilities, bicycle and pedestrian facilities, and intercity or intracity bus terminals and bus facilities.

STBG funding is apportioned directly to SCAG by the Federal Highway Administration. The funding is allocated by the State of California, with a nonfederal funding match requirement of 11.47 percent.

With respect to planning, STBG funds can be used for surface transportation planning activities, wetland mitigation, transit research and development, and environmental analysis. Other eligible projects under STBG include transit safety improvements and most transportation control measures. STBG funds are distributed in a state based on population and other programmatic categories.

Transportation Alternatives Set-Aside

Within the STBG funding above is a set amount called the Transportation Alternatives "Set-Aside" (formerly Transportation Alternatives Program, or TAP). The TA Set-Aside finances projects defined as "transportation alternatives," including on-road and off-road pedestrian and bicycle facilities, recreational programs, infrastructure projects for improving "nondriver" access to public transportation, enhanced mobility, community

improvement activities, and environmental mitigation. It also funds activities related to the former SRTS program, which helped fund the construction of infrastructure-related projects on public roads and bicycle-pedestrian pathways near schools. A funding commitment in the vicinity of Meyler Street Elementary School or Van Deene Avenue Elementary School, for example, could finance sidewalk improvements, traffic calming and speed reduction improvements, pedestrian and bicycle crossing improvements and bridges, on-street bicycle facilities, off-street bicycle and pedestrian facilities, secure bicycle parking facilities, and traffic diversion improvements anywhere within two miles of the school.

State Departments of Transportation (DOTs) and MPOs are not eligible entities as defined under the statute, and therefore are not eligible project sponsors for TA Set-Aside funds. However, such agencies may partner with an eligible entity project sponsor to carry out a project.

Economic Adjustment/Revolving Loan Fund

The Economic Development Administration, a bureau in the United States Department of Commerce, administers the Economic Adjustment/Revolving Loan Fund (RLF), which assists State and local entities in creating and implementing strategies to improve local economic conditions in areas that have experienced structural change in their economic bases.

The RLF provides capital to help small businesses and entrepreneurs expand production capabilities with gap financing. Businesses in West Carson looking for assistance would contact the local EDA Regional Office (Pacific Coast Regional) for information on interest rates. Maximum loans are \$650,000 per borrower with terms of seven years for working capital, 15 years for fixed assets, and 20 years for real estate. Start-up firms in the Biotech Park, as well as those looking to expand, could benefit from use of this loan program.





County of Los Angeles
WILLOWBROOK TOD SPECIFIC PLAN

August 2018



Metro

County of Los Angeles
WILLOWBROOK
TRANSIT ORIENTED DISTRICT
SPECIFIC PLAN

August 2018



Metro

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1.1 Specific Plan Location

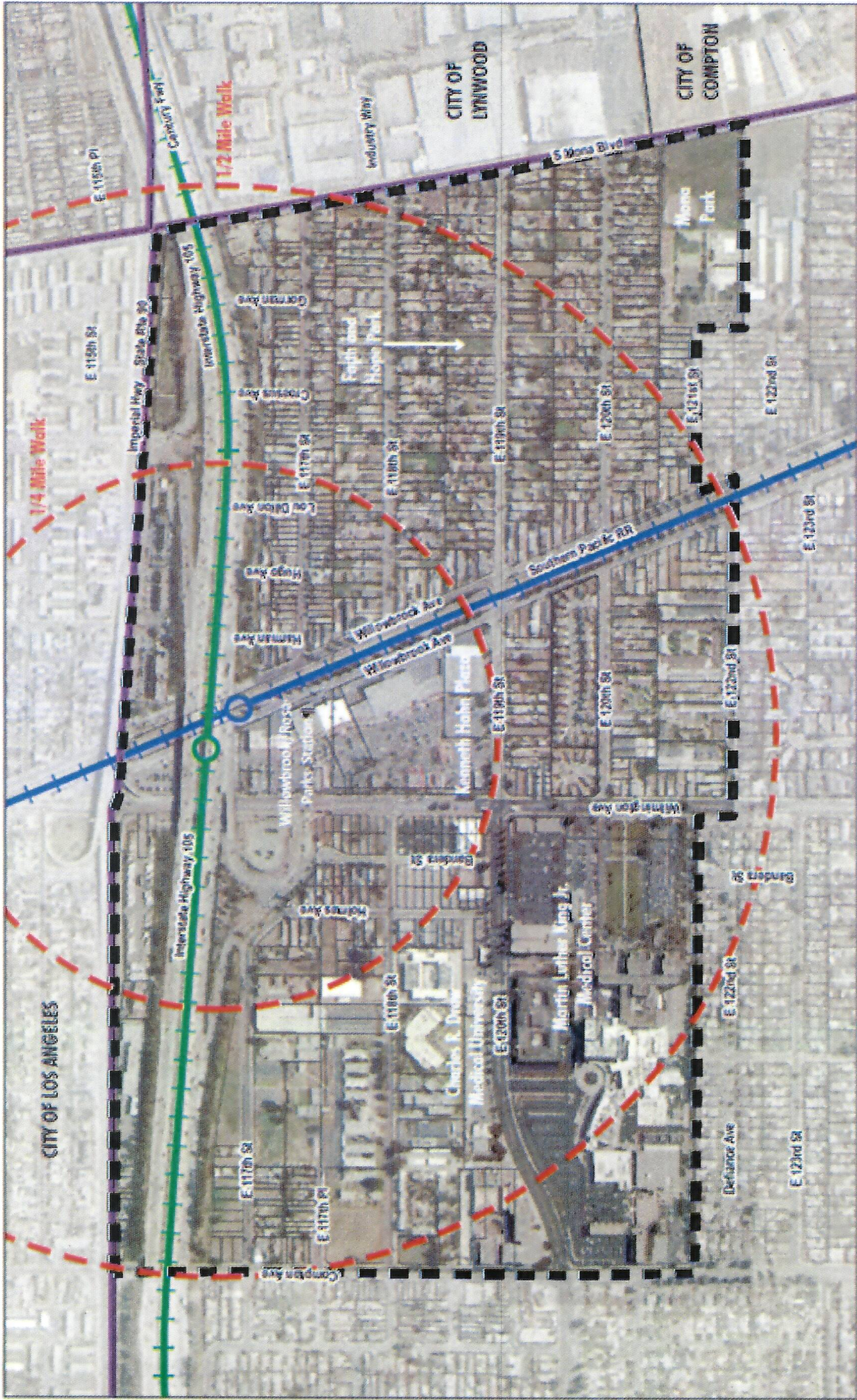
The Willowbrook Transit Oriented District (TOD) Specific Plan (Willowbrook TOD Specific Plan or Specific Plan) area (Plan Area) is located in the unincorporated community of Willowbrook within the County of Los Angeles (County). It is located along the I-105 freeway and at the junction of the Metro Blue and Green Lines. The project area is approximately ten miles south of downtown Los Angeles and is bordered by the incorporated cities of Los Angeles to the north, and Lynwood and Compton to the east.

The Willowbrook TOD Specific Plan is focused around the Willowbrook/Rosa Parks Station, which is a transfer station on the Metro Blue Line and Green Line (see Figure 1-1). The Plan Area is approximately 312 acres. Some of the key facilities that are located within the Plan Area are Martin Luther King, Jr. (MLK) Medical Center, Charles R. Drew University of Medicine and Science (CDU), Kenneth Hahn Plaza, Willowbrook Library, and MLK Center for Public Health.

What is a Specific Plan?¹

A specific plan is a tool to systematically implement the General Plan within an identified project area. Specific plans are used to ensure that multiple property owners and developers adhere to a common plan or coordinate multiple phases of a long-term development. Specific plans must further the goals and policies of the General Plan. Specific plans must be consistent with the General Plan. No local public works project may be approved; no tentative map or parcel map for which a tentative map was not required may be approved, and no zoning ordinance may be adopted or amended within an area covered by a specific plan unless it is consistent with the adopted specific plan. The authority for preparation and adoption of specific plans is set forth in the California Government Code section 65450 et seq.

¹ Chapter 6: Land Use Element, Los Angeles County General Plan.



**Figure 1-1
Willowbrook TOD Specific Plan Area**

-  County of Los Angeles
-  Willowbrook TOD Specific Plan Area
-  Metro Green Line
-  Metro Blue Line
-  1/4 Mile and 1/2 Mile radius around Willowbrook/Rosa Parks Station

WILLOWBROOK TOD SPECIFIC PLAN



1.2 Background

The Los Angeles County General Plan (General Plan) identifies TODs as priority policy areas throughout the County. To create vibrant and healthy TODs, the County envisions developing specific plans for all of the TODs that are located in the unincorporated areas.

The Willowbrook TOD Specific Plan demonstrates a clear vision for the Willowbrook community, allows development that will lead to community benefits, and facilitates economic development. The Specific Plan is accompanied by a Program Environmental Impact Report (PEIR), which was prepared for the Specific Plan. The PEIR provides environmental clearance for specified levels of development intensity and potentially streamlines future CEQA review.

1.3 Purpose

The purpose of the Willowbrook TOD Specific Plan is to allow for revitalization of the community within the project area and encouraging improvement of access to all modes of transportation, including transit, walking and bicycling. Consistent with the goals and policies outlined in the General Plan, the Willowbrook TOD Specific Plan will:

- encourage transit oriented development,
- promote active transportation,
- allow development that reduces vehicles miles traveled,
- allow development that creates community benefits, and
- streamline the environmental review process for future projects.

The Specific Plan is anticipated to facilitate development, especially residential and employment-generating uses, proximate to the Willowbrook/Rosa Parks Station. The primary objectives of the Specific Plan are to identify land use options that include mixed uses, increased housing opportunities, and neighborhood-serving retail uses. In addition, the Specific Plan is intended to foster a healthy community by improving pedestrian linkages between the Willowbrook/Rosa Parks Station, Kenneth Hahn Plaza, MLK Medical Center, CDU, future mixed use areas, and existing residential neighborhoods; as well as improving the public realm.

1.4 Relevant Studies and Plans

The following recent and ongoing planning projects have provided direction and established a foundation for the Willowbrook TOD Specific Plan:

- Willowbrook Area Access Improvements, County of Los Angeles Department of Public Works (2015-2017)
- Willowbrook Community Parks and Recreation Plan (2016)
- Los Angeles County General Plan 2035 (2015)
- Rosa Parks Station Improvements Planning, Metro (2014-2015)
- Los Angeles County Transit Oriented Districts Access Study, Southern California Association of Governments (SCAG) and County of Los Angeles (2013)
- Implementation Plan for the MLK Medical Campus and the Willowbrook Wellness Community, Initiative for Inner City Competitiveness, Next Street and Genesis LA (2013)

- Martin Luther King Medical Center Campus Master Plan & the Willowbrook MLK Wellness Community Vision (2012)
- County of Los Angeles Bicycle Master Plan (2012)
- Rosa Parks Metro Station Master Plan & Transit Oriented District (2010)
- Martin Luther King, Jr. Medical Center and Surrounding Project Area, Urban Land Institute (ULI) Technical Assistance Panel (2009)
- Imperial-Wilmington Rosa Parks Metro Blue Line Rail Station Non-Motorized Access Plan, City and County of Los Angeles, Metro, (2009)

1.5 Community Engagement

Prior to and throughout the planning process for the Willowbrook TOD Specific Plan, staff from the County of Los Angeles Department of Regional Planning (DRP) conducted community outreach and participation with the Willowbrook community. They met with and received input from community groups and stakeholders including:

- Best Start Watts-Willowbrook
- Caltrans
- CDU
- City of Los Angeles
- City of Lynwood
- Commercial and residential developers (market rate and affordable projects)
- Concerned Citizens of Willowbrook, Dr. Delores Glass
- Gabriellino Band of Mission Indians - Kizh Nation
- Housing Authority of the City of Los Angeles (HACLA)
- Kimco Realty Corporation
- Metro
- MLK Community Hospital
- Watts Century/Latino
- Watts Labor Community Action Committee

In addition, DRP staff met regularly with a County Project Task Force, comprised of representatives from various County departments, and received input and feedback about the goals and direction of the Specific Plan.

DRP staff met with representatives of the Gabrieleno Band of Mission Indians - Kizh Nation, to determine whether known tribal cultural resources are present in the Plan Area. While specific resources have not yet been identified, the Plan Area is proximate to a known early trade route that connected to the coast at San Pedro. Additionally, its characteristics of being relatively near historical water sources and hunting grounds would have been favorable to settlement. As is common in Southern California, it is possible that artifacts with tribal significance could be discovered in the Plan Area in activities that involve ground disturbance. Therefore, these activities should be undertaken with care to adequately protect potential resources.

Early Peoples

The project site is located within the territory of the native population known today as the Gabrieleno. Prior to European contact, the Gabrieleno occupied a diverse area that included the watersheds of the Los Angeles, San Gabriel, and Santa Ana rivers; the Los Angeles basin and foothills; and offshore islands of Santa Catalina, San Nicolas, and San Clemente. In ancient times the village areas adjoined and overlapped with each other, at least during the Late Prehistoric and Protohistoric periods where resources were shared by adjoining villages. The village lifestyle was focused around commerce with local and long distance trade a prominent function of the community. Resources were attained through a hunter-gatherer lifestyle while living in permanent communities located near the presence of water, a stable food supply, and some measure of protection from flooding. Houses were made of tule mats on a framework of willow poles. Basketry and steatite vessels were used rather than ceramics; ceramics became common only toward the end of the Mission Period in the nineteenth century. Villages in and around the project area exhibited similar organization and resource procurement strategies and were based on clan or lineage groups with their home base sites marked by midden deposits often with bedrock mortars and fire hearth locations. Average community populations generally ranged from 50-100 inhabitants, although larger settlements existed. Trade routes provided for daily travel, recreation, and commerce activities to occur over thousands of years. During seasonal rounds to exploit natural resources, small groups would move along trading routes within their traditional territory in search of specific plants, minerals, and animals. Their gathering strategies often left behind signs of special use sites at the locations of the resources. As well, ceremonial areas would contain buried caches of relics that were used for ceremony and would be buried and dug up every time a ceremony was conducted. These buried caches were stored in the earth much like a storage cabinet would be used in a home for protection from damage and the elements. However, there were also buried caches of relics that were never meant to be dug up and were placed in a specific orientation in the earth.

1.6 Specific Plan Goals and Policies

The goals and policies set the framework for the Willowbrook TOD Specific Plan. They serve as guidelines for decision-making and provide direction for future activities. The goals and policies were derived from input received from the community and stakeholders during the community engagement process, County Project Task Force, and DRP staff during the planning process. The seven major goals identified for the Plan Area are as follows:

1. Preserve and enhance the character of the Willowbrook community.
2. Improve the Willowbrook/Rosa Parks Station and its environs.
3. Encourage transit oriented development.
4. Provide affordable housing opportunities.
5. Promote active transportation and reduce vehicle miles traveled.
6. Improve quality of life for existing residents with improvements to the public realm.
7. Improve economic vitality and employment opportunities.

Goal 1: Preserve and enhance the character of the Willowbrook community.

Policy 1.1: Where appropriate, preserve the character of the residential neighborhoods.

Policy 1.2: Provide a mix of land uses in the Mixed Use Zones to accommodate employment, retail, and residential uses, as well as local-serving amenities.

Policy 1.3: Preserve existing neighborhood-serving retail at Kenneth Hahn Plaza.

Policy 1.4: Improve compliance with County zoning and building codes on private property by expanding code enforcement by various County Departments.

Policy 1.5: Facilitate the expansion of the MLK Medical Center and CDU campus that is compatible and sensitive to the surrounding neighborhoods.

Policy 1.6: Coordinate with CDU to integrate the planned growth of the University's campus with the surrounding community, including creating pedestrian linkages and open space connections with other area institutions and the Willowbrook/Rosa Parks Station.

Policy 1.7: Implement the concepts and ideas contained in the *MLK Medical Center Campus Master Plan & the Willowbrook MLK Wellness Community Vision* by encouraging well-designed and continuous pedestrian paths and connections between the Willowbrook/Rosa Parks Station and the employment, campus, retailing, and residential areas.

Goal 2: Improve the Willowbrook/Rosa Parks Station and its environs.

Policy 2.1: Coordinate with Metro to enhance pedestrian and bicycle connections from the Willowbrook/Rosa Parks Station to the surrounding community.

Policy 2.2: Coordinate with Metro to enhance safety and circulation between the various transit modes at the Willowbrook/Rosa Parks Station to encourage transit use.

Policy 2.3: Work with Metro and Kenneth Hahn Plaza to create better connections and access to the surrounding employment, campus, retail, and residential areas.

Goal 3: Encourage transit oriented development.

Policy 3.1: Provide a variety of housing choices within walking distance of the Willowbrook/Rosa Parks Station.

Policy 3.2: Implement mixed use zoning in targeted areas to promote employment-generating uses proximate to housing and the Willowbrook/Rosa Parks Station.

Policy 3.3: Incentivize lot consolidation where appropriate to facilitate the development of cohesive projects in Mixed Use Zones.

Goal 4: Provide affordable housing opportunities.

Policy 4.1: Preserve existing stock of affordable housing.

Policy 4.2: Promote housing affordability through diversification of housing choices (ownership, rental, single-family, multi-family) for varied income groups.

Goal 5: Promote active transportation and reduce vehicle miles traveled.

Policy 5.1: Provide a multi-modal transportation system of complete streets.

Policy 5.2: Enhance access to transit and the Willowbrook/Rosa Parks Station.

Policy 5.3: Provide a connected pedestrian and bicycle network that links Willowbrook/Rosa Parks Station, Kenneth Hahn Plaza, new mixed use areas, CDU, MLK Medical Center and residential neighborhoods.

Policy 5.4: Facilitate mixed use development that maximizes pedestrian connectivity and minimizes the need for vehicle travel.

Goal 6: Improve quality of life for existing residents with improvements to the public realm.

Policy 6.1: Enhance the public realm with street trees, street furniture, bicycle facilities, sidewalks and pedestrian paths.

Policy 6.2: Provide a consistent canopy of shade trees throughout the Specific Plan area to enhance pedestrian comfort.

Policy 6.3: Provide pedestrian-scaled lighting to improve safety and enhance pedestrian environment.

Policy 6.4: Encourage outdoor dining and seating areas and other pedestrian-friendly uses in mixed use areas.

Policy 6.5: Explore joint use agreements with schools to better utilize existing and future open space resources.

Policy 6.6: Require new development to provide public open space as a community benefit, as appropriate. Consider providing incentives to developers for such provisions.

Policy 6.7: Build pocket parks and community gardens on available County-owned vacant lots.

Goal 7: Improve economic vitality and employment opportunities.

Policy 7.1: Create economic opportunities for the Willowbrook community by fostering a complementary variety of employment, retail, residential, and institutional uses.

Policy 7.2: Build on the Willowbrook community’s economic base as a “healthcare cluster” by working with appropriate partners to provide workforce development opportunities for local residents.

Policy 7.3: Facilitate public-private partnerships to share responsibility for implementing this Specific Plan and achieving its goals.

Policy 7.4: Encourage a mix of national brand and local merchant businesses.

Policy 7.5: Efficiently manage the supply and demand of parking to accommodate customer, commuter, and resident parking, and encourage the use of shared parking where possible.

Land Use and Urban Design Framework

2.1 Introduction

The goal of the Land Use and Urban Design Framework is to develop the Plan Area as a system of cohesive and integrated uses, spaces, structures, and environments. A clear framework provides visitors and residents with an understanding of how to easily and safely find their way around and to efficiently identify uses and activities. Arrangement of land uses into identifiable subareas, improved streetscapes, connected open spaces, and enhanced landmarks will help make the Plan Area accessible and distinctive.

This Chapter describes the approach to land use and urban design that will promote and allow transformation of the Plan Area into a vibrant transit-oriented district, while strengthening its connections to the adjacent residential neighborhoods and the rest of the Willowbrook community. This chapter also presents the short and long term land use strategies for the various institutions and facilities in the Plan Area, which will be important in achieving the community goals.

2.2 Land Use and Urban Design Framework

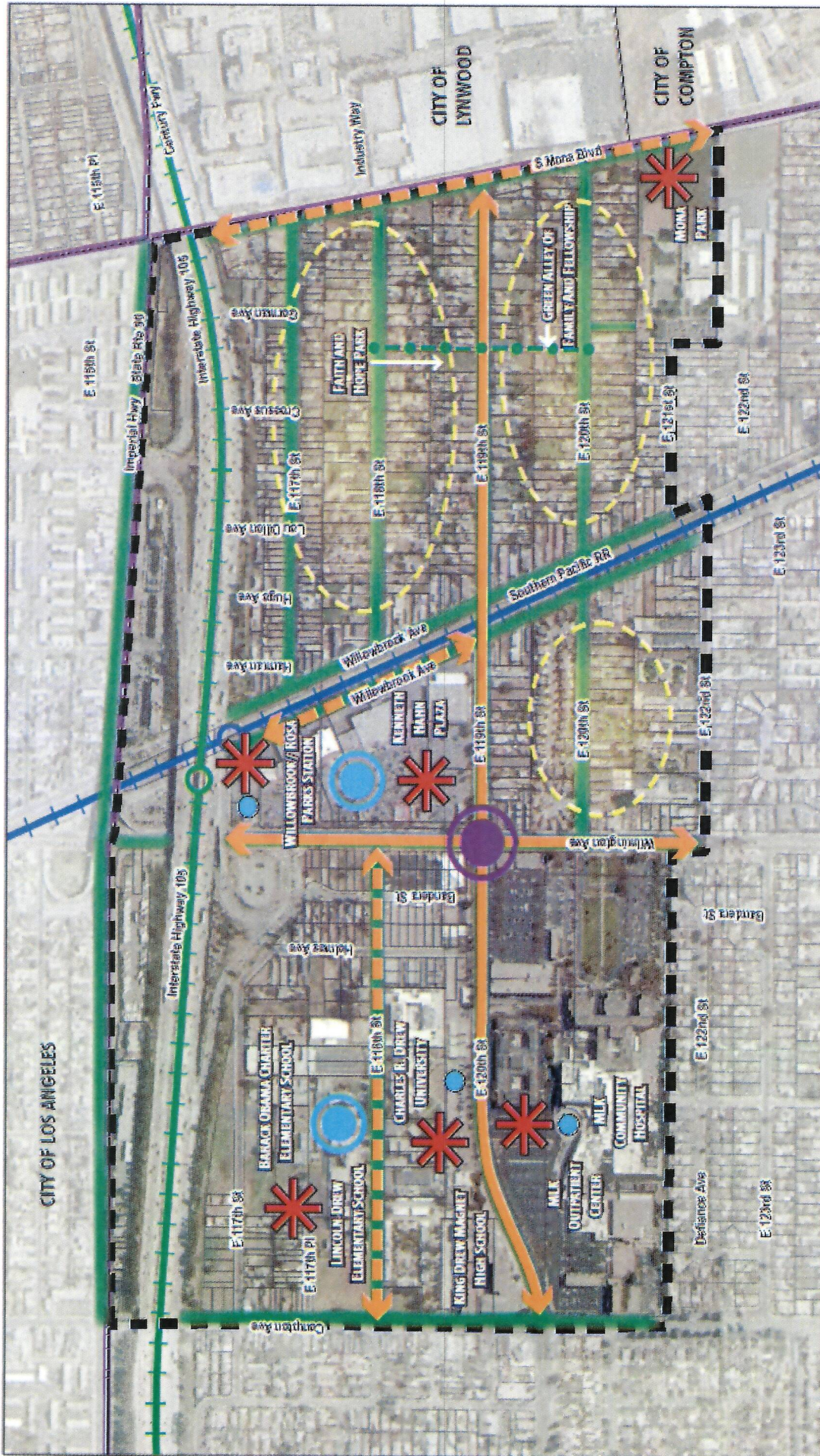
Elements of the Land Use and Urban Design Framework, as illustrated in Figure 2-1, include:

Specific Plan Subareas

The major activity centers in the Plan Area include the Willowbrook/Rosa Parks Station, Kenneth Hahn Plaza, MLK Medical Center campus, CDU campus, and Mona Park. Section 2.3 describes the distinct subareas into which the Plan Area can be divided. The existing uses in, and strategies for, planned and future uses in these subareas are also discussed in this chapter.

Mobility Improvements

Improvements to the vehicular, pedestrian, bicycle, and transit network that enhance connectivity and ease of movements for active transportation modes are further described in Chapter 4. Road diets for major pedestrian connections are further described in Chapter 4.



**Figure 2-1
Urban Design Framework**

- City of Los Angeles
- Willowbrook TOD Specific Plan Area
- Metro Green Line
- Metro Blue Line
- Major Activity Centers
- Streetscape Improvements
- Primary Pedestrian Connections
- Secondary Pedestrian Connections
- Green Alley of Family and Fellowship
- Entry Plaza
- Future Open Space/Plaza
- Major Intersection
- Protect and Enhance Residential Neighborhoods

WILLOWBROOK TOD SPECIFIC PLAN



Pedestrian and Bicycle Connections

The Urban Design Framework establishes enhanced pedestrian and bicycle connections along the street network between the Willowbrook/Rosa Parks Station and the activity centers. Additional bicycle facilities, such as a Class I bicycle path or Class IV bikeway on Mona Boulevard, to complete the bicycle network within the Plan Area are also described in Chapter 4. Enhancements to the pedestrian experience such as pedestrian-oriented intersection improvements and wider sidewalks, where feasible, are also included.

Major Intersection – Wilmington Avenue and East 120th/East 119th Street

The two major streets within the Plan Area are Wilmington Avenue in the north-south direction and East 120th/East 119th Street in the east-west direction. These two streets connect the major activity centers to each other, as well as connect across the rail tracks with the residential neighborhoods to the east. They also intersect to form the pedestrian core or “heart” of the Plan Area. Design concepts for special paving treatments at this intersection are described in Chapter 5.

Streetscape and Open Spaces

Another important element of the Urban Design Framework is improved streetscapes and connected open spaces. These streetscape improvements are intended to enhance and unify the visual and spatial experience of the driver, pedestrian, and the bicyclist, and help provide key linkages between the activity centers and the residential neighborhoods in the Plan Area. Recommended streetscape improvements consist of an interrelated palette of street trees, street furniture, street lighting, wayfinding signage and landscaping. These elements are described in Chapter 5.

Existing and planned open space resources in the Plan Area include Mona Park located along Mona Boulevard in the southeast corner of the Specific Plan area. Faith and Hope Park is located mid-block along East 119th Street and will be linked to Mona Park by the Green Alley of Family and Fellowship. Another important open space resource is the MLK Fitness Garden, primarily located along 120th Street at the MLK Center for Public Health.

The Specific Plan encourages the creation of additional public open space as a part of new or expanded mixed use developments in the Plan Area.

Residential Neighborhoods

The preservation and enhancement of the existing residential neighborhoods is an important element of the Urban Design Framework. Key strategies include maintaining the density and character of the neighborhoods (Chapters 2 and 3) such that local equestrians will continue to feel comfortable riding their horses on residential streets and ensure that residents can continue to maintain chickens on their properties and use outdoor clotheslines. Further strategies include, improving safety and comfort for navigating the street network (Chapter 4), and improving connections to the activity centers in the Plan Area (Chapter 5).

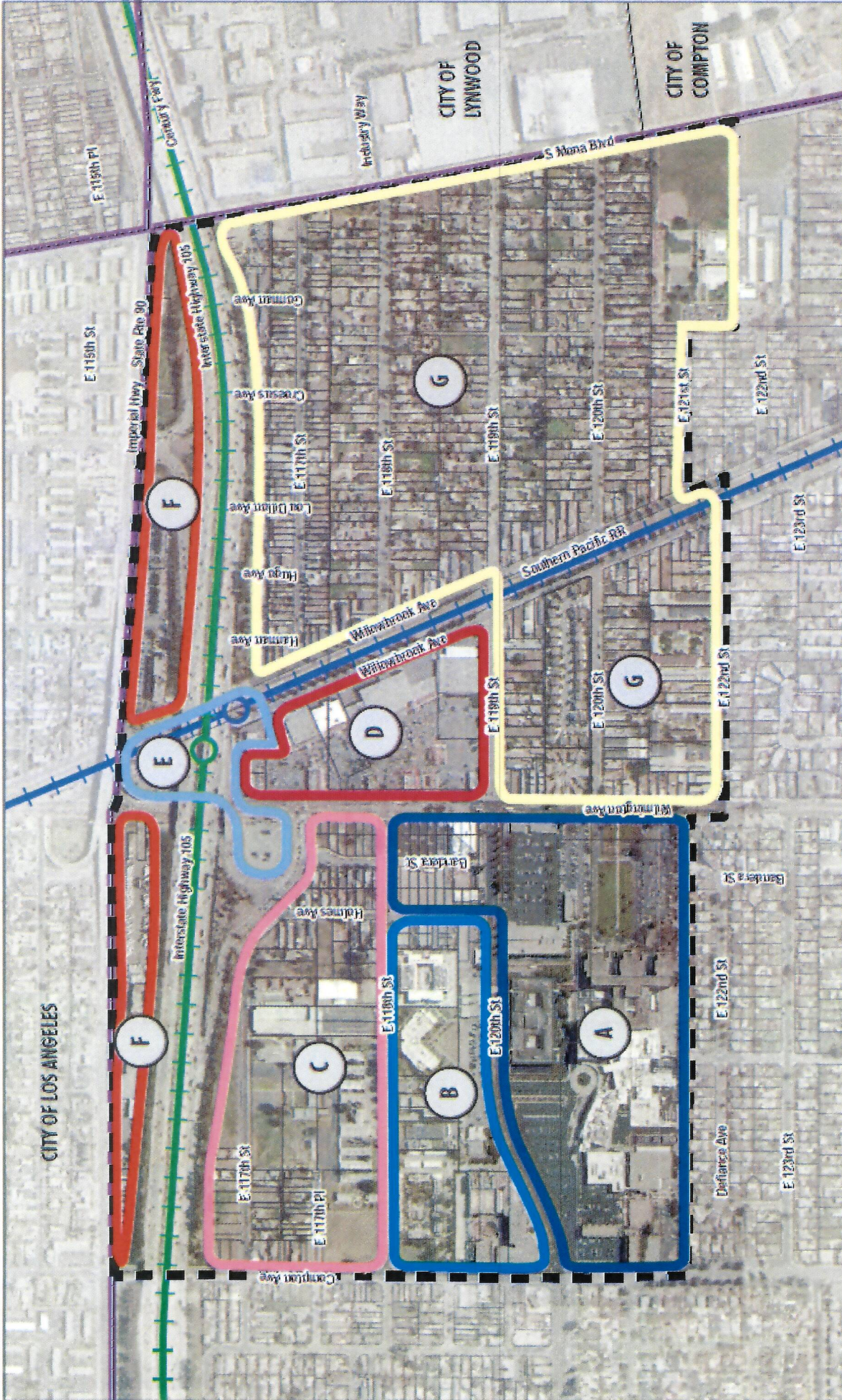


Figure 2-2
Specific Plan Subareas

- County of Los Angeles
- Willowbrook TOD Specific Plan Area
- Metro Green Line
- Metro Blue Line
- MLK Medical Center and Associated Facilities
- CDU Campus Area
- Northwest Subarea
- Kenneth Hahn Plaza
- Willowbrook/Rosa Parks Station
- Imperial Highway Corridor
- Residential Neighborhoods

WILLOWBROOK TOD SPECIFIC PLAN



2.3 Specific Plan Subareas

The Plan Area supports a range of land uses, including residential, retail, office, and other commercial, as well as educational and institutional facilities and services. It can be divided into the following seven subareas as shown in Figure 2-2:

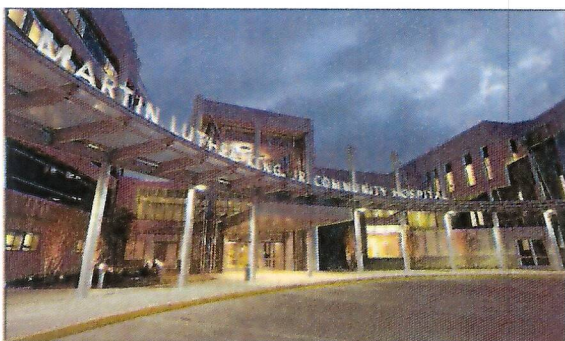
- MLK Medical Center and Associated Facilities
- CDU Campus Area
- Northwest Subarea
- Kenneth Hahn Plaza
- Willowbrook/Rosa Parks Station
- Imperial Highway Corridor
- Residential Neighborhoods

The uses in these subareas are complementary and supportive of each other. These subareas connect through the street network and a system of pedestrian walkways and open space areas. This section describes the existing conditions and the strategies for planned and future development within each subarea.

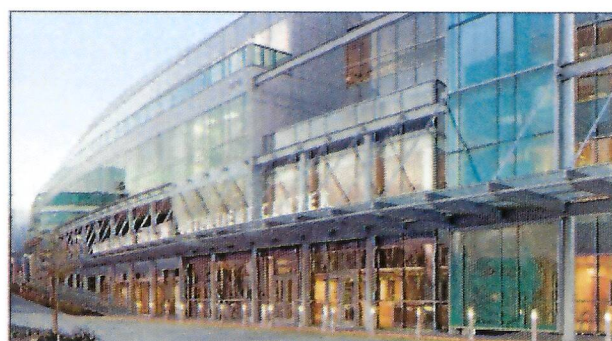
A. MLK Medical Center and Associated Facilities

Existing Conditions

The MLK Community Hospital reopened in 2015 after being closed in 2007. It is located on approximately 38 acres of land, and the campus is bounded by Wilmington Avenue to the east, East 120th Street to the north, Compton Avenue to the west, and the Specific Plan boundary to the south. The hospital is expected to serve 1.2 million residents in South Los Angeles, including Compton, Inglewood, Watts, Willowbrook, and Lynwood. It is also expected to create more than 1,800 jobs in the area with approximately 900 directly at the hospital.¹



Martin Luther King, Jr. Community Hospital



MLK Outpatient Center

Several other facilities serving the health care needs of the community are located adjacent to the hospital, including the MLK Center for Public Health, which focuses on preventative care, MLK Mental Health Center and the MLK Outpatient Center, all operated by the County. The Outpatient Center operates more than 70 primary care and specialty care clinics to serve the health care needs of

¹ www.mlkcommunityhospital.org

surrounding communities. The MLK Center for Public Health includes the MLK Fitness Garden, which is primarily located along the 120th Street frontage, and incorporates a walking path and par course fitness equipment for employees and the public.



MLK Center for Public Health



MLK Fitness Garden

The County seeks to establish MLK Medical Center Campus as a center of excellence for health care delivery, urban health promotion and prevention, health workforce development, academic research and teaching, and economic development.² In 2009, the County commissioned the preparation of an EIR that analyzed the potential for significant environmental impacts in association with the proposed MLK Medical Center Campus Redevelopment Project (MLK EIR). The MLK EIR analyzes two tiers: Tier I,³ project-level development, and Tier II,⁴ program-level development. In addition, in 2009, the County Board of Supervisors found that the repurposing of the existing facilities for hospital use was categorically exempt from CEQA.

Following the certification of the MLK EIR, the County approved the *MLK Medical Center Campus Master Plan & The Willowbrook MLK Wellness Community Vision* document in 2012 as a guiding framework for future development, which calls for a coordinated system of pedestrian and bicycle circulation within the campus and the surrounding area.

Tier I development, as described in the MLK EIR, has been completed. As part of Tier II, a 50,000 square foot medical office building and a 1,400 car parking garage are also in the planning and design stages. Other projects in the pipeline include expansion/construction of two clinics focused on providing services for children. The Los Angeles County Department of Children and Family Services (DCFS)

² *Martin Luther King, Jr. Medical Center Campus Redevelopment Draft EIR, August 2010.*

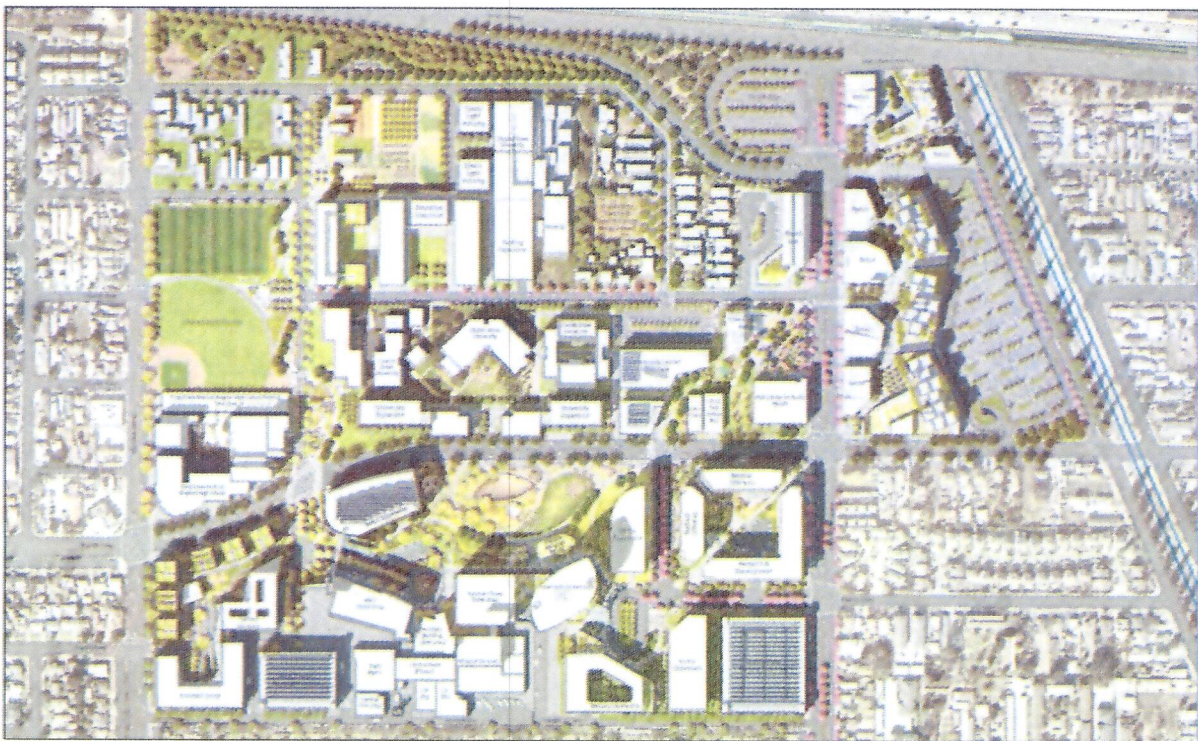
³ *Tier I analyzed the vacation of the emergency room, storage buildings, cooling towers, and existing Los Angeles County Martin Luther King, Jr. Multi-Service Ambulatory Care Center (MACC), and the development of two new environmentally sustainable buildings: the 132,000 square-foot new MACC and the 24,700 square-foot Ancillary Building. Tier I development consisted of approximately 170,332 square feet of new development. In addition, tenant improvements in existing buildings, site improvements, and potential relocation of the MRI Building was analyzed in Tier I.*

⁴ *Tier II analyzed a campus-wide Master Plan. It is anticipated that the development described in the Master Plan would prepare the proposed project site for future mixed use campus-related development that would provide the health services necessary to respond to and address the needs of the community. Tier II would have the potential to build out approximately 1,814,696 square feet of development on the proposed project site with mixed uses including medical office, commercial, retail, office space, recreation, and other development in support of the campus. The net new development of the proposed project would be approximately 1,476,010 square feet. Tier II also analyzed the construction of up to 100 residential units, to be developed at a multi-family density consistent with surrounding residential area multi-family development densities. In addition, the Tier II components would entail the reuse or replacement of the existing MACC building, emergency room, storage building, and cooling towers.*

operates the HUB Clinic, which provides mental health services to children and families who receive services through DCFS. Along with the Children’s Behavioral Clinic, the two clinics will be sited directly to the north of the MLK Center for Public Health building, replacing the surface parking lot. Parking will be relocated in the block to the west.

Specific Plan Strategy

The Willowbrook TOD Specific Plan envisions that the MLK Medical Center campus will continue to grow and thrive as a regional destination for medical, health and associated services. The development on the campus will be broadly guided by the *MLK Medical Center Master Plan and the Willowbrook MLK Wellness Community Vision*. Required parking for the medical uses will be provided on campus. The Specific Plan also builds upon the recommendations for enhanced pedestrian connections between the MLK Medical Center campus and the other activity areas in the Plan Area.



MLK Medical Center Campus Master Plan & The Willowbrook MLK Wellness Community Vision (Source: Gensler).

B. CDU Campus Subarea

Existing Conditions

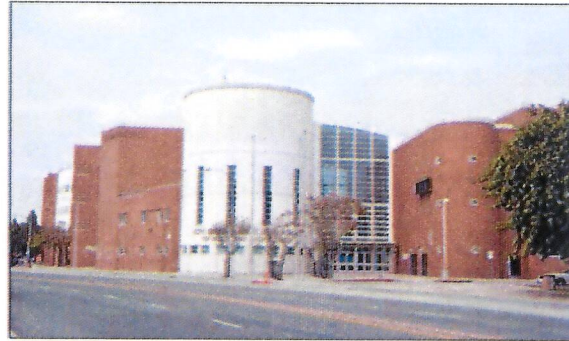
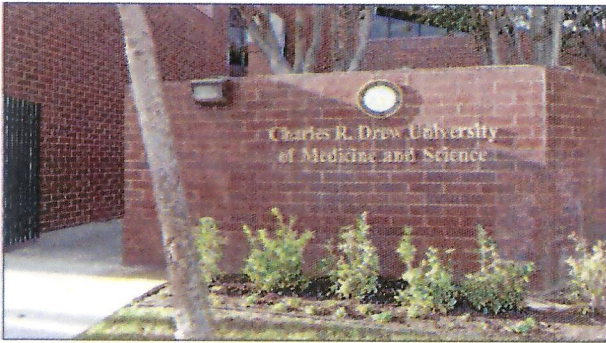
Immediately north of the MLK Medical Center campus are CDU, a private university, and King Drew Magnet High School, part of the Los Angeles Unified School District (LAUSD). These institutions are bounded by Holmes Avenue to the east, Compton Avenue to the west, East 120th Street to the south and East 118th Street to the north.

CDU has been in the Willowbrook community since the 1960s and is recognized by the Department of Education under Title III, Part B as a Historically Black Graduate Institution. CDU provides training in

medicine, medical technology, public health and nursing under the auspices of three schools: College of Medicine, College of Science and Health, and Mervyn M. Dymally School of Nursing.

Hospital occupations have been the focus of King Drew Magnet High School's curriculum since its establishment in the early 1980s. The Medicine and Science Careers Program, the centerpiece of the school's offerings, provides an opportunity for students to access experience-based learning in environments including hospitals, clinics, research laboratories, and university facilities. The school is located on the northeast corner of Compton Avenue and East 120th Street.

Other uses located within this area include a multi-family project on CDU-owned land on East 118th Street, several surface parking lots that serve CDU, and other County facilities in the area that are located along East 120th Street.



Charles R. Drew University of Medicine and Science and LAUSD King Drew Magnet High School are anchors in the CDU Campus Subarea.

Specific Plan Strategy

The Willowbrook TOD Specific Plan strategy includes supporting CDU's long range planning efforts. CDU is preparing a master plan that contemplates an increase in its student population from 625 to 1450 over a twenty to twenty-five year period. Today, CDU is primarily a commuter campus. The master plan includes new residential facilities (119 units) within the campus. The proposed housing types include dormitories for undergraduate students, shared housing for graduate students, and housing for visiting faculty. The master plan also provides conference facilities, incubator space, research labs, and additional student classrooms and facilities. The growth of the CDU campus will contribute additional buying power of the campus community (faculty, students, workers, etc.).

With MLK Community Hospital, MLK Center for Public Health, and other health-related facilities in the area, CDU and King Drew Magnet High School play an important role in fostering Willowbrook as a "health focused cluster" in South Los Angeles. The Willowbrook TOD Specific Plan reinforces the importance of these two facilities and builds upon the recommendations of CDU's master plan.

CDU's master plan aims to unify the campus into a cohesive setting with the creation of quad spaces and new buildings. The planned buildings are four to six stories in height. CDU's master plan expands the parking structure and parking lot located on East 118th Street to provide all required parking for the campus.

The Willowbrook TOD Specific Plan requires new campus buildings to be set back from the adjoining streets. To be responsive to the scale of the residential uses across Compton Avenue, new buildings along Compton Avenue are limited to four stories in height and must be set further away from the Compton Avenue property line. Architects/designers for CDU must consider architectural cohesiveness in terms of materials and style as new buildings are designed and built. The quad spaces and the required yards must be extensively landscaped.

CDU's master plan also includes a pedestrian, bicycle and shuttle circulation network system that connects with the activity centers in the Specific Plan area. Campus pedestrian and bicycle paths are connected with the public street network and activity centers in the vicinity, including the hospital to the south, and Willowbrook/Rosa Parks Station and Kenneth Hahn Plaza to the east. The CDU shuttle system routes also connect the activity centers noted above. There is an opportunity to consolidate the various shuttles in the area to improve shuttle routes and service frequency.

C. Northwest Subarea

Existing Conditions

The northwest subarea is an aggregation of a variety of uses – educational, retail, residential and institutional. Several vacant lots, owned by the Community Development Commission of the County of Los Angeles (LACDC), are located along East 117th Street. A large vacant site on the northeast corner of East 118th Street and Compton Avenue is owned by the Compton Unified School District (Compton Unified). Educational uses include Lincoln-Drew Elementary School (a part of Compton Unified), and Barack Obama Charter Elementary School. Both are located north of East 118th Street. Parking facilities for CDU are located adjacent to the schools. Other uses in



Two elementary schools, CDU parking and a corner market are found in the Northwest Subarea.

this subarea include a water well and reservoir operated by the Liberty Water Company. The residential uses are a mix of single family homes, duplexes and multi-family structures. The block between Wilmington Avenue and Bandera Street, north of East 118th Street, is the site for The Gateway at Willowbrook, an affordable senior housing project that is currently under construction. The relocated Los Angeles County Willowbrook Public Library will be located on the ground floor of this development.

Specific Plan Strategy

The northwest subarea has an advantageous location proximate to MLK Medical Center and CDU to the south and Willowbrook/Rosa Parks Station and Kenneth Hahn Plaza to the east. The Specific Plan envisions that over time, this subarea will capitalize on this proximity and become a mixed use district with employment generating uses and residential infill.

The Work Force Collaborative, among other groups and studies, has suggested that the Willowbrook community become a “health cluster” with MLK Medical Center campus and CDU as the anchors, leading to numerous opportunities for health related occupations at the two facilities. There is also opportunity for growth in related health fields. The Specific Plan encourages employment generating uses such as medical back office, laboratory facilities, hospital equipment facility, medical or university support businesses, etc. that provide job opportunities for both local residents and others. The Specific Plan also envisions residential infill that provides housing options for current and future workers, students, and residents in the area.

To achieve this long term vision, it is anticipated that the Specific Plan will facilitate the transition to more intensive uses. However, several uses within this subarea need to remain or be re-accommodated. The CDU garage site will continue to serve the parking needs of the CDU campus. The existing single-family residential uses within the subarea will continue as legal non-conforming uses until they transition to other uses.

Lincoln-Drew Elementary School and Barack Obama Charter Elementary School

The two school sites could be consolidated and reconfigured for more efficient use of the land. The California Department of Education Guide to School Site Analysis states that infill school sites can be much smaller than the previously typical recommended elementary school site size of 10 acres in urban areas. With the available LACDC-owned and Compton Unified-owned properties in the subarea, a 15-acre development parcel could be assembled with minimal acquisitions (assuming a portion of the East 117th Street right of way is vacated – see discussion below). A five-to six-acre portion of the assembled development parcel would be reserved for new school construction, with the remainder of the parcel sold for development. The proceeds from the sale could be used to fund the reconstruction of the elementary and charter schools.

Commercial users would likely include corporations looking to associate with the new MLK Medical Center campus and/or CDU, and the parcel could be an ideal “back office” administrative location. The parcel is large enough to accommodate a business park, which could attract a variety of healthcare related office uses, or function as a regional administrative facility for a healthcare provider. A portion of this parcel could also be allocated to a healthcare/wellness incubator campus.

The development parcel would also be very attractive to several types of residential uses. The development parcel’s location near CDU makes it an attractive location for university housing and

its proximity to the Willowbrook/Rosa Parks Station makes it a prime location for transit oriented development. Its location is also ideal for a sizeable workforce housing development.

If the LACDC-owned and Compton Unified-owned properties are aggregated, there is an opportunity to create an even larger consolidated development site by vacating a portion of East 117th Street that is west of the CDU garage site. It should be noted that any such vacation may only happen so long as there is no impact on access to any of the adjacent properties. Any new large scale development should provide a public open space for the community.

This subarea will benefit from parking reductions in the Specific Plan. There is also the opportunity to explore the creation of a parking management district within the subarea. The LACDC-owned properties between East 117th Street and the 105 Freeway can be the location for parking, while at the same time serving the additional purpose of providing a buffer between the freeway and new development.

Examples of employment generating uses and multi-family residential developments from other communities are depicted on the following page.

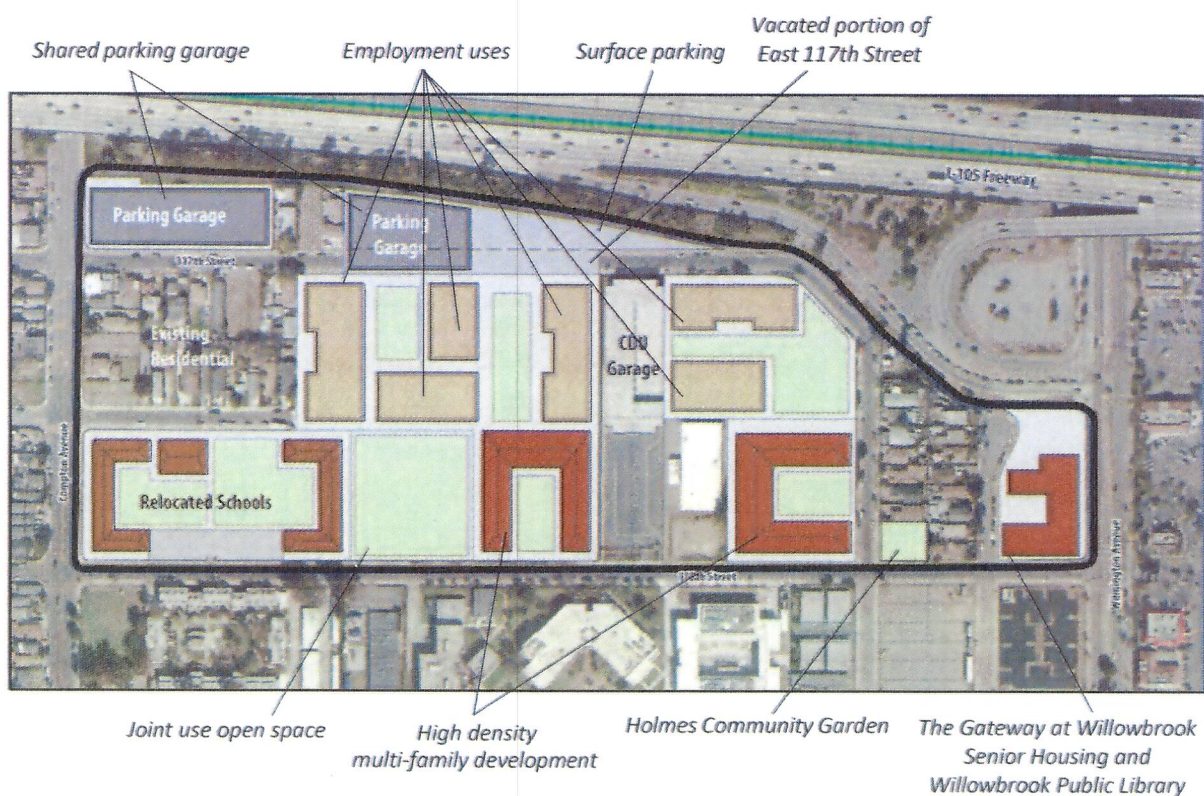
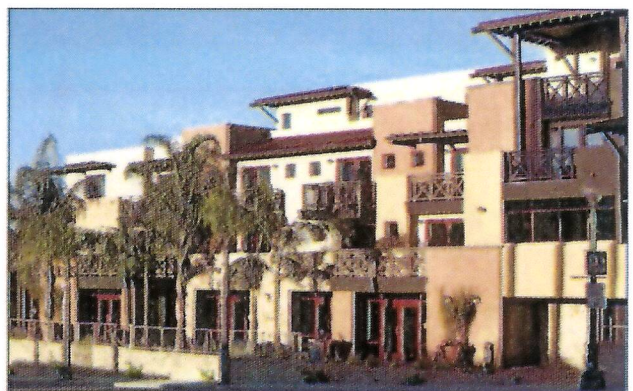
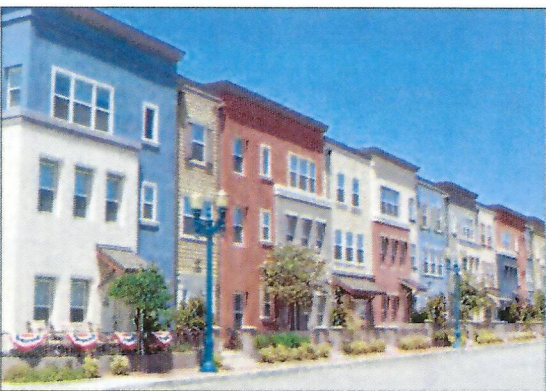


Figure 2-3: illustrative Site Plan for the Northwest Subarea

This site plan shows one way the Northwest subarea could be developed under the development regulations and standards of the Specific Plan. Through lot consolidation and development of a unified project at higher densities, this subarea offers the potential for an employment-generating district with new residential opportunities. This plan is illustrative only; it is not intended to prescribe this particular site layout or use.



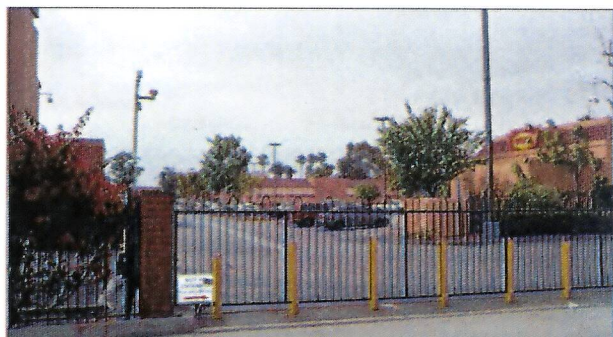
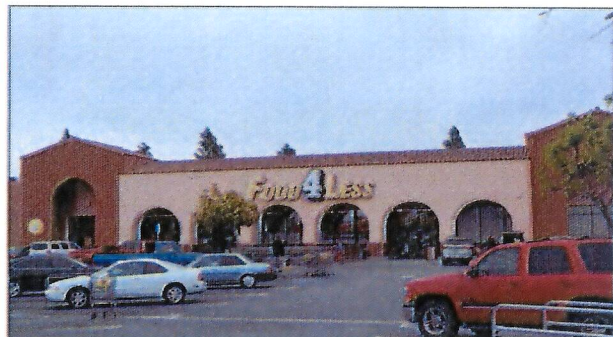
The Northwest subarea is envisioned to accommodate employment-generating uses in office and/or business park settings. Multi-family developments are anticipated to provide housing options for workforce, students, and transit-dependent users.

D. Kenneth Hahn Plaza

Existing Conditions

Kenneth Hahn Plaza is a privately owned shopping center on County-owned land. It is located immediately south of the Willowbrook/Rosa Parks Station, and is bounded by Wilmington Avenue to the west, East 119th Street to the south and Willowbrook Avenue to the east. The major anchor tenant is Food 4 Less, a grocery store. Other tenants include Rite-Aid, General Discount, and DaVita Dialysis Center. Food establishments in the center include McDonald's, Taco Bell, Pizza Hut, and Denny's, which is the only sit-down restaurant in the area. The Plaza is also the site of the Los Angeles County Willowbrook Public Library (soon to be relocated) and a Los Angeles County Sheriff substation.

The shopping center has a suburban layout with buildings sited at the rear and parking in front. Most of the fast food restaurants are located in smaller structures adjacent to Wilmington Avenue. The shopping center presents an uninviting and blank facade towards East 119th Street, which has mostly single family uses along the south side. The loading areas are located along Willowbrook Avenue. An internal service road between the buildings and rear property line provides access for trucks and other service vehicles. The entire site is surrounded by a six-foot tall wrought iron security fence blocking a pedestrian connection between the Willowbrook/Rosa Parks Station and the shopping center.



Kenneth Hahn Plaza currently has neighborhood serving retail as anchors. It is also isolated from its surroundings by fencing.

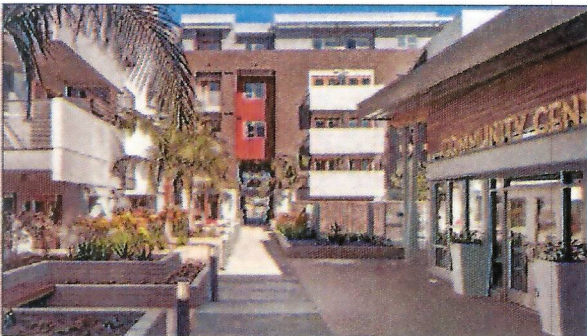
The Kenneth Hahn Plaza site is owned by the LACDC. Metro is acquiring approximately 1.5 acres of land on the northern end of the site for the expansion of the Willowbrook/Rosa Parks Station. The rest of the site is being purchased by the shopping center operator, Kimco Realty Corporation.

Specific Plan Strategy

The Kenneth Hahn Plaza site is the primary retail location within the Plan Area. While the site is a prime opportunity for redevelopment, there is also strong community desire and a need to retain a grocery store and other neighborhood serving retail in this location. The Willowbrook TOD Specific Plan sets forth the long term and short term goals for this area.

Long Term

Over the long term, the Specific Plan envisions the Kenneth Hahn Plaza will transform to a mixed use TOD project that takes advantage of the proximity of the Willowbrook/Rosa Parks Station. The MLK Medical Center Campus Master Plan, ULI (Urban Land Institute) Technical Advisory Panel study, and Rosa Park Station Study, all have also envisioned a mixed use TOD project on the site. The Specific Plan envisages that this site will provide an integrated, complementary residential and commercial development with a continued emphasis on neighborhood serving retail, restaurant and service uses to serve local Willowbrook residents, employees, pedestrians and consumers. Development can be integrated vertically, with residential or office uses above retail uses, or horizontally on site.



Over the long term, new mixed use buildings with neighborhood serving retail on the ground floor could transform Kenneth Hahn Plaza to a vibrant transit-oriented development.

Densities allowed per this Specific Plan are appropriate for both vertical and horizontal mixed use projects. Buildings may be up to four stories tall. Residential uses must be designed to minimize the noise and vibration impacts of the rail traffic along Willowbrook Avenue. Parking may either be in consolidated lots, under podiums or in structures.

East 119th Street is the primary connection with the residential neighborhoods across the rail tracks within the Plan Area. As such, it is an extremely important connection that needs to be improved in terms of appearance, safety, visibility and amenities. Infill uses along East 119th Street, at least on the north side, could either be storefronts with retail or service uses (child care, senior care, attorney offices etc.) or residential town-home/walk-ups facing the single-family residential uses on the south side of the street. The massing and scale of these buildings will be lower than the rest of the site in light of the single-family residential uses on the south side of East 119th Street.



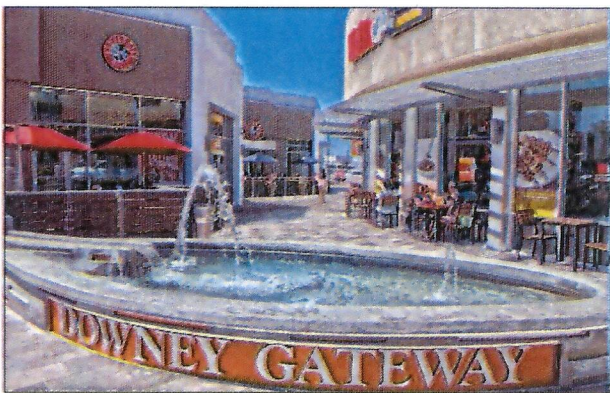
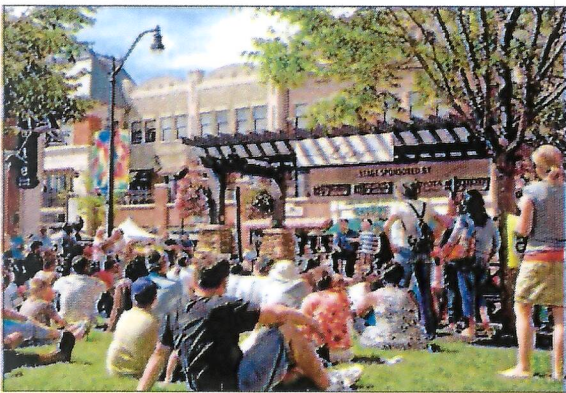
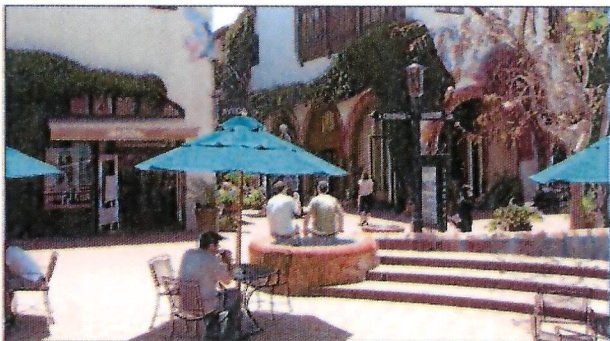
Retail or service uses will activate the north side of East 119th Street, the primary east-west connection in the Specific Plan area.



New retail infill along Wilmington Avenue should activate the Wilmington frontage.

As private development occurs on the Kenneth Hahn Plaza along the north side of East 119th Street, buildings will be required to be set back from the East 119th Street property line. This segment of East 119th Street is an important pedestrian travel corridor and is a connection to the residential neighborhoods to the east. Although privately maintained, this setback area could be improved with sidewalk paving and thus become a part of the expanded sidewalk.

As a part of the site’s redevelopment, a large outdoor plaza or gathering space should be created, similar to other successful shopping centers in the region. Such a space should have amenities such as street furniture, landscaped open space, public art, a water feature, and concessions.



The redevelopment of Kenneth Hahn Plaza should include a large outdoor plaza or gathering space.

The site also has the potential for retail infill along Wilmington Avenue while maintaining views to the existing retail on the site. Additional massing along the Wilmington frontage will also help create a more pedestrian friendly environment along that street.

Short Term

In the short term, it is likely that the Kenneth Hahn Plaza will continue as a shopping center that provides neighborhood-serving retail and service uses.

A better mix of retail tenants is needed to serve the shopping needs of Willowbrook area residents, employees of, and visitors to the newly opened MLK Community Hospital and other existing facilities in the Plan Area. Active tenant recruitment will be necessary to improve the tenant mix at the Kenneth Hahn Plaza.

Physical upgrades and improvements that can improve the environment in the Kenneth Hahn Plaza include enhanced landscaping in the parking lot and along the periphery of the shopping center. Landscaping can also help create clearly identifiable paths of travel for pedestrians to navigate around the shopping center and minimize conflicts between the drive-thru lane entrances and pedestrian travel. The main entry drive on Wilmington Avenue should be improved with sidewalks on both sides of the driveway, better landscaping, and marked crosswalks for pedestrian safety.

Removal of the perimeter fence should be considered to better integrate the Kenneth Hall Plaza with surrounding uses. At a minimum, pedestrian access to the Willowbrook/Rosa Parks Station

should be restored by removing the fence between the two facilities. Other improvements include better lighting in the parking lot, and improved pedestrian linkages with the surrounding activity centers, such as the hospital, CDU, mixed use areas, as well as the residential neighborhoods.

E. Willowbrook/Rosa Parks Station

Existing Conditions

The Plan Area has many options for public transportation, including bus service and the Willowbrook/Rosa Parks Station, which is a major transfer point between the Metro Blue Line and Green Line light rail lines. The Green Line platform is located in the median of the I-105 Freeway (Glenn Anderson) and the Blue Line platform is at grade, one level below. In addition to the two rail lines, the Willowbrook/Rosa Parks Station is served by six Metro bus lines, as well as several municipal bus lines and community shuttles.

A Metro public parking lot is located north of the I-105 freeway at Wilmington Avenue. Another Metro park-and-ride lot is located across Wilmington Avenue, within the off-ramp loop.

The physical environment at the Willowbrook/Rosa Parks Station needs major improvements. It is poorly lit as well as difficult to access and navigate, and it is poorly connected to its surrounding environment. The Willowbrook/Rosa Parks Station is blocked off from Kenneth Hahn Plaza by a fence, and access to the residential neighborhoods to the east of the tracks is limited. This station also has one of the highest crime rates in the Metro system.

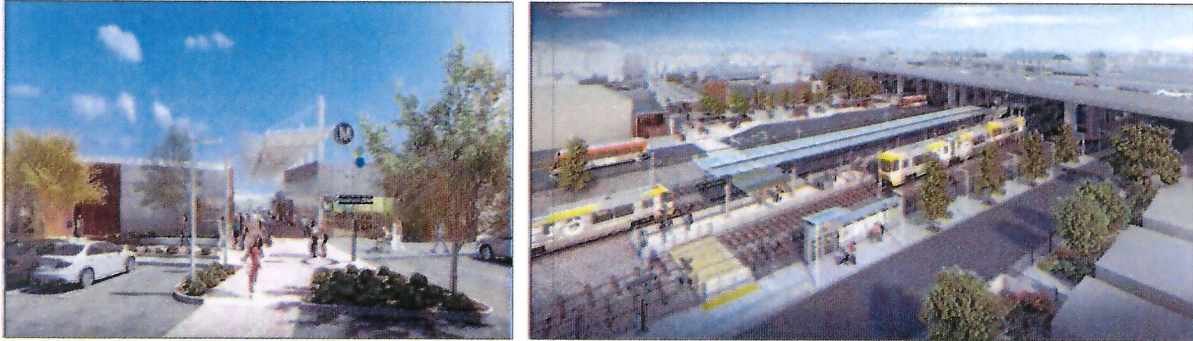


The Willowbrook/Rosa Parks Station environment is inhospitable and unattractive.

Metro is working on the Willowbrook/Rosa Parks Station Improvement Project that is intended to solve many of the above identified issues. The design, still preliminary at the time of writing this Specific Plan, includes relocating the Blue Line platform south, restructuring the entry area for automobiles, buses and pedestrians, and creating better connections for all modes of transportation. Acquisition of approximately 1.5 acres of the Kenneth Hahn Plaza is a part of this Metro project. The following improvements are included:

- Extend the Blue Line platform to reduce crowding;
- Create a new southern at-grade pedestrian crossing and entrance to the extended Blue Line platform;
- Upgrade the elevators, escalator and stairs to the Metro Blue and Green Line platforms;

- Improve the East Bus Depot to create a safer and more comfortable area for patrons;
- Reconfigure the West Bus Depot to relocate the bays from underneath the freeway;
- Reconfigure both Park & Ride lots for optimal circulation and pedestrian safety;
- Improve pedestrian circulation through the Station by creating a new Transit Hall;
- Add a new Sheriff's facility;
- Add a new Mobility Hub, including a Bike Hub;
- Add a new Metro Customer Service Center;
- Add new canopies on the extended Blue Line platform and at the open-air bus bays; and
- Improve lighting, landscaping, and signage throughout the site.



These images illustrate the reconceived entry plaza for the Willowbrook/Rosa Parks Station and its relationship with the rest of Kenneth Hahn Plaza. (Source: Metro)

Specific Plan Strategy

The Specific Plan supports the improvements planned at the Willowbrook/Rosa Parks Station. Several of the improvements planned at the Willowbrook/Rosa Parks Station are critical to improving pedestrian access and connectivity in the Plan Area. However, additional improvements are necessary to meet the goals of the Specific Plan.

The rail right-of-way is a barrier between the eastern neighborhoods and institutional uses to the west. Improvements to the at-grade crossing at the north end of the Blue Line platform will greatly enhance pedestrian connectivity to the residential neighborhoods to the east of the rail tracks. Another at-grade crossing is planned at the south end of the Blue Line platform.

Improvements to the intersection of Wilmington Avenue and the I-105 Freeway off-ramps are also necessary to improve access across Wilmington Avenue and to the Metro parking lot that is located in the ramp loop. These are included in the improvements described in Chapter 4 of the Specific Plan.

Another key improvement in this Specific Plan is enhanced bicycle access to the Station – a Class I bicycle path or Class IV cycle track is planned along Willowbrook Avenue West between Imperial Highway and East 119th Street, completing a missing connection within the bicycle network in the Specific Plan area. This stretch of Willowbrook Avenue West will also become a busway instead of its current use as a public roadway. Coordination with Metro to ensure all improvements proposed for Willowbrook/Rosa Parks Station are implemented will be necessary.

F. Imperial Highway Corridor

Existing Conditions

Imperial Highway is one of the busiest east-west streets in Los Angeles County. It is generally three lanes in each direction within the Plan Area and is also grade separated through the central portion across Wilmington Avenue. Frontage access roads provide access to some of the properties. The streetscape environment is generally uninviting to pedestrians.

The uses along Imperial Highway are generally a mix of auto repair, retail, residential, Metro facilities, and underutilized and vacant lots. A school bus parking lot and a Metro maintenance yard are located to the west of Wilmington Avenue. A barber shop and auto shop (towing) are located further west, near Compton Avenue. A couple new homes have been built recently in this section as well. Other Metro facilities, including a parking lot, are located to the east of Wilmington Avenue. A vacant site owned by the Housing Authority for the City of Los Angeles (HACLA) is also located along this segment of Imperial Highway. On the north side of Imperial Highway is Imperial Courts, a public housing project. While Imperial Courts is not in the Plan Area, there is interest in creating a pedestrian connection across the street to the HACLA site.

Specific Plan Strategy

The parcels within the Plan Area along Imperial Highway are sandwiched between the street and the I-105 Freeway. This corridor is suited to less intensive, commercial and service uses. Some of the existing uses, such as Metro maintenance yards and parking facilities, fall within this category. Other similar uses, such as self-service public storage facilities, are permitted by the Specific Plan. HACLA has tentative plans to use their vacant parcels to house a WorkSource center, as well as temporary staging space during Metro's station improvements. Another use under consideration is to house a communications equipment building for high-speed Internet service for the City of Los Angeles.

If the WorkSource Center or another use that is intended or likely to attract pedestrian traffic is proposed in the Imperial Highway Corridor subarea, pedestrian crossings on Imperial Highway, including new crosswalks when feasible, should be considered to facilitate pedestrians crossing Imperial Highway because there is currently no crossing between Mona Boulevard and Willowbrook Avenue.

While new residential uses along this corridor are prohibited, existing residential uses will be allowed to continue as non-conforming uses.



Low-intensity commercial and service uses are appropriate for the Imperial Highway corridor.

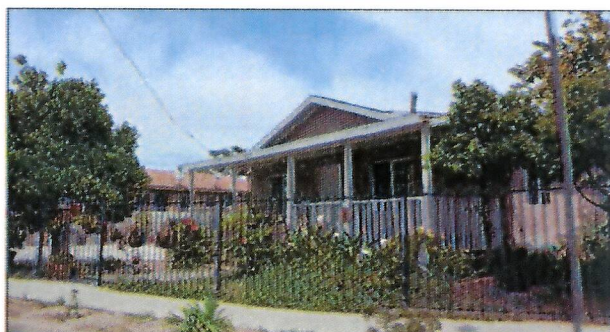
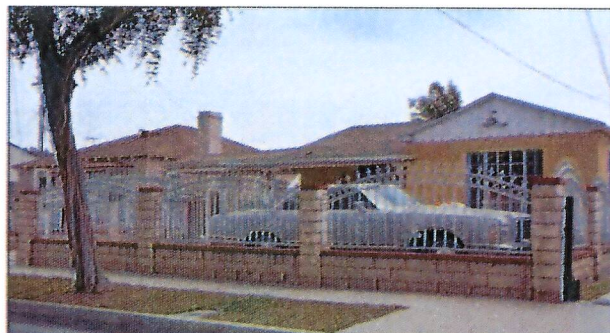
G. Residential Neighborhoods

Existing Conditions

Residential uses within the Plan Area are a mix of single-family homes, two-family units, and apartments, condominiums and townhouses. The residential area south of Kenneth Hahn Plaza, east of Wilmington Avenue and west of Willowbrook Avenue is primarily multi-family, with a mix of apartments and detached town homes. Most of these multi-family developments are two stories in height.

The area bounded by Mona Boulevard, I-105 Freeway, Willowbrook Avenue, and East 121st Street has mostly single-family uses, with the exception of the Willowbrook Avenue East frontage along the Metro Blue Line tracks. The latter is primarily multi-family uses. The single-family homes are predominantly single-story with an occasional two-story portion. The homes are fenced along the front property line. Many of the homes have landscaped front yards. Parking and property upkeep are identified problems, with vehicles often parked in the front setback and on sidewalks.

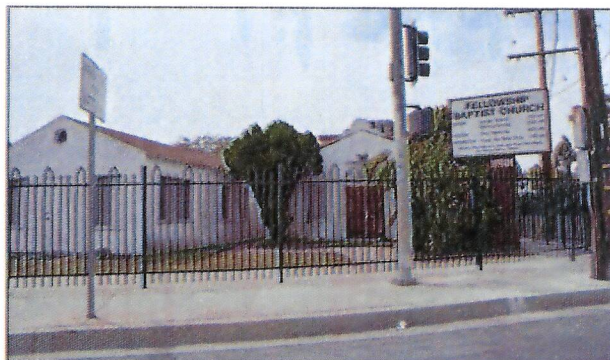
Residential parcel configurations vary dramatically across the Plan Area. In a majority of the residential blocks, parcels are 90 feet wide and more than 200 feet deep. However, some of the parcels are as narrow as 30 feet wide and approximately 100 feet deep. Many of the larger parcels have two (or more) units constructed on them, some illegally.



Single family homes are the predominant use east of the rail tracks within the Willowbrook TOD Specific Plan area.

Some of the other issues identified for the residential areas include:

- Illegal conversions/construction.
- Lack of street amenities.
- The Willowbrook Community Standards District (CSD) has several restrictions in place that need to be addressed.



Several important community churches are located in the Specific Plan area.

- This subarea includes an industrially zoned property at the northeastern corner of the residential neighborhoods on Mona Boulevard and I-105 Freeway. The site is just over an acre in size and is approximately 225 feet in depth. The current use is a commercial vehicle parking lot. The community has indicated a desire for better aesthetic screening and long-term conversion to residential use.

Specific Plan Strategy

The Willowbrook TOD Specific Plan preserves and enhances existing residential uses in this area. In keeping with the community’s desires, the Specific Plan does not increase base residential densities.

Industrial Property on Mona Boulevard

As described earlier, the only industrially zoned property in the Plan Area is currently being used as a truck parking lot, which is an incompatible use in a single- and two-family residential neighborhood. The Specific Plan rezones this property to residential in keeping with its surroundings. The current industrial use may continue as non-conforming per the provisions in Title 22 of the Los Angeles County Code (Title 22) and as noted in Chapter 3 of this Specific Plan. Per Chapter 22.172 (Nonconforming Uses, Buildings and Structures), a non-conforming use on an unimproved property shall terminate one year after a use becomes non-conforming. This Specific Plan extends the time limit for termination to five years from the adoption date of this Specific Plan.

Residential Street Improvements

The Specific Plan lays out the street improvements for the residential neighborhoods including a street tree program, pedestrian-scaled street lighting, and other street amenities. The Specific Plan also improves pedestrian and bicycle connectivity for the residents to the major activity centers in the Plan Area.

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3.1 Introduction

This Chapter establishes the zones, use regulations, development and design standards for the Plan Area. The zones with accompanying use regulations, and development and design standards, are intended to provide property owners, business owners, developers, and their designers with basic development and design criteria that are intended to reinforce the desired building and district character.

This Chapter shall be used in conjunction with all other relevant adopted County plans and ordinances, including:

- Los Angeles County General Plan
- Los Angeles County Code

3.2 Applicability

The provisions of this Specific Plan shall apply to all properties included in the Plan Area. No construction, modification, addition, placement or installation of any building or structure shall occur, nor shall any new use commence on any lot, on or after the effective date of this Specific Plan, except in conformity with the provisions of the Willowbrook TOD Specific Plan and except as noted in section 3.2A, below.

A. Applications filed prior to Specific Plan Adoption

The following provisions shall apply to complete applications filed prior to the effective date of this Specific Plan and accompanying General Plan amendment.

The applicant may choose whether the application will be reviewed for consistency with the land use designations and zoning regulations in effect prior to the adoption of this Specific Plan or with the land use designations and zoning consistent with this Specific Plan. In either case, approval of the application is not guaranteed.

If the applicant chooses to have the application reviewed for consistency with the land use designations and zoning regulations in effect prior to the adoption of this Specific Plan, the application

may be modified prior to consideration by the Los Angeles County Board of Supervisors, the Regional Planning Commission, the Hearing Officer or the Director, and still be reviewed for consistency with the land use designations and zoning regulations in effect prior to the adoption of this Specific Plan as long as the requested modification does not:

- Change the project's housing type (e.g., from single-family to two-family or multi-family);
- Increase the project's residential density;
- Increase the project's floor area or lot coverage for non-residential space;
- Increase the amount of grading for the project; or
- Increase the area of ground disturbance resulting from the project.

Such a modification may necessitate submittal by the applicant of revised, updated, or additional materials, including, but not limited to, site plans, elevations, and oak tree reports. If the requested modification does not meet all of the criteria listed above, the modified project shall be reviewed for consistency with this Specific Plan and the General Plan.

An application for a modification to an approved but not used permit, that is valid on the effective date of this Specific Plan, where the modification will result in a project that substantially conforms with the previously approved project, as determined by the Director, may, at the election of the applicant, be reviewed for consistency with the land use designations and zoning regulations in effect prior to the adoption of this Specific Plan. In all other cases, an application for a modification to such permit shall be considered a new application and shall be reviewed for consistency with this Specific Plan and the General Plan.

If an approved permit has been used prior to the effective date of this Specific Plan and the permit contains a grant term, the approved use may continue until the end of the grant term, and, at the end of the grant term, shall be subject to this Specific Plan and General Plan policies in effect at that time. If, during the grant term, a request for a modification to the previously approved permit is made, and the modification will result in a use that substantially conforms with the previously approved permit, as determined by the Director, the modified permit shall be reviewed for consistency with the land use designations and zoning regulations in effect prior to the adoption of this Specific Plan. In all other cases, a request for a modification to the previously approved permit shall be subject to this Specific Plan and the General Plan policies in effect at the time of filing the application for the modification.

If an approved permit has been used prior to the effective date of this Specific Plan, and the permit does not have a grant term, the approved use may continue indefinitely without regard to the policies in this Specific Plan. Notwithstanding the preceding sentence, all applicable non-conforming use provisions of Title 22 shall apply to the previously approved permit. If a request for a modification to the previously approved permit is made, and the modification will result in a use that substantially conforms with the previously approved permit, as determined by the Director, the modified permit shall be reviewed for consistency with the land use designations and zoning regulations in effect prior to the adoption of this Specific Plan. In all other cases, a request for a modification to the previously approved permit shall be subject to the Specific Plan and the General Plan policies in effect at the time of filing the application for the modification.

B. Consistency with Title 22 of the Los Angeles County Code

The Willowbrook TOD Specific Plan does not convey any rights not otherwise granted under the provisions and procedures contained in Los Angeles County Code, Title 22: Planning and Zoning (Title 22) and other applicable ordinances, except as specifically provided herein. Where general land use regulations and/or development standards of Title 22 are inconsistent with this Specific Plan, the standards and regulations of the Specific Plan shall prevail and supersede the applicable provisions of Title 22. The provisions of this Specific Plan shall also supersede the provisions of the Willowbrook Community Standards District (Chapter 22.352).

Any issue not specifically covered in the Specific Plan shall be subject to Title 22 and/or other titles of the Los Angeles County Code, or to interpretation by the Director if not specifically covered in the County's existing regulations.

3.3 Nonconforming Uses

Single-family residences located in zones that do not allow such uses may be continuously maintained as such, pursuant to the provisions of Chapter 22.172 (Nonconforming Uses, Buildings and Structures).

The existing legally established M-1 use on the parcel on the corner of Mona Boulevard and I-105 Freeway (Assessor Identification Number 6150-017-021) at the time of adoption of this Specific Plan may be continuously maintained as such, pursuant to the provisions of Chapter 22.172 (Nonconforming Uses, Buildings and Structures) of Title 22 with the following exception:

- The current use shall terminate five years after the adoption of the Specific Plan.

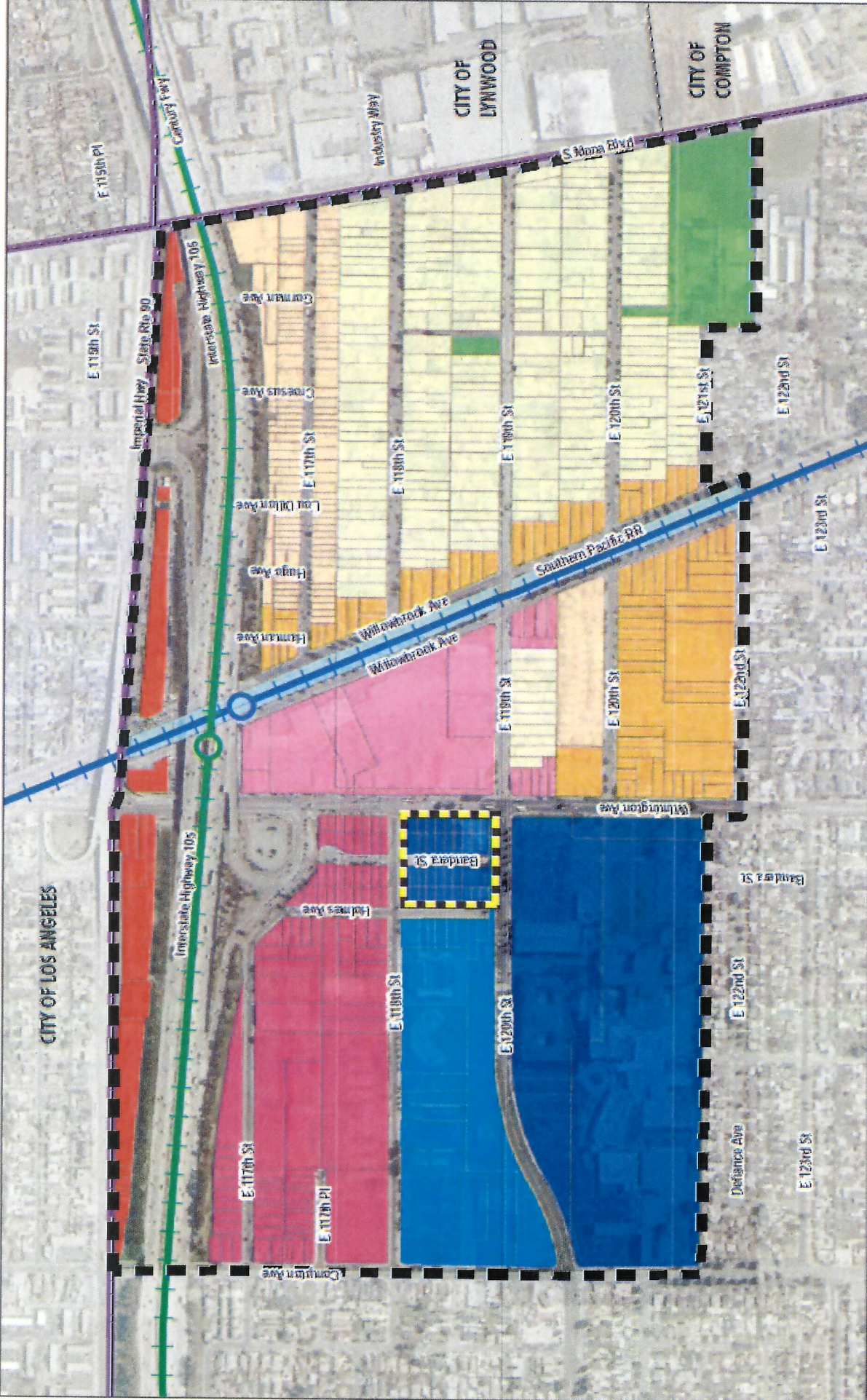
Any other use within the Plan Area that is nonconforming to the requirements and standards of this Specific Plan shall be subject to Chapter 22.172 (Nonconforming Uses, Buildings and Structures).

3.4 Specific Plan Zones

The Willowbrook TOD Specific Plan establishes zoning for parcels within the Specific Plan boundary as identified in Figure 3-1. The zones for the Plan Area are as follows:

- Mixed Use 1 (MU-1)
- Mixed Use 2 (MU-2)
- MLK Medical and Overlay
- Drew Educational
- Imperial Commercial
- Willowbrook Residential 1
- Willowbrook Residential 2
- Willowbrook Residential 3
- Open Space (O-5)

The following sections of this Chapter identify the land use regulations and development and design standards for each Specific Plan zone. Parking requirements for uses in the Specific Plan are contained in Section 3.13 of this Chapter. Additional design guidelines are contained in Sections 3.14-3.18 of this Chapter.



**Figure 3-1
Specific Plan Zones**

- | | | | |
|--|--|--|---|
| | County of Los Angeles | | Imperial Commercial (FAR 1.0) |
| | Willowbrook TOD Specific Plan Area | | Willowbrook Residential 1 (9 du/acre) |
| | Metro Green Line | | Willowbrook Residential 2 (18 du/acre) |
| | Metro Blue Line | | Willowbrook Residential 3 (30 du/acre) |
| | Rail Right-of-Way | | Open Space (O-S) |
| | Mixed Use 1 (MU-1) (FAR 1.5, 30 du/acre) | | MLK Medical (FAR 1.65) |
| | Mixed Use 2 (MU-2) (FAR 3.0, 60 du/acre) | | MLK Medical Overlay (FAR 2.5, 60 du/acre) |
| | Bandiera St | | Draw Educational (FAR 1.5) |

WILLOWBROOK TOD SPECIFIC PLAN



3.5 Mixed Use Zones

A. Purpose

The Mixed Use Zones are established to promote walking, bicycling, recreation, transit use, and community reinvestment and are intended to facilitate integrated commercial and residential development through optimal site planning and efficient use of land.

1. Mixed Use 1 (MU-1) Zone. The Mixed Use 1 (MU-1) zone is intended to provide commercial and residential development, with an emphasis on neighborhood serving retail, restaurant, and service uses. The area is appropriate for a large retail or mixed use center, with a neighborhood plaza or community gathering space as a focal point and strong pedestrian connections to the Willowbrook/Rosa Parks Station, as well as the educational and medical campuses to the west.

2. Mixed Use 2 (MU-2) Zone. The Mixed Use 2 (MU-2) zone is intended to provide commercial and residential development, with an emphasis on employment-generating uses and residential infill development. The area is appropriate for office, business park, or mixed use developments, with open space components and pedestrian connections to the Willowbrook/Rosa Parks Station, and the educational and medical campuses to the south.

B. Land Use Regulations

1. **General.** This section prescribes the land use regulations for the Mixed Use Zones.
2. **Permit and review requirements.** Table 3-1 identifies the permit or review required to establish each use.

**Table 3-1:
Permit and Review Requirements**

Abbreviation	Permit or Review Requirement	Reference
--	Not Permitted	
P	Permitted	
SPR	Ministerial Site Plan Review	Chapter 22.186
CUP	Conditional Use Permit	Chapter 22.158
SEP	Special Events Permit	Chapter 22.192

3. **Principal uses.** Table 3-2 identifies the principal uses and the permit or review required to establish each use. Additional regulations contained in Title 22 and/or this Specific Plan are also identified. Principal uses identified in Table 3-2 below may be established on a single site either as an integrated project, or as stand-alone use, subject to the provisions of this Chapter.
4. **Use requirements in Mixed Use 1 (MU-1) Zone.** The following use requirements apply in the MU-1 Zone:
 - a) Retail uses are required on the ground floor fronting Wilmington Avenue and East 119th Street.

**Table 3-2:
Principal Use Regulations for Mixed Use Zones**

Use	MU-1	MU-2	Additional Regulations
Agricultural and Resource-Based Uses			
Community gardens	SPR	SPR	
Animal-Related Uses			
Veterinaries, small animal			
A. Clinics	SPR	--	
B. Hospitals	CUP	--	
Cultural, Educational and Institutional Uses			
Churches, temples or other places used exclusively for religious worship, including accessory educational and social activities	SPR	CUP	
Community centers, including accessory facilities	SPR	SPR	
Libraries	SPR	SPR	
Museums	SPR	SPR	
Schools, including accessory facilities			
A. College, university, professional	CUP	CUP	
B. Elementary and secondary (public)	SPR	SPR	
C. Elementary and secondary (private)	--	CUP	
D. Vocational, technical and trade schools	SPR	CUP	
E. Tutorial, dance, art, martial arts, drama, etc.	SPR	SPR	
Theaters and other auditoriums	CUP	--	
Industrial Uses			
Laboratories, research and testing	--	CUP	
Lodging Uses			
Hotel	--	CUP	
Recreational Uses			
Billiards or pool halls	CUP	--	
Bowling alleys	CUP	--	
Gymnasiums	SPR	CUP	
Parks, playgrounds and open space (plazas, etc.)	SPR	SPR	
Residential Uses			
Adult residential facility			
Facilities serving six or fewer persons	P	P	Chapter 22.140.520
Foster family home, in an approved residential use	P	P	
Group homes for children			
Facilities serving six or fewer persons	P	P	Chapter 22.140.520
Joint live and work units	SPR	SPR	Chapter 22.140.320
Multi-family housing			
A. Apartment houses	SPR	SPR	
B. Townhouses	SPR	SPR	Chapter 22.140.600
Small family homes for children	P	P	

Table 3-2 (continued)

Use	MU-1	MU-2	Additional Regulations
Retail/Commercial Uses			
Art gallery	SPR	SPR	
Drug store or pharmacy	SPR	SPR	
Farmers' market	SPR	SPR	Chapter 22.140.220
Food and Beverage Sales			
A. Grocery stores and markets	SPR	SPR	
B. Specialty stores (deli, coffee, bakery, produce)	SPR	SPR	
Health clubs	SPR	CUP	
Home improvement, retail sales and service (hardware, lumber and building material stores)			
A. Under 10,000 sf	SPR	--	
B. 10,000 sf or more	CUP	--	
Office machines and equipment sales	SPR	SPR	
Pet store, including the sale of pets (sales and grooming, no boarding)	SPR	--	
Retail stores			
A. 10,000 sf and under	SPR	SPR	
B. More than 10,000 sf	SPR	--	
Secondhand stores	SPR	--	
Service Uses			
Banks and financial institutions	SPR	SPR	
Business support services & facilities (including graphic reproduction, computer services, etc.)	SPR	SPR	
Catering establishments	SPR	SPR	
Conference facilities	--	CUP	
Day care			
A. Adult day care	CUP	CUP	
B. Child day care center	SPR	SPR	
C. Large family child care home	--	--	
D. Small family child care home	P	P	
Health retreat	--	CUP	
Medical services - clinic, medical/dental offices, medical laboratory, and urgent/express care	SPR	SPR	
Offices, business and professional	SPR	SPR	
Personal services (barber, beauty salon, spa, tailor, dry cleaner, self-service laundry, etc.)	SPR	SPR	
Parking lots and parking buildings (stand alone)	SPR	SPR	Chapter 22.112
Repair shops, household and fix-it	SPR	--	
Restaurants and other eating establishments, including food take-out and outdoor dining	SPR	SPR	Chapter 22.140.410; Section 3.5.C.5 of this Chapter

Table 3-2 (continued)

Use	MU-1	MU-2	Additional Regulations
Transportation, Electrical, Gas, Communication Utilities and Public Service Uses			
Comfort stations	SPR	SPR	
Communications equipment buildings	SPR	SPR	
Earth station	CUP	CUP	
Electric distributions substations, including related microwave facilities	SPR	SPR	Chapter 22.140.200
Fire station	SPR	SPR	
Gas metering and control stations, public utility	SPR	SPR	
Microwave stations	SPR	SPR	
Police station	SPR	SPR	
Post office	SPR	SPR	
Publicly owned uses, other than those already listed, that are necessary to maintain the public health convenience or general welfare	CUP	CUP	
Public utility service center	SPR	SPR	
Stations - bus, rail or taxi	SPR	--	
Telephone repeater station	SPR	SPR	
Water reservoirs, dams, treatment plants, gauging stations, pumping stations, tanks, wells and any use normal or accessory to the storage and distribution of water	CUP	CUP	
Wireless telecommunications facilities	CUP	CUP	

5. **Accessory uses.** Table 3-3 identifies uses accessory to an established principal use listed in Table 3-2.

**Table 3-3:
Accessory Use Regulations for Mixed Use Zones**

Use	MU-1	MU-2	Additional Regulations
Accessory buildings and structures	SPR	SPR	Chapter 22.110
Alcoholic beverage sales for off-site consumption	CUP	--	Chapter 22.140.030
Alcoholic beverage sales for on-site consumption	CUP	--	Chapter 22.140.030
Home-based occupations	P	P	Chapter 22.140.290
Live entertainment	SPR	SPR	Chapter 22.140.330
Outdoor storage and display	SPR	SPR	Chapter 22.140.420
Signs	As specified in Chapter 22.114 and Section 3.5.C.4 of this Chapter		

6. **Temporary uses.** Table 3-4 identifies uses that are temporary and accessory to an established principal use listed in Table 3-2.

**Table 3-4:
Temporary Use Regulations for Mixed Use Zones**

Use	MU-1	MU-2	Additional Regulations
Holiday and seasonal sales	SPR	SPR	Chapter 22.140.280
Special events	SEP	SEP	
Storage of materials and construction equipment used in construction or maintenance of streets and highways, sewers, storm drains, underground conduits, flood control works, pipelines and similar uses for up to one year	CUP	CUP	

7. **Uses not listed.** If a use is not specifically listed in Tables 3-2, 3-3 or 3-4, the Director shall have the authority to determine whether a use is permitted if that use is comparable to a particular use permitted in the zone.
8. **Prohibited uses.** Uses expressly prohibited in the Mixed Use Zones are as follows:
- a) Bars and cocktails lounges.
 - b) Drive-through establishments.
 - c) Liquor stores.
 - d) Pawn shops.
 - e) Tobacco and vape shops.
 - f) Vehicle-related uses, including vehicle sales, rentals, storage, washing and service.
 - g) Alternative financial services.

C. Development and Design Standards

1. **General.** Table 3-5 identifies the development standards applicable to all development in the Mixed Use Zones. Additional regulations contained in Title 22 and/or this Specific Plan are also identified.

**Table 3-5:
Development Standards for Mixed Uses**

Development Standards	MU-1	MU-2	Additional Regulations
Lot Area - Minimum	1 acre	1 acre	For new lots only. Lots may be subdivided to less than one acre for buildings that are part of a larger development in which parking and access is shared.
Minimum Street-fronting Yard Depth			Additional setback may be required depending on existing street frontage.
E. 119th Street	10 ft	n/a	
Willowbrook Avenue	20 ft	n/a	
Wilmington Avenue	5 ft	4 ft	
E. 117th Street	n/a	10 ft	
E. 117th Place	n/a	10 ft	
E. 118th Street	n/a	10 ft	
Bandera Street	n/a	10 ft	
Holmes Avenue	n/a	10 ft	
Compton Avenue	n/a	15 ft	
Minimum Interior Yard (side or rear)	0 ft	0 ft	
Minimum Interior Yard (side or rear) from Residential Use	n/a	20 ft	
Building Height - Maximum	50 ft and 4 stories	50 ft and 4 stories	Section 3.5.C.2. of this Chapter; Chapters 22.04 and 22.110
Floor Area Ratio - Maximum	1.5	3.0	
Residential Density - Maximum	30 du/acre	60 du/acre	
Non-Residential Common Open Space Area (courtyard or plaza) - Minimum	1,000 sf /acre	1,000 sf /acre	Minimum size shall be 1,000 sf and minimum dimension shall be 25 feet.
Residential Common Open Space Area - Minimum	50 sf/du	50 sf/du for at least 50% of the units	Required for residential uses only. Minimum dimension shall be 25 feet.
Residential Private Open Space Area - Minimum	50 sf/du	50 sf/du	Required for residential uses only. Minimum dimension shall be 5 feet.
Residential Floor Areas - Minimum			
Efficiency	500 sf	500 sf	
One Bedroom	700 sf	700 sf	
Two Bedroom	900 sf	900 sf	
Three Bedroom	1,100 sf	1,100 sf	
Each Additional Bedroom	200 sf	200 sf	
Live/work unit	1,000 sf	1,000 sf	Chapter 22.140.320

2. **Building height adjacent to residential zone or use.** At residential-adjacent locations, buildings shall maintain lower profiles to provide a transition between mixed use developments and residential zones and uses. The following standards apply:
- New development in the MU-1 Zone fronting East 119th Street shall not exceed 40 feet and three stories for a depth of 50 feet from East 119th Street property line.
 - New development in the MU-2 Zone fronting Compton Avenue shall step back the fourth story of the building by a minimum of 10 feet.
 - New development sharing an interior property line with an existing single- or two-family residence shall step back the fourth story of the building by a minimum of 10 feet (see Figure 3-2).

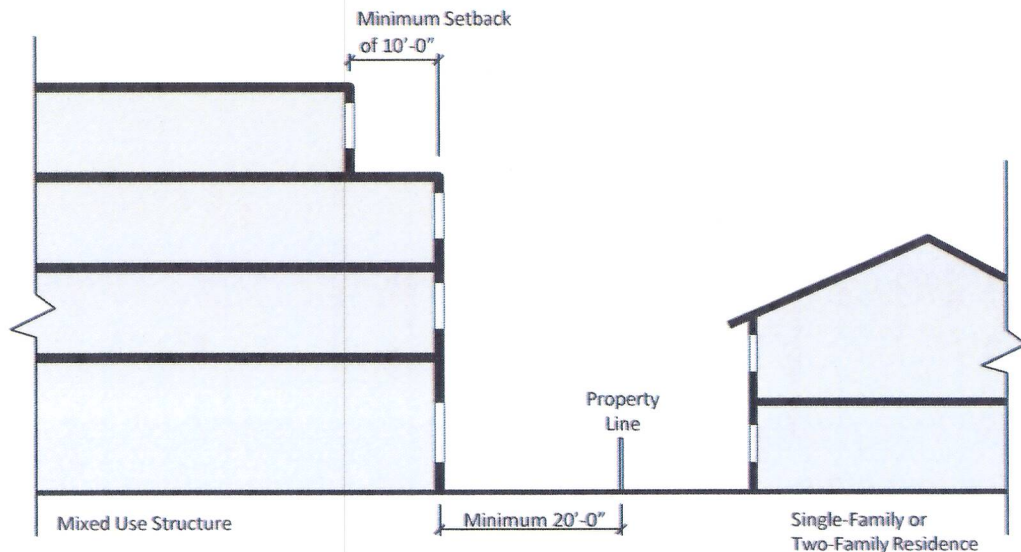


Figure 3-2: Building height step back adjacent to residential zone or use

3. **Minimum transparency requirements.**
- Windows and openings facing streets shall constitute a minimum of 50 percent of street-level building facades, with the exception of residential units with individual entries, in which case windows and openings facing streets shall constitute a minimum of 30 percent of street-level building facades.
 - On upper-floors, windows and openings facing streets shall constitute a minimum of 40 percent of building facades for commercial uses, and 20 percent of building facades for residential uses.
4. **Minimum floor-to-ceiling height.** The floor-to-ceiling height of the ground floor for a street-facing retail component of a mixed use development shall be a minimum of 15 feet.
5. **Signs.** The provisions of Chapter 22.114 (Signs) for signs in the C-3 Zone shall apply to all development in the Mixed Use Zones, with the following exceptions:
- Roof signs shall be prohibited.
 - Signs on perimeter fences shall be prohibited.

6. **Standards for outdoor dining.** Outdoor dining is encouraged in the Mixed Use Zones and may be provided either in private patios or within the public sidewalk right-of-way. Standards for outdoor sidewalk dining are provided in Chapter 16.27 (Outdoor Sidewalk Dining), Los Angeles County Code, Title 16: Highways. The following standards apply to outdoor patio dining spaces.
- a) **Patio dining locations.** Patio dining shall be permitted within private property adjacent to the streets or public alley walkways and shall be used exclusively for dining, drinking and circulation therein. Outdoor patio dining is allowed within the street yard area.
 - b) **Patio Dining Coverage.** Patio dining spaces may be open or covered with temporary structures such as trellises, umbrellas or permanent structures such as overhangs and upper floors of the building.
 - c) **Demarcation of patio dining spaces.** The patio dining spaces adjacent to streets or alley walkways shall be demarcated by either temporary or permanent boundary definers such as railings, fencing, or a combination of railings or fencing, and landscaping in planter boxes, or movable bollards.
 - d) **Parking Requirements.** The area used for patio dining shall be included in the eating or drinking establishment's area for the purposes of calculating the establishment's parking requirement.
 - e) **Hours of operation.** The outdoor dining space hours of operation shall be limited to the hours of operation of the associated dining establishment.
 - f) **Building Code-Awnings.** All awnings shall conform to the requirements of Los Angeles County Code, Title 26: Building Code, related to roof coverings.
 - g) **Music.** There shall be no amplified sound or music in the outdoor patio dining areas.
 - h) **Display of outdoor menus.** A single-sided framed menu attached to a moveable barrier that defines the patio dining space is permitted within the patio dining space. The size of the frame shall not exceed three square feet. Freestanding pedestal menus, sandwich boards or A-frame displays are prohibited.
 - i) **Health and Safety Code.** All applicable provisions of Los Angeles County Code, Title 11: Health and Safety shall be observed in all areas of the restaurant.
7. **Residential open space.**
- a) The minimum required common and private open space area for residential uses is listed in Table 3-5. More than one open space area may be provided on a lot. The sum of square footages for all eligible open space areas on a lot shall comprise the total open space area for that lot. Required side or rear yard areas may be included in the calculated open space area, but a required front yard area shall not be included.
 - b) Open space areas shall have no parking, driveway or right-of-way encroachments.
 - c) Private useable open space shall be contiguous to the residential unit served and screened from public view for privacy. All patios that front a public street shall be substantially enclosed for screening and privacy. All balconies that front a public street shall have solid rails for screening and privacy. Private open space areas that are intended for use by residents only shall not be accessible from the commercial portion of the mixed use development.
8. **Site landscaping.**
- a) A minimum of 10 percent of the lot shall be landscaped with trees, ground cover, shrubbery and flowers, and shall be continuously maintained in good condition. Parking lot landscaping does not count towards this requirement. Incidental walkways may be developed in the landscaped area.
 - b) Chapter 22.126 (Tree Planting Requirements) establishes site tree requirements, including those for surface parking lots.
 - c) The provisions of Section 4.106.5 (Landscape Design), Los Angeles County Code, Title 31: Green Building Standards Code (Title 31), shall also apply.

9. Pedestrian circulation.

- a) Pedestrian walkways shall be a minimum of four feet in width.
- b) The use of asphalt for paving walkways is prohibited.

10. Vehicular circulation.

Vehicular access, drives and circulation routes shall be designed so that all movements involved in parking, turning, or loading shall occur on-site and not within the public right-of-way.

11. Parking lots.

- a) Parking areas shall be designed with end-stall turnarounds or a continuous circulation pattern.
- b) Parking is not permitted in the required street-fronting yard.
- c) Continuous concrete curbs shall be provided as wheel stops where parking adjoins landscaping.
- d) Parking areas shall be separated from buildings by a pedestrian walkway and/or landscape strip.
- e) Separate parking facilities shall be provided for residential uses and commercial uses.

12. Fences, walls, gates and hedges.

- a) Wall sections greater than 50 feet in length fronting a street shall incorporate at least two of the following design features, in proportion to the length of the wall:
 - A minimum 2-foot change in horizontal plane for at least 10 feet.
 - A minimum 18-inch change in height for at least 10 feet.
 - A minimum 18-inch high raised planter for at least half the length of the wall.
 - Use of pilasters at 25-foot maximum intervals and at changes in wall planes.
- b) Section 22.110 (General Site Regulations) establishes the maximum height of fences, walls, and hedges.

13. Utility and mechanical equipment.

- a) All mechanical equipment shall be screened from public view. This includes all ground, wall, and roof mounted equipment.
- b) Screening elements shall be an integral part of the building; no screening method shall give the appearance of being "tacked on."

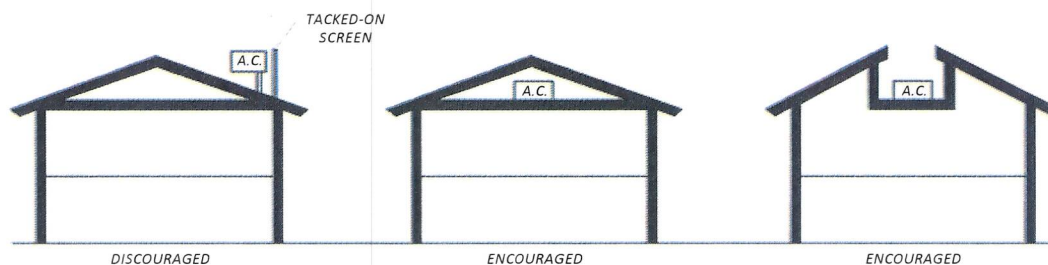


Figure 3-3: Equipment Screening

14. Refuse and recyclable collection facilities.

Separate refuse collection facilities shall be provided and maintained for the residential units in a mixed use development from those for the commercial uses. It shall be clearly marked for residential use only.

D. Performance Standards

The purpose of this section is to ensure that residential uses in Mixed Use Zones are not adversely impacted by adjacent non-residential uses, including but not limited to traffic, noise, light, and safety impacts.

1. **Hours of operation.** The hours of operation for non-residential uses shall be no earlier than 6:00 a.m. and no later than 11:00 p.m. daily, unless modified by a conditional use permit.
2. **Loading.** Loading, unloading and all maintenance activities shall be conducted within the hours of operation noted above, and in such a fashion so as to prevent annoyance to adjacent residents and tenants.
3. **Noise.** Noise generated by activities on the premises shall be controlled in such a manner so as not to create a nuisance or hazard on any adjacent property, in accordance with Chapter 12.08 (Noise Control), Los Angeles County Code, Title 12: Environmental Protection. Residential units shall be constructed and designed to reduce the noise, particularly when located proximate to the Southern Pacific Railroad (SPRR), Metro Blue Line tracks, and Caltrans right-of-way. Proper design may include, but shall not be limited to, building orientation, double-paned windows, wall and ceiling insulation and orientation of vents. Common walls between residential and non-residential uses shall be constructed to minimize the transmission of noise and vibration.
4. **Light and Glare.** All outdoor lighting associated with non-residential uses adjacent to or within the immediate vicinity of residential uses shall be designated with fixtures and poles that illuminate non-residential uses, while minimizing light trespass into residential areas. An unacceptable level of light trespass shall be 0.8 foot-candles or greater when the light trespass falls onto an adjoining residentially-zoned lot, or open space-zoned lot.
5. **Operating activities prohibited.** The following operating activities shall be prohibited in non-residential uses located within mixed use developments:
 - a) Storage or shipping of flammable liquids or hazardous materials beyond that normally associated with a residential use; and
 - b) Welding, machining, or open flame work.
6. **Graffiti.** To encourage the maintenance of exterior walls free from graffiti that would impact pedestrian views, all structures, walls, and fences open to public view shall remain free of graffiti. In the event of such graffiti occurring, the property owner, tenant, or their agent shall remove or cover said graffiti within 72 hours, weather permitting. Paint utilized in covering such graffiti shall be a color that matches, as closely as possible, the color of the adjacent surfaces.
7. **Security.** The residential units shall be designed to ensure the security of residents through the provision of separate and secured entrances and exits that are directly accessible to secured parking areas. Where residential units are in the same structure as a non-residential use, access to residential units shall be from a secured area located on the first floor at the ground level. Non-residential and residential uses located on the same floor shall not have common entrance hallways or common balconies.
8. **Outdoor Electrical Outlets.** All new development shall have electrical outlets such that all landscaping equipment could be electrically operated.

3.6 MLK Medical Zone and MLK Medical Overlay

A. Purpose

The MLK Medical Zone and MLK Medical Overlay are established to meet the existing and future needs of the MLK Medical Center campus, while ensuring compatibility with adjacent land uses. The intent is to maintain and promote medical, clinic, medical office, and associated supportive uses such as incidental retail, supportive residential and parking, and expand pedestrian linkages and connectivity between the MLK Medical Center, CDU, Willowbrook/Rosa Parks Station, and the Willowbrook community.

The MLK Medical Overlay, as identified in Figure 3-1, applies to the two blocks bounded by Wilmington Avenue, East 120th Street, Holmes Street and East 118th Street. The properties within this Overlay are suitable for more intensive uses because of their proximity to Willowbrook/Rosa Parks Station. Besides continuing to allow existing medical and public service uses, additional medical and new residential development are permitted on properties within this Overlay.

B. Land Use Regulations

The land use regulations for the Institutional (IT) Zone contained in Chapter 22.26 shall apply to all development in the MLK Medical Zone and Overlay with the following modifications:

1. **Senior citizen housing development.** Senior citizen housing developments shall be permitted in the MLK Medical Zone and Overlay.
2. **Multi-family residential uses.** Multi-family residential uses shall be permitted in the MLK Medical Overlay.

C. Development and Design Standards

The standards for the IT Zone contained in Chapter 22.26 and general site regulations contained in Chapter 22.110 shall apply to all development in the MLK Medical Zone and Overlay with the following modifications:

1. **Maximum height.**
 - a) The maximum building height of multi-family residential buildings in the MLK Medical Overlay shall be 75 feet and six stories.
2. **Maximum floor area ratio (FAR).**
 - a) The maximum FAR in the MLK Medical Zone shall be 1.65.
 - b) The maximum FAR in the MLK Medical Overlay shall be 2.5.
3. **Maximum residential density.**
 - a) The maximum residential density in the MLK Medical Overlay shall be 60 dwelling units/acre.

D. Performance Standards

The purpose of this section is to ensure that residential uses in MLK Medical Zone and Overlay are not adversely impacted by adjacent medical and other non-residential uses. Performance Standards in Section 3.5.D shall apply to all development where both residential and non-residential uses are present.

3.7 Drew Educational Zone

A. Purpose

The Drew Educational Zone is established to meet the existing and future needs of CDU and King Drew Magnet High School, while ensuring compatibility with adjacent land uses. The intent is to create a medical university campus for CDU by maintaining and promoting educational and associated support uses, while maintaining sensitivity to surrounding development; as well as expand pedestrian linkages and connectivity between the CDU campus, MLK Medical Center, Willowbrook/Rosa Parks Station, Kenneth Hahn Plaza, and the Willowbrook community. The future CDU Master Plan shall comply with the regulations and standards contained in this Section 3.7.

B. Land Use Regulations

The land use regulations for the Institutional (IT) Zone contained in Chapter 22.26 shall apply to all development in the Drew Educational Zone with the following additions:

1. **Student and faculty housing.** Student and faculty housing shall be permitted in the Drew Educational Zone.
2. **Restaurants and supportive retail.** Restaurant and supportive retail uses shall be permitted in the Drew Educational Zone as an ancillary use providing necessary support to CDU's primary activities or operations.

C. Development and Design Standards

The standards for the IT Zone contained in Chapter 22.26 shall apply to all development in the Drew Educational Zone, with the following modifications:

1. **Maximum height.**

The maximum building height shall be 75 feet and 6 stories. New development fronting Compton Avenue shall be limited to 50 feet and four stories for the first 100 feet from the property line fronting Compton Avenue. Development standards related to height per Chapter 22.04 and Chapter 22.110 of Title 22 shall also apply.
2. **Maximum floor area ratio (FAR).**

The maximum FAR in the Drew Educational Zone shall be 1.5.
3. **Street-fronting yard depths.**

The minimum street-fronting yard depths shall be 15 feet from Compton Avenue and 10 feet from East 118th Street, East 120th Street, and Holmes Avenue.
4. **Required residential open space.**
 - a) The minimum open space area requirements for multi-family developments in the Drew Educational Zone shall be as follows:
 - i. 50 square feet/dwelling unit of common open space area with a minimum dimension of 25 feet; and
 - ii. 50 square feet/dwelling unit of private open space area with a minimum dimension of 5 feet.

- b) More than one open space area may be provided on a lot. The sum of square footages for all eligible open space areas on a lot shall comprise the total open space area for that lot. Required side or rear yard areas may be included in the calculated open space area, but a required front yard area shall not be included.
- c) Open space areas shall have no parking, driveway or right-of-way encroachments.
- d) Private useable open space shall be contiguous to the residential unit served and screened from public view for privacy. All patios that front a public street shall be substantially enclosed for screening and privacy. All balconies that front a public street shall have solid rails for screening and privacy.
- e) Private open space areas that are intended for use by residents only shall not be accessible from the non-residential portion of the development.

5. Site landscaping.

- a) A minimum of 20 percent of the lot shall be landscaped with trees, ground cover, shrubbery and flowers, and shall be continuously maintained in good condition. Parking lot landscaping does not count towards this requirement. Incidental walkways may be developed in the landscaped area.
- b) Chapter 22.126 (Tree Planting Requirements) establishes site tree requirements, including those for surface parking lots.
- c) The provisions of Section 4.106.5 (Landscape Design) of Title 31 shall also apply.

6. Pedestrian circulation.

- a) Pedestrian walkways shall be a minimum of four feet in width.
- b) The use of asphalt for paving walkways is prohibited.

7. Vehicular circulation.

Vehicular access, drives and circulation routes shall be designed so that all movements involved in parking, loading, or turning shall occur onsite and not within the public right-of-way.

8. Parking lots.

- a) Parking areas shall be designed with end-stall turnarounds or a continuous circulation pattern.
- b) Parking is not permitted in the required street-fronting yard.
- c) Continuous concrete curbs shall be provided as wheel stops where parking adjoins landscaping.
- d) Parking areas shall be separated from buildings by a pedestrian walkway and/or landscape strip.
- e) Separate parking facilities shall be provided for residential uses and institutional uses.

9. Fences, walls, gates, and hedges.

Wall sections greater than 50 feet in length fronting a street shall incorporate at least two of the following design features, in proportion to the length of the wall:

- A minimum 2-foot change in horizontal plane for at least 10 feet.
- A minimum 18-inch change in height for at least 10 feet.
- A minimum 18-inch high raised planter for at least half the length of the wall.
- Use of pilasters at 25-foot maximum intervals and at changes in wall planes.

10. Utility and mechanical equipment.

- a) All mechanical equipment shall be screened from public view. This includes all ground, wall, and roof-mounted equipment.
- b) Screening elements shall be an integral part of the building; no screening method shall give the appearance of being “tacked on.”

3.8 Imperial Commercial Zone

A. Purpose

The Imperial Commercial Zone is established to meet the commerce and service needs of the resident and business communities, while ensuring compatibility with adjacent land uses. The intent is to maintain and promote commercial uses between Imperial Highway and the I-105 Freeway. The Imperial Commercial Zone provides for the development of a broad range of retail and service uses, as well as freeway-oriented, regional-serving retail, office complexes, and light manufacturing businesses.

B. Land Use Regulations

The land use regulations for the Unlimited Commercial (C-3) Zone contained in Chapter 22.20 shall apply to all development in the Imperial Commercial Zone, with the following additions and exceptions:

1. **Self-service storage facilities.** A self-service storage facility shall be permitted in the Imperial Commercial Zone with a Conditional Use Permit (CUP) application (Chapter 22.158), and subject to the provisions of Section 22.140.560.
2. **Limitations on use.** The following uses shall not be permitted in the Imperial Commercial Zone:
 - a) Liquor stores.
 - b) Tobacco and vape shops.
 - c) New residential uses.

C. Development and Design Standards

The standards for the C-3 Zone contained in Chapter 22.20 shall apply to all new development in the Imperial Commercial Zone, with the following modifications:

1. **Maximum lot coverage.**

The maximum lot coverage by structures of any type in the Imperial Commercial Zone shall be 50 percent.
2. **Maximum height.**

The maximum building height in the Imperial Commercial Zone shall be 35 feet and two stories. Development standards related to height per Chapter 22.04 and Chapter 22.110 shall also apply.
3. **Maximum floor area ratio (FAR).**

The maximum FAR in the Imperial Commercial Zone shall be 1.0.
4. **Site landscaping.**
 - a) A minimum of 10 percent of the lot shall be landscaped with trees, ground cover, shrubbery and flowers, and shall be continuously maintained in good condition. Parking lot landscaping does not count towards this requirement. Incidental walkways may be developed in the landscaped area.
 - b) Chapter 22.126 (Tree Planting Requirements) establishes site tree requirements, including those for surface parking lots.

c) The provisions of Section 4.106.5 (Landscape Design) of Title 31 shall also apply.

5. Pedestrian circulation.

- a) Pedestrian walkways shall be a minimum of four feet in width.
- b) The use of asphalt for paving walkways is prohibited.

6. Vehicular circulation.

Vehicular access, drives and circulation routes shall be designed so that all movements involved in parking, loading or turning shall occur on-site and not within the public right-of-way.

7. Parking lots.

- a) Parking areas shall be designed with end-stall turnarounds or a continuous circulation pattern.
- b) Parking is not permitted in the required street-fronting yard.
- c) Continuous concrete curbs shall be provided as wheel stops where parking adjoins landscaping.
- d) Parking areas shall be separated from buildings by a pedestrian walkway and/or landscape strip.

8. Utility and mechanical equipment.

- a) All mechanical equipment shall be screened from public view. This includes all ground, wall, and roof mounted equipment.
- b) Screening elements shall be an integral part of the building; no screening method shall give the appearance of being "tacked on."

3.9 Willowbrook Residential 1 Zone

A. Purpose

The Willowbrook Residential 1 Zone is established to preserve and enhance desirable characteristics of single-family residential areas.

B. Land Use Regulations

The land use regulations for the Single-Family Residence (R-1) Zone contained in Chapter 22.18 shall apply to all development in the Willowbrook Residential 1 Zone, with the following modification:

Noncommercial keeping of chickens. The noncommercial keeping of chickens shall be permitted as an accessory use in the Willowbrook Residential 1 Zone, subject to the following provisions:

- a) No more than five chickens per residence are allowed, located within the rear yard and kept outside the dwelling unit.
- b) All such chickens shall be kept in an enclosure within the rear yard and located a minimum of 35 feet from any habitable building.
- c) Roosters shall not be permitted.
- d) For the purposes of this section, the occasional sale of eggs or chickens, which is incidental to the keeping of chickens as a hobby, does not constitute a commercial use.

C. Development and Design Standards

The standards for the R-1 Zone contained in Chapter 22.18 shall apply to all development in the Willowbrook Residential 1 Zone, with the following modifications:

1. Maximum height.

The maximum height permitted in the Willowbrook Residential 1 Zone shall be 35 feet and two stories. Development standards related to height per Chapter 22.04 and Chapter 22.110 shall also apply.

2. Minimum floor area.

The minimum floor area of a new single family residence in the Willowbrook Residential 1 Zone shall be 1,200 square feet.

3. Driveways and walkways.

The use of asphalt for paving driveways and walkways is prohibited.

4. Front yard landscaping.

- a) With the exception of the required paved driveway and walkway having a width not to exceed four feet, all areas within the street-fronting yard shall be landscaped and maintained.
- b) Where possible, existing trees shall be maintained.
- c) The provisions of Section 4.106.5 (Landscape Design) of Title 31 shall also apply.

5. Fences.

- a) If street fencing is required for security reasons, wrought iron style fences which do not obscure views may be permitted to the maximum height of six feet within front yards and corner side yards, subject to Director's review.
- b) Those portions of fences more than three and one-half feet high must be substantially open (75 percent transparency), except for pillars used in conjunction with wrought iron fences and shall not cause a significant visual obstruction.
- c) Barbed wire and chain link fencing are prohibited.
- d) Hanging, displaying or drying clothes on fencing is not permitted.

6. Clotheslines.

Clotheslines or clothesline structures are permitted, provided they are located to the rear of the structure, and not visible from adjoining streets when viewed at ground level.

7. Utility and mechanical equipment.

- a) All utility and mechanical equipment shall be placed in locations that are not exposed to view from the street or they shall be screened from view. Satellite dishes shall also be located away from public view.
- b) Screening elements shall be an integral part of the building; no screening method shall give the appearance of being "tacked on."

3.10 Willowbrook Residential 2 Zone

A. Purpose

The purpose of the Willowbrook Residential 2 Zone is to preserve and enhance single-family neighborhood characteristics, while also providing an environment suitable for two-family residences. The intent is to promote desirable characteristics of low to medium density neighborhoods.

B. Land Use Regulations

The land use regulations for the Two-Family Residence (R-2) Zone contained in Chapter 22.18 shall apply to all development in the Willowbrook Residential 2 Zone.

C. Development and Design Standards

The standards for the R-2 Zone contained in Chapter 22.18 shall apply to all development in the Willowbrook Residential 2 Zone, with the following modifications:

1. Maximum height.

The maximum height permitted in the Willowbrook Residential 2 Zone shall be 35 feet and two stories. Development standards related to height per Chapter 22.04 and Chapter 22.110 shall also apply.

2. Driveways and walkways.

The use of asphalt for paving driveways and walkways is prohibited.

3. Front yard landscaping.

- a) With the exception of the required paved driveway and walkway having a width not to exceed four feet, all areas within the street-fronting yard shall be landscaped and maintained.
- b) Where possible, existing trees shall be maintained.
- c) The provisions of Section 4.106.5 (Landscape Design) of Title 31 shall also apply.

4. Fences.

- a) If street fencing is required for security reasons, wrought iron style fences which do not obscure views may be permitted to the maximum height of six feet within front yards and corner side yards, subject to Director's review.
- b) Those portions of fences more than three and one-half feet high must be substantially open (75 percent transparency), except for pillars used in conjunction with wrought iron fences and shall not cause a significant visual obstruction.
- c) Barbed wire and chain link fencing are prohibited.
- d) Hanging, displaying or drying clothes on fencing is not permitted.

5. Clotheslines.

Clotheslines or clothesline structures are permitted, provided they are located to the rear of the structure, and not visible from adjoining streets when viewed at ground level.

6. Utility and mechanical equipment.

- a) All utility and mechanical equipment shall be placed in locations that are not exposed to view from the street or they shall be screened from view. Satellite dishes shall also be located away from public view.
- b) Screening elements shall be an integral part of the building; no screening method shall give the appearance of being “tacked on.”

3.11 Willowbrook Residential 3 Zone

A. Purpose

The Willowbrook Residential 3 Zone is established to provide opportunities for developments containing multiple units, such as apartments or condominiums. The intent is to promote desirable characteristics of medium density neighborhoods, and provide a variety of housing options to serve the needs of the Willowbrook community.

B. Land Use Regulations

The land use regulations for the Limited Multiple Residence (R-3) Zone contained in Chapter 22.18 shall apply to all development in the Willowbrook Residential 3 Zone.

C. Development and Design Standards

The standards for the R-3 Zone contained in Chapter 22.18 shall apply to all development in the Willowbrook Residential 3 Zone, with the following modifications:

1. Maximum height.

The maximum height permitted in the Willowbrook Residential 3 Zone shall be 35 feet and three stories. Development standards related to height per Chapter 22.04 and Chapter 22.110 shall also apply.

2. Required residential open space.

- a) The minimum open space area requirements for multi-family developments in the Willowbrook Residential 3 Zone shall be as follows:
 - i. 50 square feet/dwelling unit of common open space area with a minimum dimension of 25 feet; and
 - ii. 50 square feet/dwelling unit of private open space area with a minimum dimension of 5 feet.
- b) More than one open space area may be provided on a lot. The sum of square footages for all eligible open space areas on a lot shall comprise the total open space area for that lot. Required side or rear yard areas may be included in the calculated open space area, but a required front yard area shall not be included.
- c) Open space areas shall have no parking, driveway or right-of-way encroachments.
- d) Private useable open space shall be contiguous to the residential unit served and screened from public view for privacy. All patios that front a public street shall be substantially enclosed for screening and privacy. All balconies that front a public street shall have solid rails for screening and privacy.

3. Site landscaping.

- a) A minimum of 20 percent of the lot shall be landscaped or hardscaped with open, useable outdoor space. Required common open space areas may be counted in the calculated landscaped area.
- b) With the exception of the required paved driveway and walkway(s), all areas within the street-fronting yard shall be landscaped and maintained.
- c) Chapter 22.126 (Tree Planting Requirements) establishes site tree requirements, including those for surface parking lots.
- d) The provisions of Section 4.106.5 (Landscape Design) of Title 31 shall also apply.

4. Pedestrian circulation.

- a) Pedestrian walkways shall be a minimum of four feet in width.
- b) The use of asphalt for paving walkways is prohibited.

5. Vehicular circulation.

- a) Vehicular access, drives and circulation routes shall be designed so that all movements involved in loading, parking, or turning shall occur onsite and not within the public right-of-way.
- b) Principal vehicular access into multi-family developments shall be through an entry driveway, rather than a parking aisle. Entry driveways lead to designated parking and shall not be lined with or offer direct access to parking spaces.

6. Parking.

- a) Parking for multi-family developments shall be provided on site in on-grade or underground structures, surface parking lots, carports, or attached garages.
- b) Parking shall be located proximate to the building and located to the rear of the parcel wherever possible. Parking shall be designed to avoid awkward turning maneuvers and the backing of vehicles into public streets. Parking areas shall be designed with end-stall turnarounds or a continuous circulation pattern.
- c) Parking shall not occupy more than 30 percent of any linear street frontage.
- d) Parking is not permitted in the street-fronting yard.
- e) Continuous concrete curbs shall be provided as wheel stops where parking adjoins landscaping.
- f) Parking areas shall be separated from buildings by a pedestrian walkway and/or landscape strip.
- g) Carports and parking structures shall be architecturally compatible with the design of the main structures in the project.

7. Fences, walls, gates, and hedges.

- a) If street fencing is required for security reasons, wrought iron style fences which do not obscure views may be permitted to the maximum height of six feet within front yards and corner side yards, subject to Director's review. Those portions of fences more than three and one-half feet high must be substantially open (75 percent transparency), except for pillars used in conjunction with wrought iron fences and shall not cause a significant visual obstruction.
- b) Barbed wire and chain link fencing are prohibited.
- c) Hanging, displaying or drying clothes on fencing is not permitted.

- d) Wall sections greater than 50 feet in length fronting a street shall incorporate at least two of the following design features, in proportion to the length of the wall:
- A minimum 2-foot change in horizontal plane for at least 10 feet.
 - A minimum 18-inch change in height for at least 10 feet.
 - A minimum 18-inch high raised planter for at least half the length of the wall.
 - Use of pilasters at 25-foot maximum intervals and at changes in wall planes.
8. **Utility and mechanical equipment.**
- a) All utility and mechanical equipment shall be screened from public view. This includes all ground, wall, and roof mounted equipment. Satellite dishes shall also be located away from public view.
- b) Screening elements shall be an integral part of the building; no screening method shall give the appearance of being “tacked on.”
9. **Refuse and recyclable collection facilities.**
- Refuse enclosures shall be located for the convenience of the residents and be easily accessible for refuse and recyclables collection, but shall not impede circulation during loading operations to the extent feasible.

3.12 Open Space (O-S) Zone

A. General

The provisions of Chapter 22.16 shall apply to all development in the Open Space (O-S) Zone in the Plan Area.

3.13 Parking

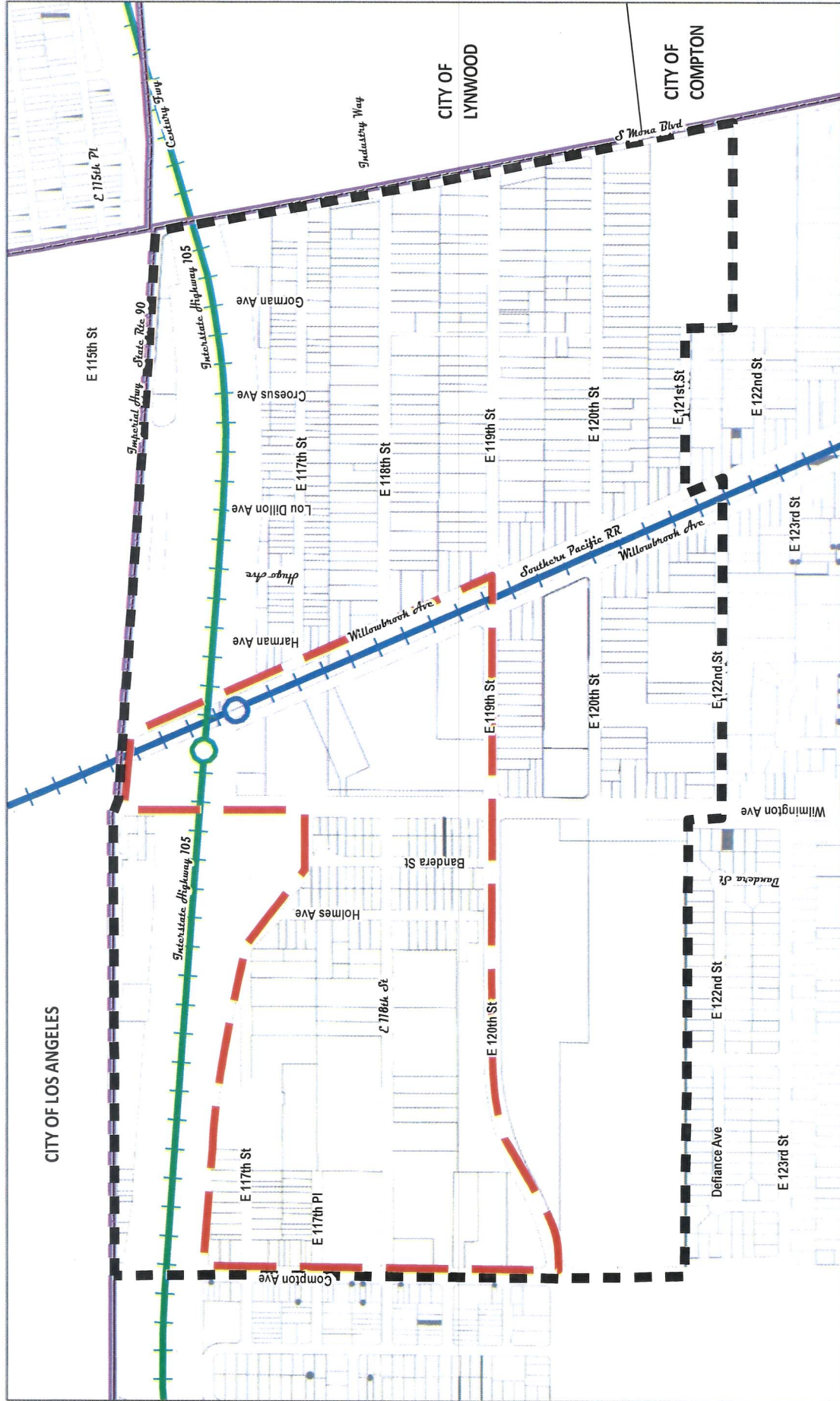
A. Introduction

The Willowbrook TOD Specific Plan provides modifications to the parking requirements contained in Chapter 22.112. These modifications are intended to provide a parking supply that is more closely tailored to transit-oriented development and to allow greater flexibility in the provision of required parking.

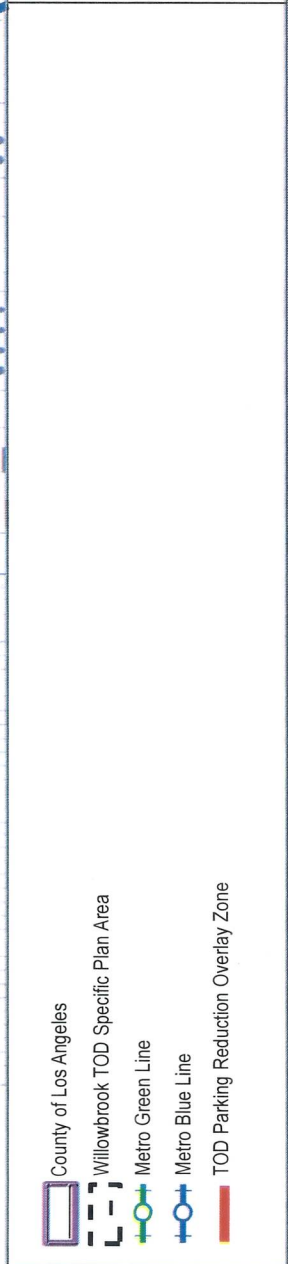
B. Parking Requirements

The standards for parking requirements contained in Chapter 22.112 shall apply to all development in the Plan Area with the following modifications:

1. This Specific Plan establishes a TOD Parking Reduction Overlay Zone with reduced parking requirements and maximum parking standards. The boundaries for this zone are shown in Figure 3-4. Within the TOD Parking Reduction Overlay Zone, the following standards apply:
 - a) **Residential uses.** Minimum and maximum parking standards for residential uses are established in Table 3-6.
 - b) **Non-residential uses.** Maximum parking standards for non-residential uses are established in Chapter 22.112. Minimum parking standards for non-residential uses are 40 percent of the maximum requirement.



**Figure 3-4
TOD Parking Reduction
Overlay Zone**



- County of Los Angeles
- Willowbrook TOD Specific Plan Area
- Metro Green Line
- Metro Blue Line
- TOD Parking Reduction Overlay Zone

5 minute walk (1,200')

0 325 650 1,300 1,500
Feet

**Table 3-6:
Parking Requirements for Residential Uses**

Land Use	Minimum	Maximum
Residential		
Single Family	2.0/DU	2.0/DU
Bachelor Apartment	0.60/DU	1.0/DU
1-Bed Apartment	0.90/DU	1.5/DU
2-Bed Apartment	1.20/DU	2.0/DU
Guest ¹	0.15/DU	0.25/DU

1. Guest parking is only required when the number of required guest spaces is greater than one.

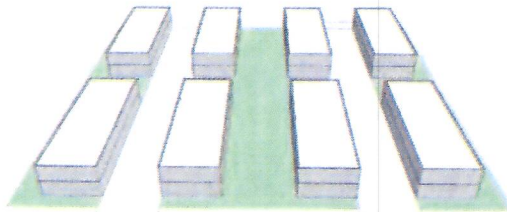
2. Parking requirements for non-residential uses may be met within 1,500 feet from the site of the development.
3. A Transportation Demand Management Program shall be implemented for all new non-residential uses in the Specific Plan Area exceeding 50,000 square feet to reduce vehicle trips and reduce parking needs in the TOD (as identified in Chapter 4, Section 4.7).
4. Required residential parking shall be provided on site; and is not permitted within the required front yard or corner side yard. Required parking for new single family uses may be uncovered. A Ministerial Site Plan Review (SPR) application, as provided in Chapter 22.188, is required for the conversion of covered parking for existing single family uses to uncovered parking.
5. Tandem parking is allowed for single-family uses.
6. The provisions regarding shared parking in Chapter 22.180 (Parking Permits) of Title 22 shall apply.

3.14 Design Guidelines for Mixed Use, Commercial and Institutional Uses

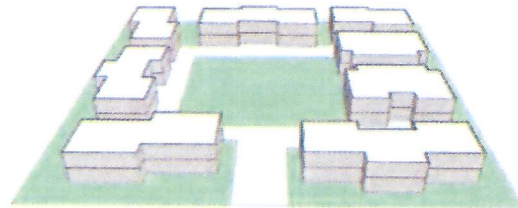
The purpose of this section is to facilitate high quality development for mixed use, institutional and commercial uses within the Plan Area. These design guidelines address site planning and building design and are in addition to the above land use regulations and development standards contained in this chapter.

1. Building siting and orientation.

- a) Buildings should be sited to avoid random and irregular building relationships, and arranged to create a sense of unity and overall harmony. To the maximum extent possible, new structures should be clustered to create plazas and open space areas and avoid creating of "barrack-like" rows of structures. Where clustering is impractical, a visual link between separate structures should be established. This link can be accomplished through the use of an arcade system, trellis or other open structure.

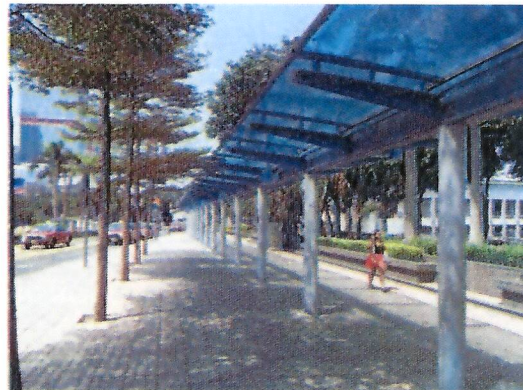
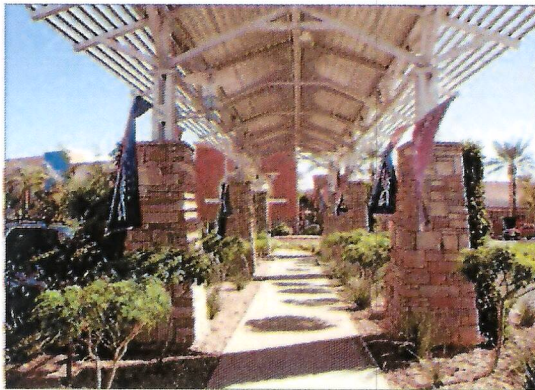


Avoid "barrack-like" layout



Orient buildings to create useable open space

Figure 3-5: Site Layout to Create Usable Space

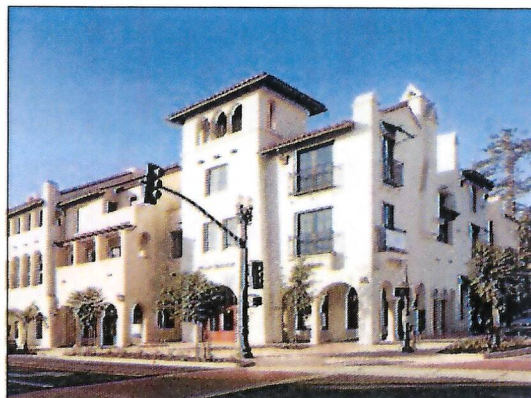
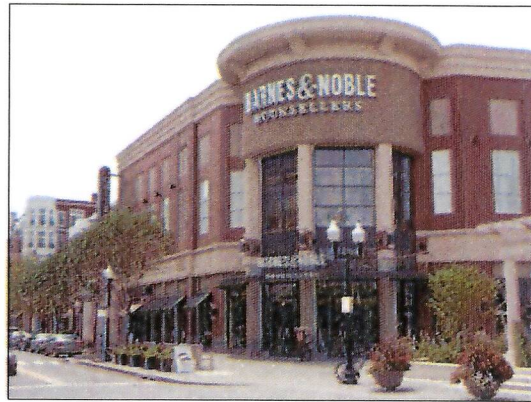


Arcades and trellises provide shade, connections and define usable space.

- b) Commercial facilities in mixed use projects should be oriented to the street, with parking generally located in the rear, side or underneath buildings. The perimeter of parking areas and driveways adjacent to streets and sidewalks should be screened with an attractive low wall, berm, fence or landscaping.
- c) Existing residential uses should be buffered from incompatible commercial development to mitigate negative impacts due to noise, vibration, shading, light and glare, and aesthetics. Intensified landscaping, increased yard depths and/or appropriate building orientation should be utilized as a means of providing adequate separation between such land uses.
- d) Entry and edge design features such as landscaping, architectural signage and monumentation, and/or enhanced paving should be incorporated.

2. Building modulation and detailing.

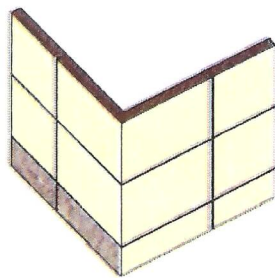
- a) Building design should avoid large monotonous facades, long straight-line building fronts, plain box shapes, and barren exterior treatment. All building elevations visible from a public way, including freeways, should be highly articulated and incorporate the chosen design theme in a consistent manner.



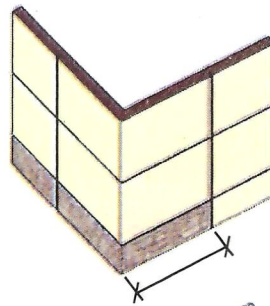
Varied building forms, volumetric and planar changes, and variations in roof forms and height, contribute to a well-articulated building massing.

- b) Commercial facades of mixed use projects should be modulated at least every 50 feet by changes in building mass or facade treatment, such as projected entrance windows, roof form or other architectural features.
- c) Building facades should be designed to give individual identity to each vertical module of residential units, using techniques such as providing a deep notch between the modules; varying architectural elements between units (e.g., window color, roof shape, window shape, stoop detail, railing type); providing porches and balconies; varying color or materials of each individual module within a harmonious palette of colors and materials, etc.
- d) Building form and articulation should be used to accentuate public entrances, de-emphasize service areas, and define and shelter pedestrian walkways and exterior spaces.
- e) Awnings, canopies and arcades should be used as appropriate to provide visual interest and shade.
- f) Buildings should be treated as a whole and finished appropriately on all sides to provide continuity. Backs of buildings shall use similar materials; however, less expensive and more utilitarian substituted materials are acceptable, provided they are compatible with the overall design.

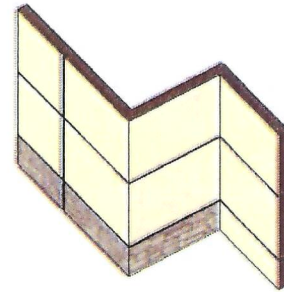
- g) Material changes shall not occur at corners. The same material shall continue around corners for a minimum distance of four feet. If feasible, the same material should continue to the next change in the wall plane.



Prohibited
Change of material at corner



Required
Continue material around corner
for a minimum of four feet



Encouraged
Continue material around corner
to a change in wall plane

Figure 3-6: Treatments for Material Changes at Corners

3. Building entrances.

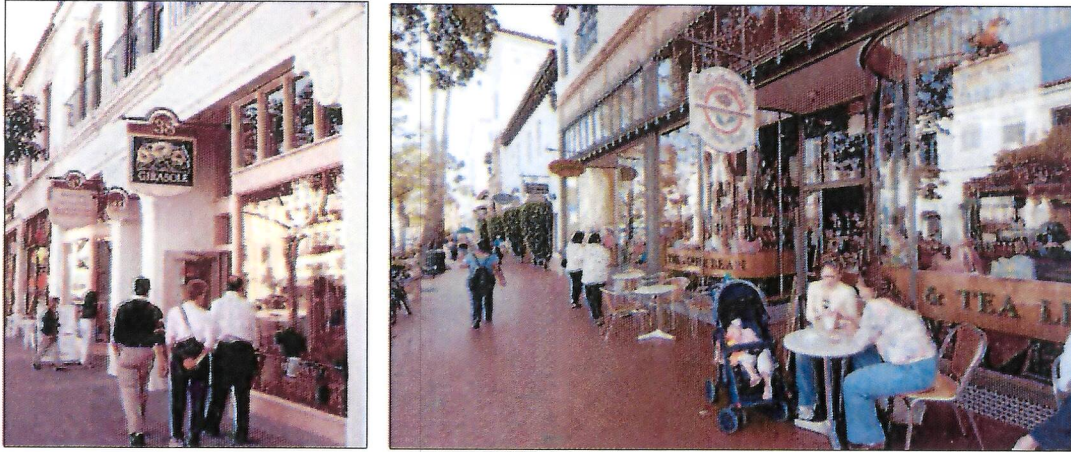
- In mixed use projects, when residential and commercial uses are combined in the same structure, separate entrances shall be provided for each use.
- The main entrance or entrances should be emphasized and oriented to the street or major plazas or open space. Main entries to buildings should be clearly demarcated, visible and accessible from the street and/or pedestrian walkways. Examples include the use of special materials, color, detailing, or equivalent architectural treatment.
- Secondary entries may be from parking areas.



Entrances should be clearly demarcated. Entrances may be emphasized with building elements, material and color variations and signage.

4. Transparency.

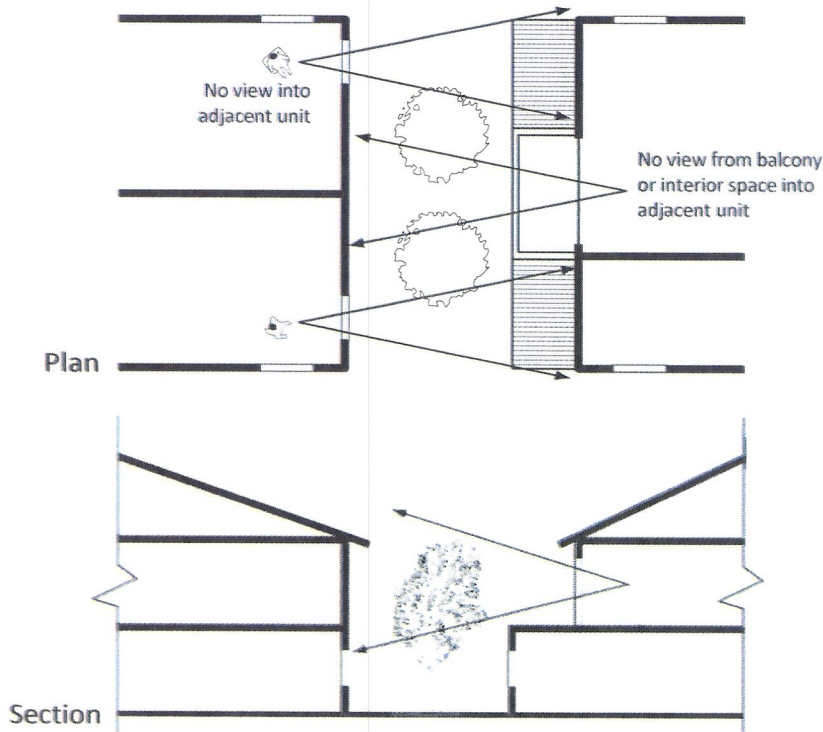
- Placement and orientation of doorways and windows should create strong, direct relationships with the street. Street-facing façades of all buildings shall incorporate windows and openings providing light to adjacent spaces and rooms, and to create visual interest and provide “eyes on the street.”
- Windows and openings shall provide clear and transparent views into the ground floor retail uses or shall display merchandise to reinforce a pedestrian scale. Dark tinted, reflective or opaque glazing is not permitted for any required wall opening along street level retail façades.



Retail uses shall have transparent windows that help activate the street.

5. Privacy for residential units.

- a) Buildings should be oriented to promote privacy to the greatest extent possible. In mixed use projects, residential windows should face away from loading areas and docks. To the extent residential windows face the windows of an adjacent unit; the windows should be offset to maximize privacy.
- b) Windows, balconies or similar openings should be oriented so as not to have a direct line-of-sight into adjacent units within the development. In addition, units above the first story should be designed so that they do not look directly onto private patios or backyards of adjoining residential property or units.



Use appropriate landscaping and offset windows and balconies (or patios) to maintain privacy between residential units

Figure 3-7: Privacy for Residential Units

6. Vehicle circulation and access.

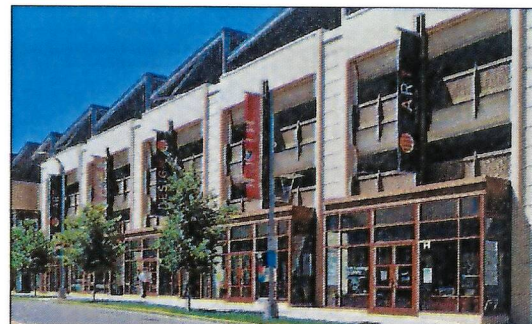
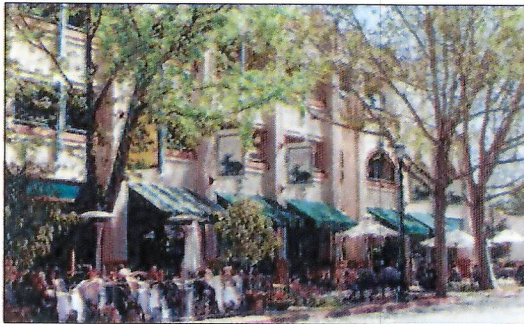
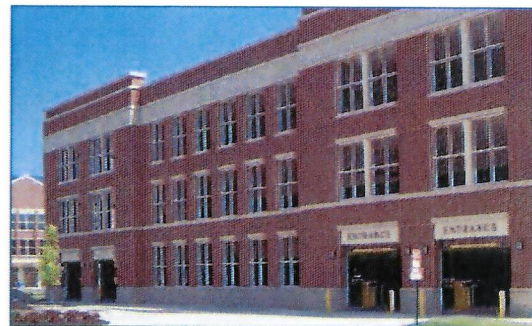
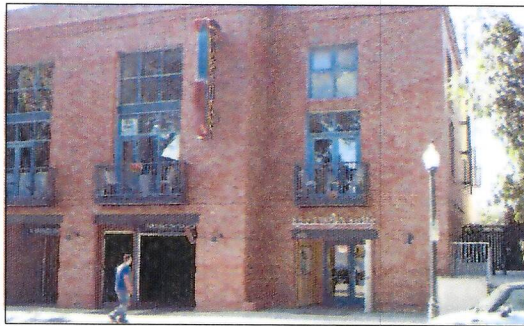
- a) Site access and internal circulation in all developments shall be safe, efficient, and convenient. Vehicular traffic should be adequately separated from pedestrian circulation. Vehicular entrances shall be clearly identified to minimize pedestrian/vehicle conflict.
- b) The number of site access points or driveway aprons should be minimized for aesthetic purposes, to achieve efficient and productive use of paved pathways, and to eliminate traffic hazards. They should be located as far as possible from street intersections and be coordinated with existing or planned median openings and driveways on the opposite side of the roadway. Common driveways that promote access to more than one site are encouraged.

7. Parking lots.

- a) Parking lots should be designed with a clear hierarchy of circulation: major access drives with no direct access to parking spaces; major circulation drives with little or no parking; and parking aisles for direct access to parking spaces. Loading and service areas should be provided with separate access and circulation whenever possible.
- b) The visual impact of parking lots should be mitigated with landscaping or screened by buildings.

8. Parking structures.

- a) The massing, height and architectural design of the parking structure should be consistent and compatible with the rest of the development within which the parking structure is to be located.
- b) Vehicular access to parking structures should be designed so as not to obstruct free flow of traffic on adjoining public streets. Vehicular entrances and exits should be located so as to minimize turning movement conflicts as vehicles enter and exit the structure.
- c) Pedestrian access points to the parking structure should be located to avoid pedestrian/vehicle conflicts.



Parking structures should be designed to integrate with the area they serve. Well-designed structures, ground-level storefronts, and enhanced landscape treatments can minimize the visual impact of parking structures.

9. Pedestrian circulation.

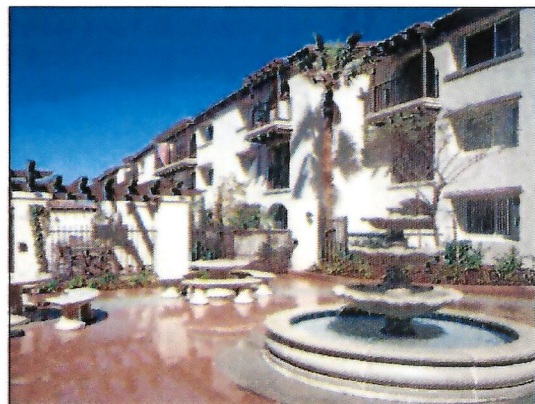
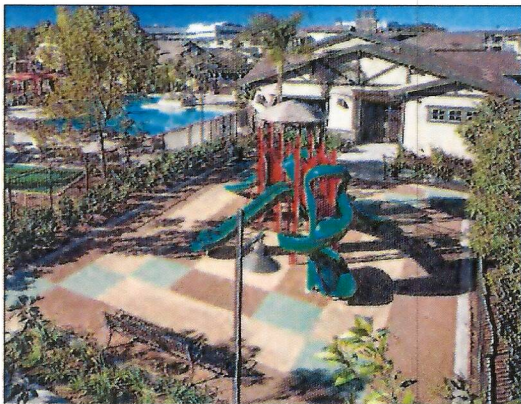
- a) All developments shall be designed to enhance pedestrian movement within the project and to adjacent uses. The design of new projects shall promote pedestrian walkability and connectivity.
- b) Pedestrian walkways between parking areas and buildings shall be provided. Pedestrian walkways should also be provided to link dwelling units with commercial or institutional uses in a mixed use or institutional development respectively, as well as with common open space, plazas and courtyards, and public sidewalks.
- c) Where pedestrian paths or walkways cross parking areas or driveways, decorative paving or some other method shall be used to define the pedestrian space and delineate crossings.
- d) Pedestrian paths or walkways to building entrances shall be paved with high quality paving materials such as, but not limited to, stone pavers, decorative concrete pavers and brick.

10. Residential open space.

- a) Common open space areas shall be convenient to the residential portion of a mixed use development, separate and secure from the non-residential component, and clearly marked for residential use only.
- b) Common open space areas shall contain amenities appropriate to the project's size; i.e. pools, spas, recreation buildings are encouraged for larger projects, while barbeque areas and gazebos may be more appropriate for smaller projects. Children's play areas shall be sited to be visible from residential units and not visible from the public right-of-way.



Play areas can be incorporated into the common areas.



Pools, spas and other recreational amenities are encouraged for larger projects.

- c) In mixed use projects, accessible rooftop open space may be used as common useable open space by the residents. It may also be used for private useable open space, when the space is directly accessible from the unit(s).

11. Fences, walls, gates and hedges.

- a) Walls and fences shall be planned and designed as integral parts of all projects as necessary to screen vehicles, loading and storage areas, and utility structures, and shall be consistent with the landscaping and building design. Decorative types of view fencing, such as wrought iron, are generally encouraged for street fencing.

12. Exterior lighting.

- a) Exterior lighting shall be used to provide illumination for the security and safety of on-site areas such as building entrances, parking, loading, shipping and receiving, walkways, and working areas. The design of light fixtures and their structural support shall be architecturally compatible with the main buildings on-site.
- b) Site, parking lot and building security lighting shall not impact surrounding or neighboring properties. The type and location of such lighting shall preclude direct glare onto adjoining property, streets, or skyward. All lighting fixtures must be shielded to confine light spread on-site.
- c) In mixed use projects, all site, parking lot and building security lighting shall be appropriately shielded so as not to spill over, into or otherwise adversely impact the residential portion of a mixed use development.

13. Loading areas.

- a) Loading areas should be designed to prevent interference with vehicular circulation and parking, and to provide an unobstructed area for trucks to maneuver when accessing loading spaces.
- b) Loading areas should be located away from main customer entrances and the street, preferably toward the rear of the property.
- c) Overhead (roll-up) doors shall not be directly open to public view, and shall be substantially screened from the street and residentially zoned properties. Screening may be accomplished in a variety of ways, including the use of wing walls, the recessing of overhead doors (building articulation), landscaping, or a combination of these techniques. Fixed hardware for roll up doors shall be located on the inside of buildings to minimize visual clutter. If located adjacent to a residential use, the design of overhead doors shall minimize noise through devices such as rubber seals and/or other dampening features.

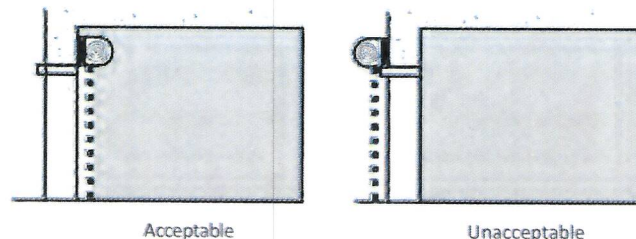


Figure 3-8: Roll-Up Doors

14. Outdoor storage and service areas.

- a) Outdoor storage and service areas (including, but not limited to, service entrances, loading docks and bays, outdoor storage of commercial vehicles) shall be clearly defined and designated for convenient access. They shall not conflict with vehicular access, on-site parking facilities, pedestrian walkways, and customer entrances.
- b) All outdoor storage and service areas for commercial uses shall be located so as to be convenient to the commercial users and where associated odors and noise will not adversely impact the residential units in a mixed use project, or adjacent residential uses.
- c) Outdoor storage and service areas shall not be visible from public right-of-ways and shall be screened from on-site and off-site public view with a combination of building features, decorative walls and landscaping consistent with the architectural style of the building.

15. Refuse and recyclable collection facilities.

- a) Refuse and recyclable storage shall be enclosed within or adjacent to the main structure or located within separate freestanding enclosures. The location of refuse and recyclable collection facilities shall be coordinated with the location of loading/service areas, and not be readily visible to public view.
- b) In mixed use developments, all refuse collection facilities for commercial uses shall be located so as to be convenient to the commercial users and where associated odors and noise will not adversely impact the residential uses.
- c) Screening for refuse and recyclable collection facilities shall use a combination of building features, decorative walls and landscaping consistent with the architectural style of the building.

3.15 Design Guidelines for Multi-Family Residential Uses

The purpose of this section is to facilitate high quality multi-family residential development within the Plan Area. These design guidelines address site planning and building design and are in addition to the above land use regulations and development standards for multi-family uses in this chapter.

1. Scale and mass.

- a) Multi-family residential projects should avoid having a massive "big-box" appearance. Multi-family units in large projects should be clustered, or broken up into groups of structures. Windows should be provided facing the street for all units adjacent to the street to break up massing and to contribute to "eyes on the street" which helps neighborhood security.

2. Building orientation and entrances.

- a) Multi-family developments should have a strong street presence with public entrances oriented towards the street. The majority of unit entrances must be accessed from the street or from the main open space. Where there is a common building entrance for all units, it should be accessed from the street or main open space.
- b) Multi-family developments should have transitional spaces in the form of stoops, overhangs and porches between public areas and entrances to the units.

3. Building articulation and detailing.

- a) Multi-family building design should avoid large monotonous façades, long straight line building fronts, plain box shapes, and barren exterior treatment. All building elevations visible from a public right-of-way should be articulated and incorporate the chosen design theme in a consistent manner.
- b) Multi-family building facades should be designed so as to give individual identity to each vertical module of residential units, using techniques such as providing a deep notch between the modules; varying architectural elements between units (e.g., window color, roof shape, window shape, stoop detail, railing type); providing porches and balconies; varying color or materials of each individual module within a harmonious palette of colors and materials, etc.



Articulate buildings with architectural detailing, staggered building planes, material and color changes, and varied roof lines.

4. Privacy for residences.

- a) Multi-family buildings should be oriented to promote privacy to the greatest extent possible. Windows, balconies or similar openings above the first story should be oriented so as not to have a direct line-of-sight into adjacent units within the development.
- b) Multi-family units above the first story should be designed so that they do not look directly onto private patios or backyards of adjoining residential property or units. Landscaping should be used to aid in privacy screening in ground floor units.

5. Vehicle circulation and access.

- a) Site access and internal circulation in multi-family developments shall be safe, efficient, and convenient. Vehicular traffic shall be adequately separated from pedestrian circulation. Vehicular entrances shall be clearly identified and easily accessible to minimize pedestrian/vehicle conflict.
- b) The number of site access points or driveway aprons should be minimized for aesthetic purposes, to achieve efficient and productive use of paved accessways, and to eliminate traffic hazards. They should be located as far as possible from street intersections and be coordinated with existing or planned median openings and driveways on the opposite side of the roadway.

6. Parking.

Parking for multi-family developments shall be provided on-site in on-grade or underground structures, parking courts, carports, or attached garages. Adverse visual impacts from parking lots, carports, structures, and garage doors on the residential character of the street or project site shall be minimized through proper siting and design.

7. Pedestrian circulation.

- a) Pedestrian circulation shall be adequately separated from vehicular traffic. Pedestrian entrances and walkways should be clearly identified and easily accessible to minimize pedestrian/vehicle conflict.
- b) Pedestrian walkways should link dwelling units with common open space and recreation areas, parking areas, and the street at the project entries. Where possible, multi-family developments shall incorporate pedestrian connections to adjoining residential and commercial projects and other compatible land use facilities.
- c) Pedestrian walkways shall be safe, visually attractive, and well defined by landscaping and lights.

8. Exterior lighting.

- a) Every multi-family project should have adequate lighting to provide for security and visibility, particularly along walkways and driveways, entrances to parking areas, and open space areas.
- b) Site lighting shall not impact surrounding or neighboring properties. The type and location of site and building lighting shall preclude direct glare onto adjoining property, streets, or skyward. All lighting fixtures must be shielded to confine light spread on-site.
- c) The design of all exterior light fixtures should be compatible with the building's architecture.

3.16 Design Guidelines for Single Family Residential Uses

The purpose of this section is to facilitate high quality single family residential development within the Willowbrook Residential 1 and Willowbrook Residential 2 Zones. These design guidelines address site planning and building design and are in addition to the above land use regulations and development standards for the Willowbrook Residential 1 and Willowbrook Residential 2 Zones.

1. **Yard areas.** While meeting the minimum front and side yard depths established in Chapter 22.18, a new dwelling's front and side yard depths should approximate that of adjacent residences. The front and side yard pattern on the block shall be identified and respected to help unify the neighborhood.

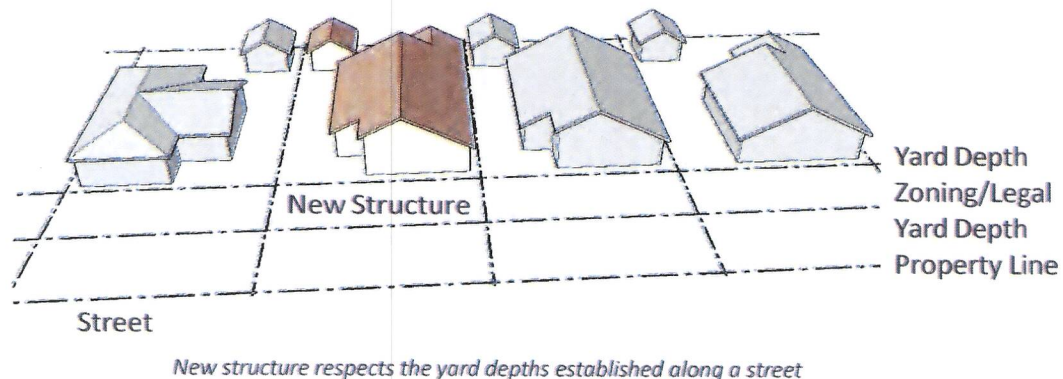


Figure 3-9: Yard Depths

2. **Scale and mass.** The scale and mass of a new dwelling should be similar to that of neighboring houses and not overwhelm them with disproportionate size or a design that is out of character.
3. **Height and rooflines.** Height and rooflines should be consistent with the style of architecture of the house, and complement qualities of neighboring residential structures such as type, slope, size, material, and color.
4. **Additions and accessory buildings.** Additions and accessory structures should maintain the look and appearance of the existing primary structure so that they do not appear as an addition or new building. They should respect the architectural style, scale, rhythm, and building elements of the existing primary structure. An addition should complement and balance the overall form, mass, and composition of the existing primary structure on the property. An addition shall maintain the same floor-to-floor height of the original structure.

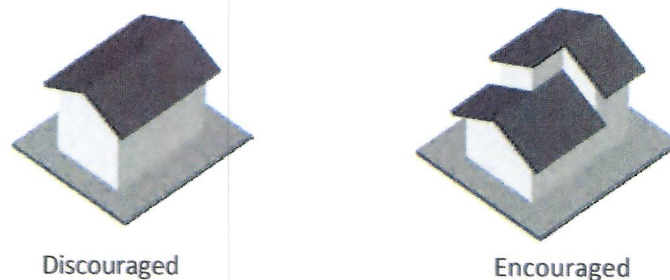
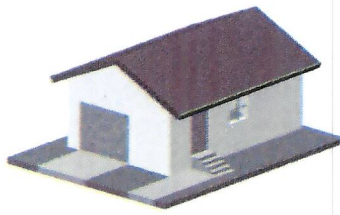
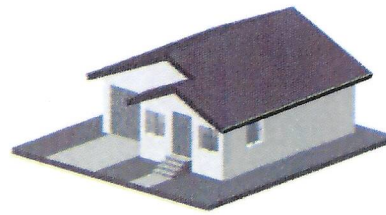


Figure 3-10: Massing

5. **Orientation.** Dwellings shall orient themselves to the street with a clearly identifiable front door and windows that face the street. Front windows and the front door contribute to “eyes on the street” which helps neighborhood security.



Discouraged



Encouraged

Orient the front of the house and entry to face the street

Figure 3-11: Orientation

6. **Privacy for residences.** New houses and/or accessory buildings should minimize their visual impact on adjacent properties. New windows and second floor balconies should be placed where they promote maximum privacy between properties. Windows should be offset or staggered from neighboring windows to maintain privacy between houses. Avoid locating balconies to directly overlook a neighboring patio or yard.
7. **Exterior lighting.** Exterior lighting includes all lighting fixtures on front façades, security lighting, and landscape lighting. Adequate exterior lighting shall be provided on the front of the house to ensure neighborhood safety and security. Recessed porches shall be lit. Exterior lighting shall be positioned so that no direct light extends into neighboring properties or public right-of-ways.

3.17 Sustainable Design Guidelines

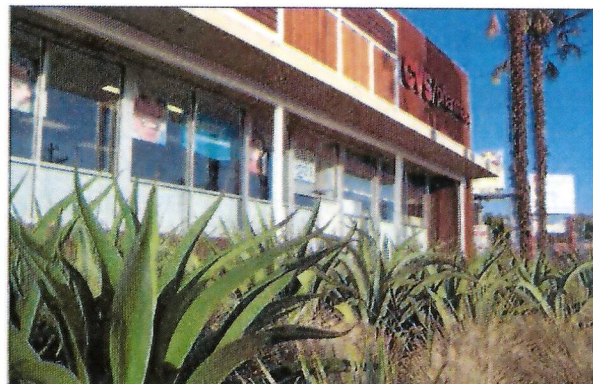
Building green means reducing the use of resources, minimizing harmful impacts to the environment, and creating healthier environments for people. Green buildings can incorporate both passive, low-tech design, such as daylighting; and active, high-tech strategies and systems, such as photovoltaic panels. To be most effective, green building strategies should be incorporated into all phases of a project from early programming and budgeting, to design and construction, to commissioning, operations, and maintenance and post-use demolition/recycling.

Site Design and Passive Solar Design

- Buildings should be sited and designed to maximize the use of sunlight and shade for energy savings, and respect the solar access of adjacent buildings.
- Buildings should be clustered for shade, and incorporate protective courtyards, recessed windows and doors, and insulated walls.
- To reduce energy use, the east and west walls of buildings should be shaded with evergreen trees to reduce summer heat gain. South walls should be shaded with deciduous trees.
- Walkways and plazas should be designed to collect stormwater, where feasible.

Water Efficiency

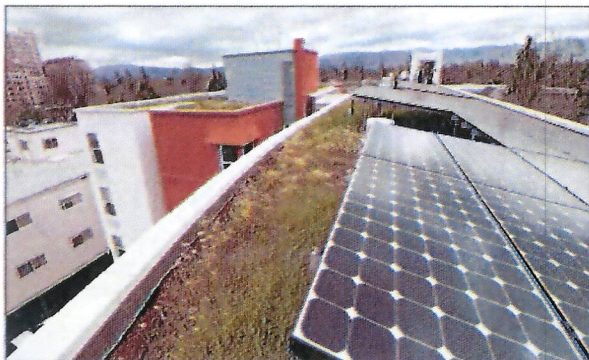
- To reduce water use and maintenance costs, the majority of plant materials should be drought tolerant and require relatively low maintenance.



Landscape materials should be drought-tolerant and low maintenance. Pervious, non-vegetative ground cover interspersed with native plantings can provide a sustainable, attractive landscaped area.

Building Design

- The provision of a green roof should strongly be considered to reduce solar gain and to reduce the quantity of water entering the storm drain system.
- Solar panels on roofs should be considered to capture solar energy for internal use of the project.
- Arcades, covered walkways, trellises and passages should be incorporated to provide sheltered areas for pedestrian circulation as well as to shade the buildings to reduce energy usage.



Green roofs serve several purposes for a building, such as absorbing rainwater, providing insulation, and helping to lower urban air temperatures and mitigate the heat island effect. Parking structures and other buildings offer potential for solar roofs, which capture sunlight and transform it into energy.

3.18 Street and Sidewalk Standards and Guidelines

The Plan Area has a mix of commercial, institutional and residential uses. While both paved sidewalks and sidewalks with parkways can be found within the area, this Specific Plan differentiates the sidewalk treatment between residential and non-residential frontages as follows:

- All existing parkways facing residential uses shall be retained.
- All other street segments (with institutional, commercial or mixed use zone frontages) shall have paved sidewalks instead of parkways.

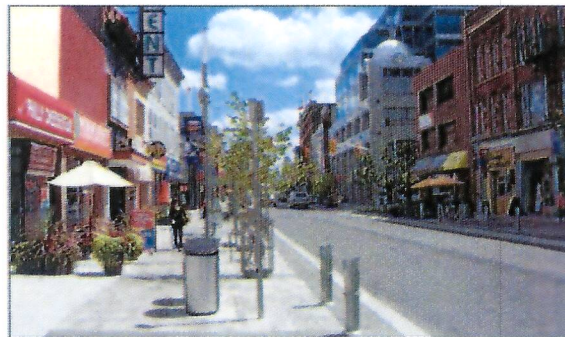


Existing parkways along residential frontages include turf and drought tolerant landscaping.



Existing sidewalks along non-residential parkways have a mix of paving materials.

Each property owner is responsible for the maintenance of the parkway that fronts their lot. Drought-tolerant landscaping is encouraged in the residential parkways.



Sidewalks along non-residential frontages should have attractive paving materials and patterns. Tree wells can also become a part of a Low Impact Development strategy.

As new development occurs in the Mixed Use Zones, the adjoining property owner/developer shall follow the following standards and guidelines for upgrading roadways and sidewalks. Property owners in other areas are also encouraged to follow these standards and guidelines.

- New or reconstructed roadways and sidewalks shall be designed in coordination with the County Department of Public Works (DPW).
- Green Streets and Law Impact Development (LID) strategies, such as the use of vegetated swales and decomposed granite, shall be utilized to manage stormwater, improve water quality, reduce flows and enhance watershed health.
- New sidewalks should be graded to allow stormwater run-off into adjacent unpaved areas, such as planter strips and parkways.
- Sidewalks should be designed or modified such that street furniture, signage, lighting, landscaping, etc. do not interfere with a continuous pedestrian path. The pedestrian path should be clearly identified in project plans.
- Street treatments in which stormwater runoff is captured and retained shall be located within street medians and parkways. Stormwater Best Management Practices per the California Stormwater Quality Association's Stormwater Best Management Practice Handbook shall be applied.
- Decorative non-vegetative groundcover, such as river cobble, pebbles, and crushed stone, should be used in streetscape composition. Non-vegetative groundcovers shall allow proper drainage. Any material, such as crushed tires, that may leach harmful chemicals into the soil or generate heat is not permitted.
- Sidewalks shall comply with all Americans with Disabilities Act (ADA) standards.



Drought-tolerant landscaping in the parkways is an attractive replacement for turf.

4.1 Introduction

This Chapter describes the Mobility Strategy for the Willowbrook TOD Specific Plan, which includes recommendations for the roadway network as well for improving pedestrian, bicycle, and transit circulation.

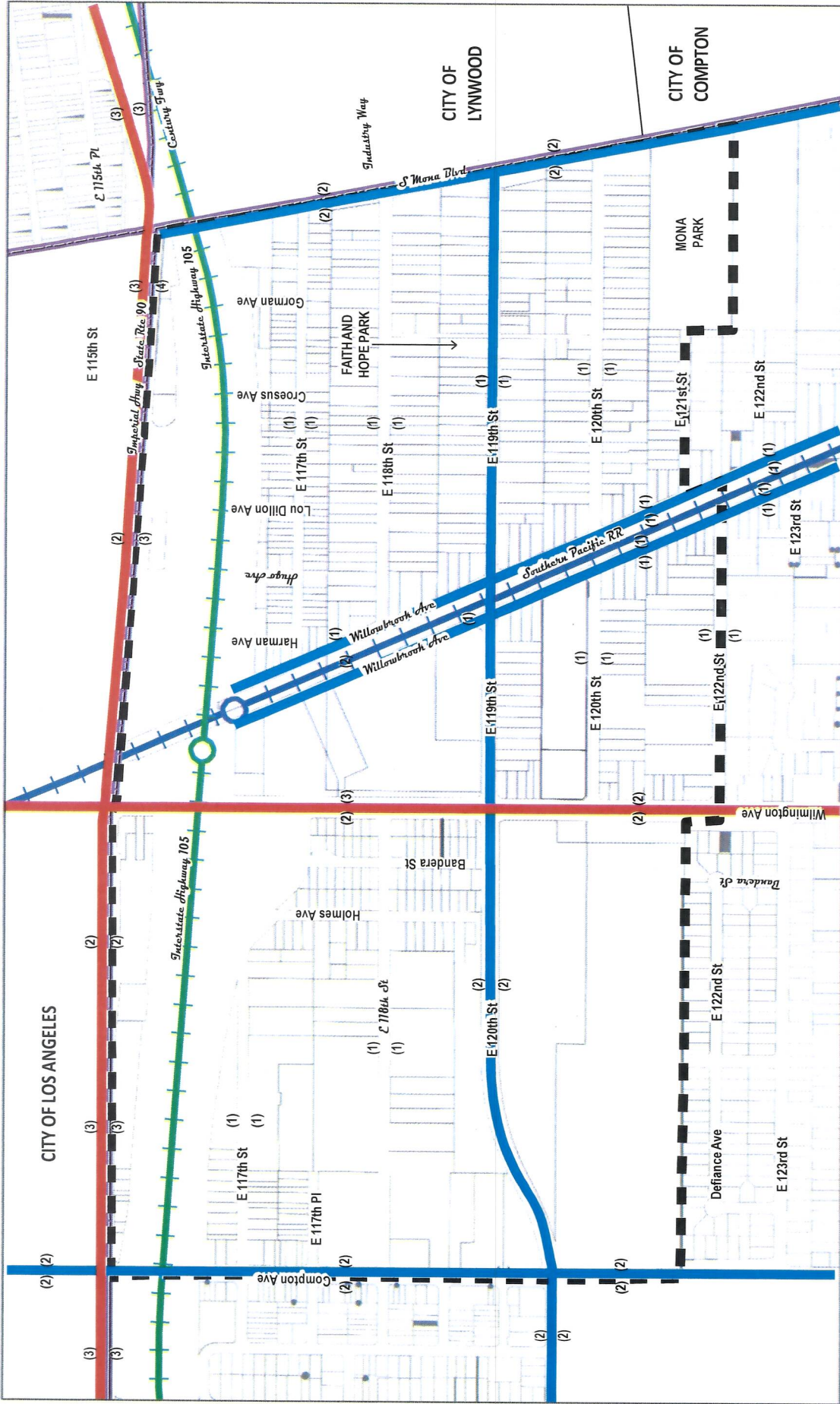
The overall goal for mobility in the Plan Area is to enhance connectivity and the ease of movements for non-auto transportation modes, particularly pedestrians and bicyclists. These enhancements will support healthy transportation options.

4.2 Background

The Plan Area is centered on the Willowbrook/Rosa Parks Station with stops for both the Metro Blue Line and the Metro Green Line.

Generally, the existing circulation system in the area is automobile-oriented and auto-dominated. The rail station is generally difficult to access, and the station area is inhospitable to transit users. There are however numerous shuttles and bus routes serving the Plan Area and connecting to the rail station.

The local community has expressed concerns over the high speed of vehicle traffic on key roadways and the poor pedestrian environment at most intersections and on many of the sidewalks. There are few locations to conveniently cross streets, intersections without a clear roadway and pedestrian markings, and a poor pedestrian sidewalk environment – particularly on the east side of Wilmington Avenue by the Willowbrook/Rosa Parks Station. There is a lack of pedestrian infrastructure in some areas, with unpaved paths and poor lighting. Furthermore, there is a lack of bicycle infrastructure on streets in the Plan Area, and inadequate bicycle parking.



**Figure 4-1
Street Network**

- County of Los Angeles
- Willowbrook TOD Specific Plan Area
- Metro Green Line
- Metro Blue Line
- Major Roadway
- Secondary Roadway
- (x) Number of Lanes

WILLOWBROOK TOD SPECIFIC PLAN



4.3 Roadway Network

The roadway system, shown on Figure 4-1, provides the backbone circulation system for all modes of transportation. While historically street systems have been oriented to serving the automobile, the roadway system should provide a network of complete streets that provides for the safe and efficient circulation of transit, bicycles, and pedestrian, as well as automobiles.

The existing street system will be largely maintained in its current configuration, with some changes/improvements designed to improve access and circulation, and walkability. The Major Roadways (as identified in the General Plan) will continue to be Wilmington Avenue and Imperial Highway. Secondary Roadways (as identified in the General Plan) will continue to be Compton Avenue, Willowbrook Avenue and Mona Boulevard in the north-south direction, and 120th Street/119th Street in the east-west direction. The number of traffic lanes and roadway lane configurations will generally remain the same, except where otherwise noted in the Specific Plan (for example where road diets will be implemented).

The following street enhancements, shown in Figure 4-2, are intended to improve circulation for bicycles and pedestrians in the Plan Area. These are also described in more detail in Sections 4.4 and 4.5.

Road Diet and Bicycle Lanes on 120th Street

The section of 120th Street between Compton Avenue and Wilmington Avenue will be reduced from four lanes to three lanes, with a bicycle lane in each direction and no on-street parking. This is part of the Willowbrook Area Access Improvement Project.

Willowbrook Avenue West

The section of Willowbrook Avenue West between the Willowbrook/Rosa Parks Station and 119th Street, will be reduced from two lanes southbound to one lane southbound, and a bicycle path installed on the west side of the street. This lane will be a dedicated busway.

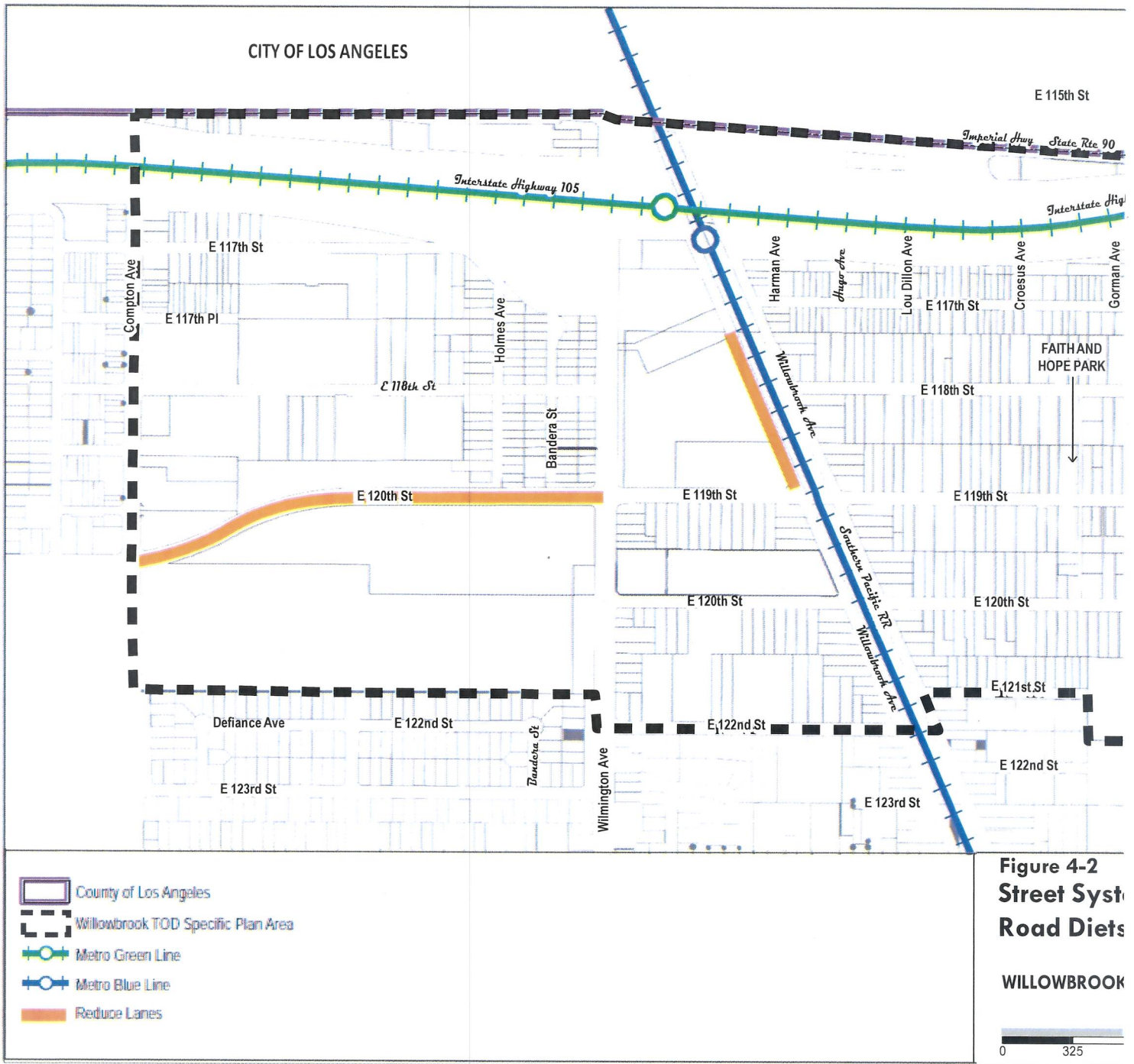
Road Diet and Bicycle/Pedestrian Trail on Mona Boulevard

Mona Boulevard from the I-105 Freeway to 124th Street will be converted from a four lane street to a three lane street, and a pedestrian/bicycle trail installed on the west side of the street. Parking will be retained on the west side of the street. The pedestrian/bicycle trail shall include a paved section to meet Class I design guidelines. The design of the trail will be subject to the requirements of DPW. Figure 4-3 illustrates a typical cross section of reconfigured Mona Boulevard.

The majority of properties on the west side of Mona Boulevard are accessed from the east-west streets. However, because there are some driveways and curb cuts on Mona Boulevard (approximately twelve along the 3,000 foot section), the pedestrian/bicycle trail must be designed to safely accommodate those driveways, with adequate signage and surface material details. (It is noted that typical street conditions often require driveway access across sidewalks and across bicycle lanes).

Rename 120th Street between Compton and Wilmington Avenues

Currently, the segment of 120th Street west of Wilmington Avenue (between Compton and Wilmington Avenues) aligns with 119th Street east of Wilmington Avenue, which causes confusion. This segment of 120th Street, which fronts the MLK Medical Center, should be renamed. A community contest should be organized to select an appropriate street name, similar to that for "Healthy Way" on the Medical Campus.



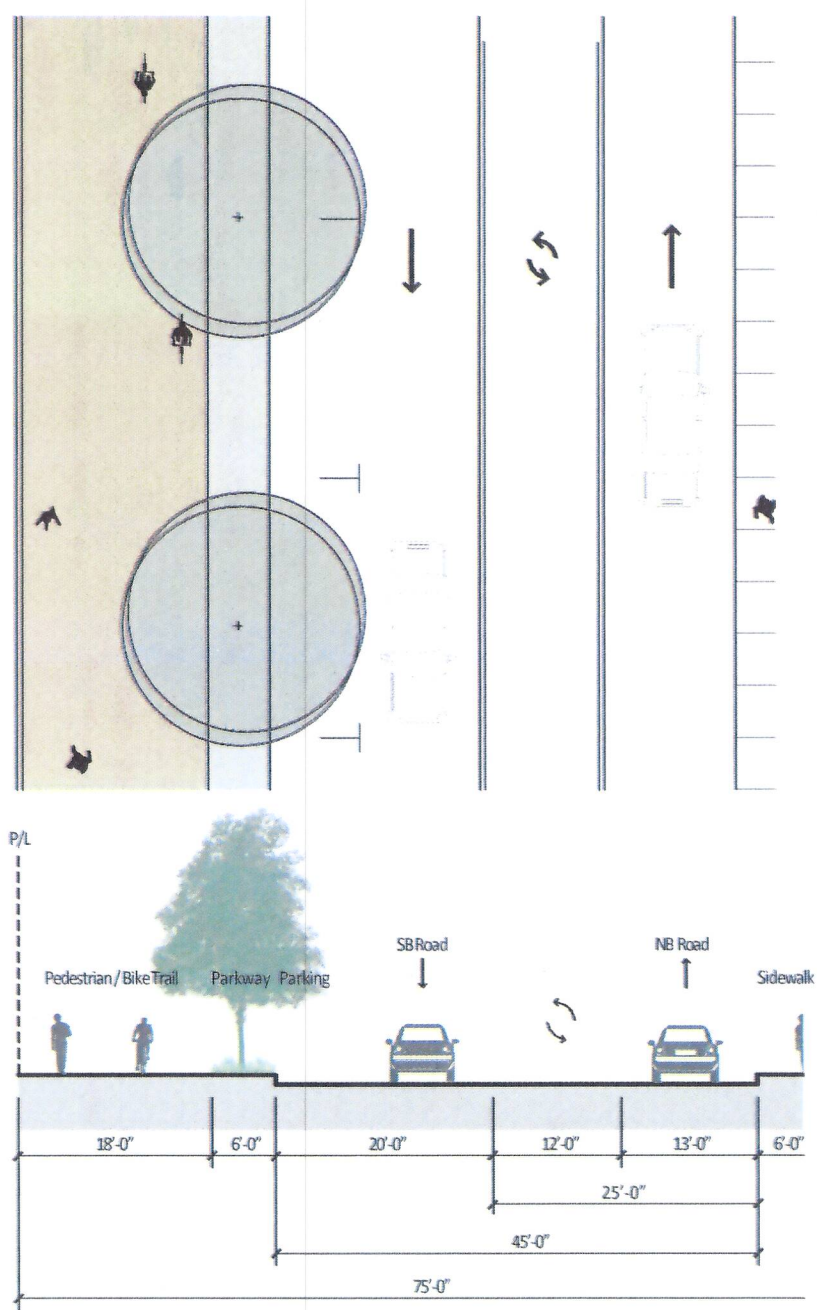
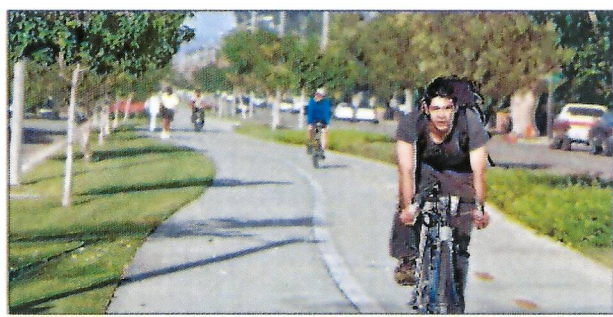
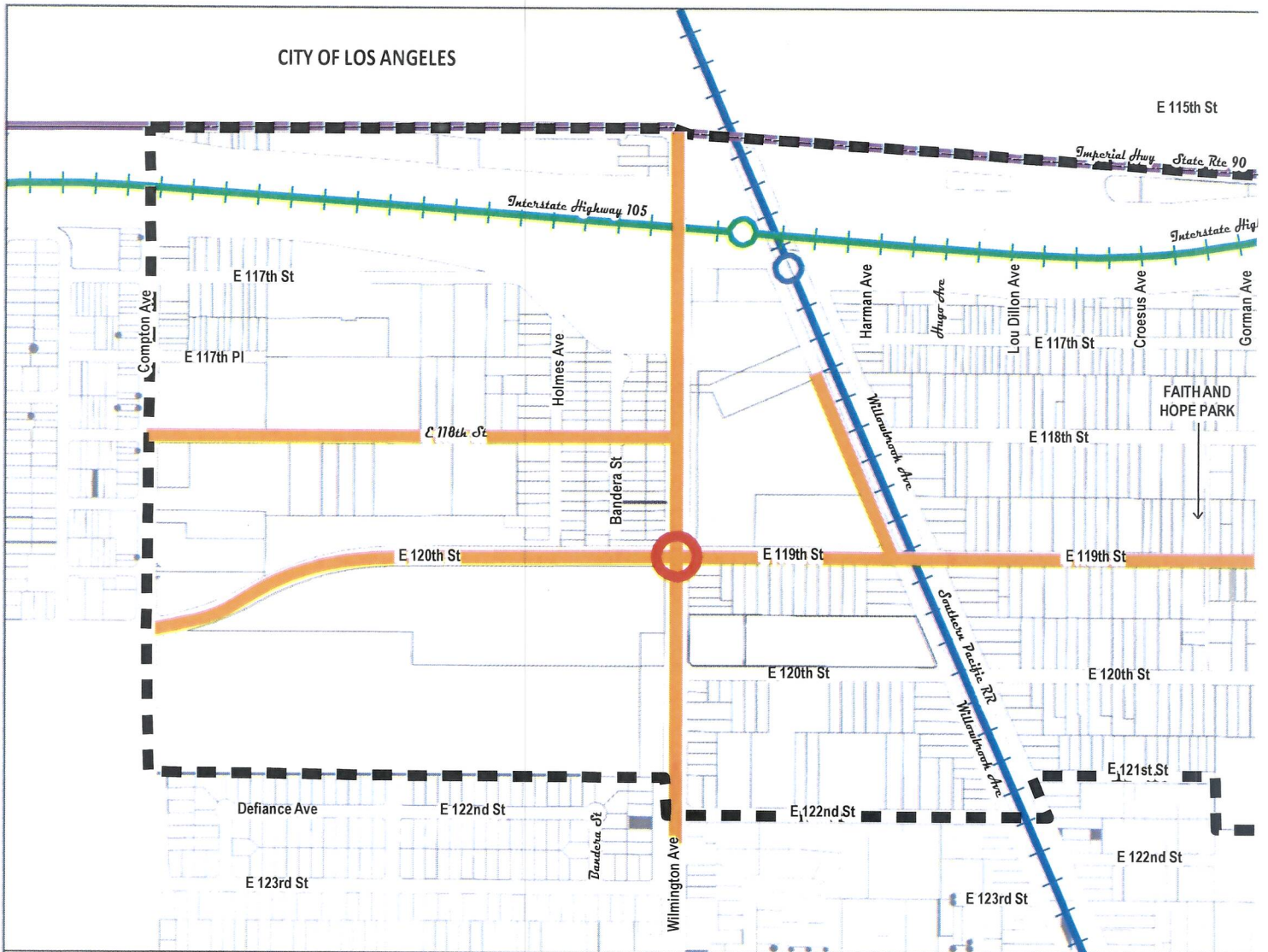


Figure 4-3: Proposed Mona Boulevard Section



Examples of a Class I bicycle path and pedestrian trail

CITY OF LOS ANGELES



-  County of Los Angeles
-  Willowbrook TOD Specific Plan Area
-  Metro Green Line
-  Metro Blue Line
-  Key Pedestrian Routes
-  Pedestrian Hub

Figure 4-4
Key Pedes

WILLOWBROOK



4.4 Pedestrian Circulation

The key pedestrian routes in the Plan Area are shown in Figure 4-4. The backbone of the pedestrian system is formed by Wilmington Avenue in the north-south direction and 120th/119th Street in the east-west direction. These corridors connect activity centers of the Willowbrook/Rosa Parks Station, the Kenneth Hahn Plaza, and the MLK Medical Center Campus. They also cross at the intersection of Wilmington Avenue and 120th/119th Street – which is the functional pedestrian hub of the Plan Area. Section 5.2C in Chapter 5 discusses and illustrates concepts for special treatment for this intersection.

Additional key elements of the pedestrian system are 118th Street between Compton Avenue and Wilmington Avenue – which connects the CDU campus to the rest of the Plan Area, Willowbrook Avenue West between 119th Street and the Willowbrook/Rosa Parks Station – providing access from residential areas to the Station, and 119th Street between Willowbrook Avenue and Mona Boulevard provides access from the residential areas to the activity centers within the Plan Area. Mona Boulevard also provides north-south pedestrian access on the east side of the Plan Area including access to Mona Park, MLK Elementary School, and Dr. Ralph Bunche Middle School.

Pedestrian Sidewalk Improvements

Sidewalks currently exist on most streets in the Plan Area, although some are narrow or substandard in quality. Sidewalks will be improved as new development occurs, and through the landscaping provisions contained in this Willowbrook TOD Specific Plan.

Specific recommendations include:

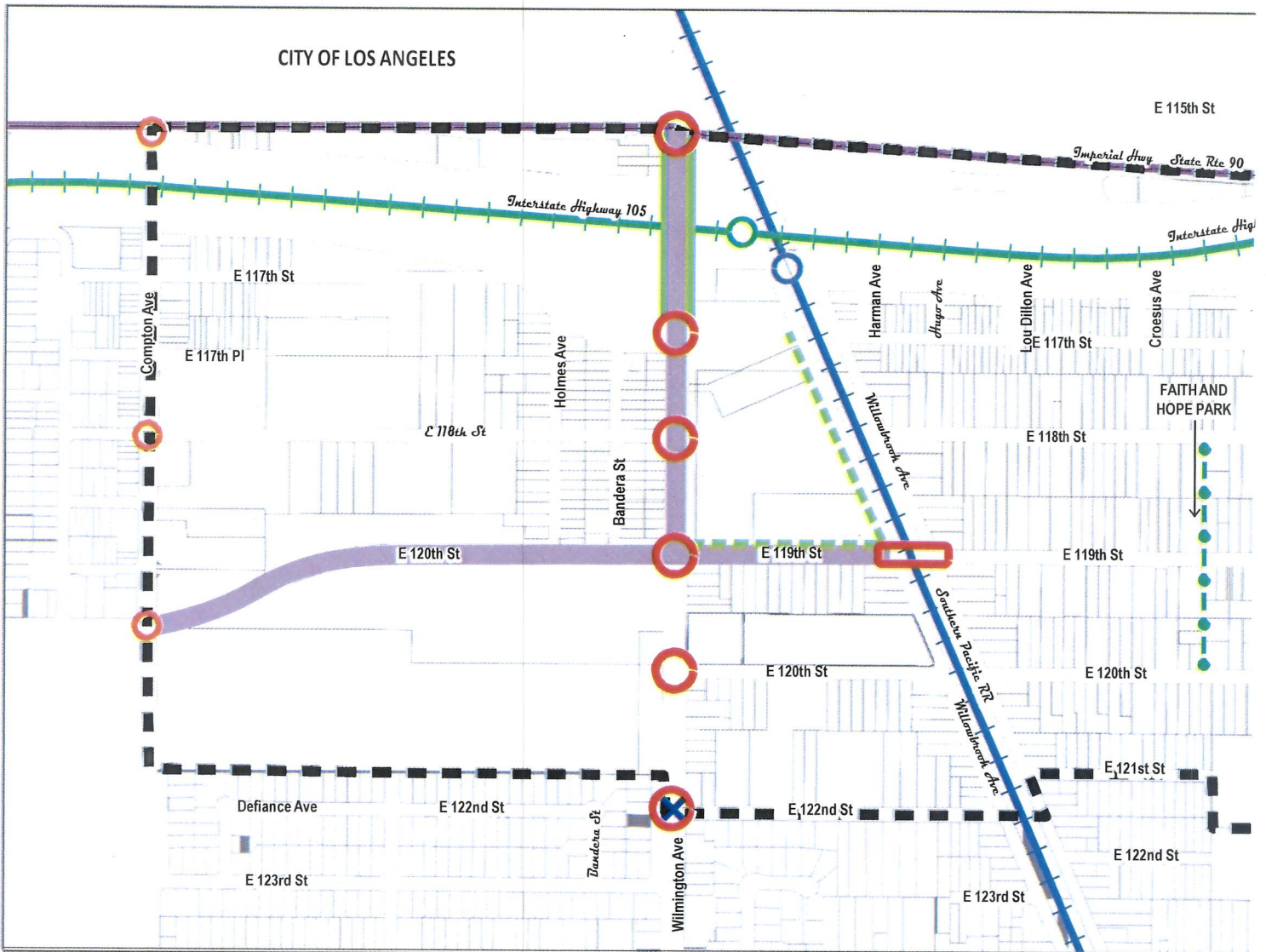
- A sidewalk will be added to the currently unpaved west side of Willowbrook Avenue West between the Willowbrook/Rosa Parks Station and 119th Street.
- The sidewalks on Wilmington Avenue between the I-105 Freeway Off-Ramps and Imperial Highway will be improved through widening and streetscape improvements, including improved pedestrian-scaled street lighting.
- Streetscape standards will provide for wider sidewalks where feasible as redevelopment occurs, for example the north side of 119th Street between Wilmington Avenue and Willowbrook Avenue.

Pedestrian Oriented Intersection Improvements

To enhance the pedestrian environment and to calm traffic, a number of pedestrian oriented intersection improvements will be implemented throughout the Plan Area. These will be based on a menu of improvements that includes the following:

- Adding high visibility crosswalks at both existing and new intersections recommended in this Specific Plan.¹
- Adding passive pedestrian detection and pedestrian push buttons for crosswalks at traffic signals at intersections.
- Adding pedestrian countdown pedestrian signals and audio signals to crosswalks at intersections.
- Adding advance stop lines to signalized intersection approaches.
- Adding sidewalk bulb-outs and extensions, or reducing curb returns, on intersection corners where feasible.

¹ Design of any high visibility crosswalks should be reviewed on a case-by-case basis, and consider ADA access, maintenance cost, and feasibility. Pavers or textured surfaces should not be considered in the pedestrian walking path of the crosswalks.



**Figure 4-5
Pedestrian**

WILLOWBROOK



For all bulb-out locations, where two curb ramps are proposed per corner, curb ramps shall be situated close enough to the intersection so that pedestrian visibility is not hindered. Any proposed landscaping shall not interfere with sight distance of pedestrians waiting to cross. The intersection geometrics shall be evaluated to determine if bulb-outs can fit within the proposed curb to curb width.

These measures will facilitate pedestrian circulation, by reducing the width of roadway for pedestrians to cross, providing additional sidewalk space, and making pedestrian crossings more visible to pedestrians and motorists. A typical diagram is illustrated in Figure 4-6.

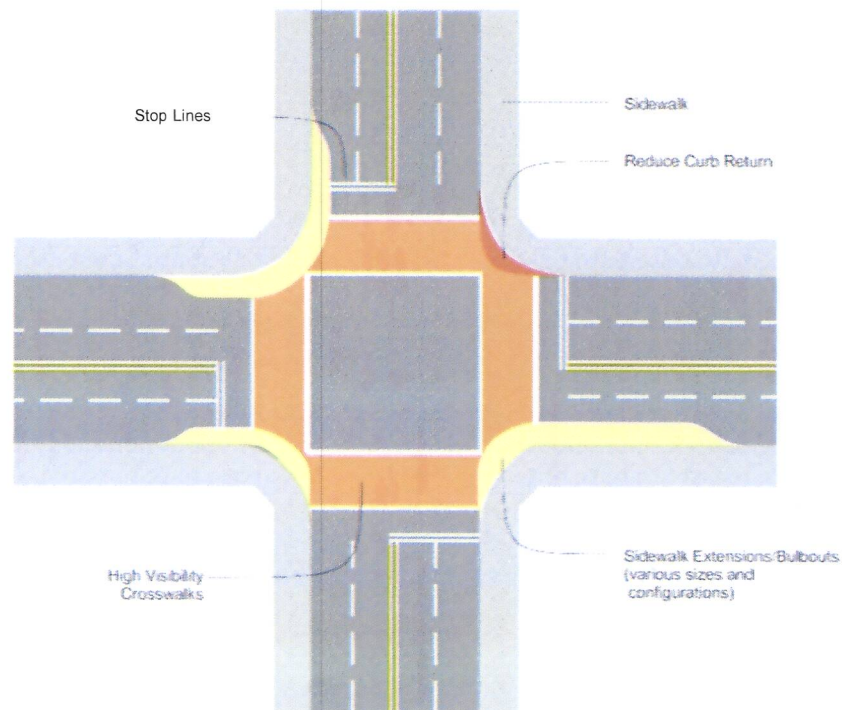
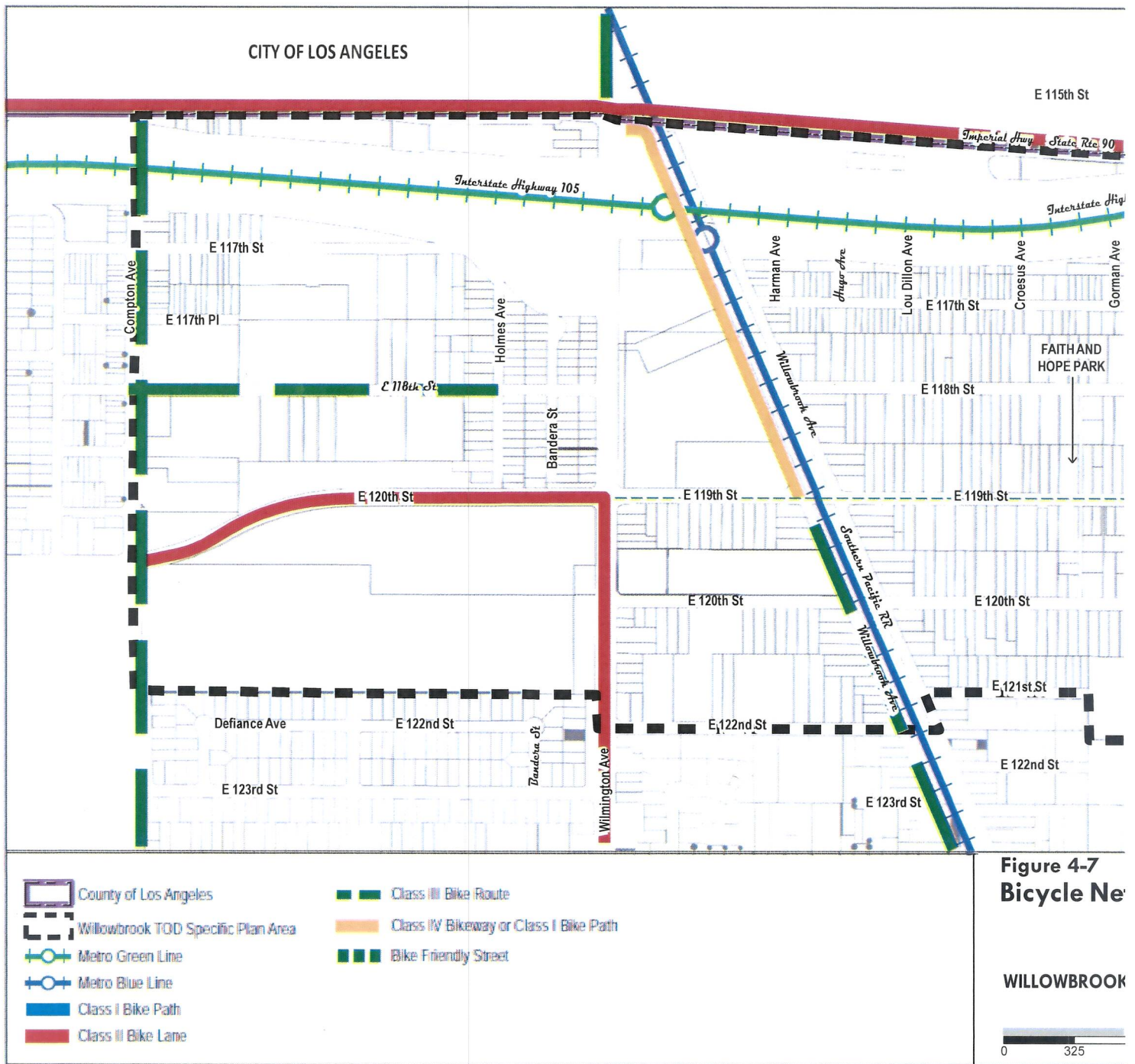


Figure 4-6: Pedestrian-oriented intersection improvements

The locations for such improvements are shown in Figure 4-5, and listed below.

- Wilmington Avenue and Imperial Highway
- Wilmington Avenue and I-105 Eastbound Ramp
- Wilmington Avenue and 118th Street
- Wilmington Avenue and 120th/119th Street
- Wilmington Avenue and 120th Street
- Wilmington Avenue and 122nd Street
- Willowbrook Avenue West and 119th Street
- Willowbrook Avenue East and 119th Street
- Mona Boulevard and Imperial Highway
- Mona Boulevard and 119th Street
- Mona Boulevard and 120th Street
- Compton Avenue and Imperial Highway
- Compton Avenue and 118th Street
- Compton Avenue and 120th Street



The type of improvements will follow the concepts identified in the “Los Angeles County Transit Oriented Districts Access Study, SCAG and County of Los Angeles.” The specific improvements to be implemented at each location will be determined following detailed design studies to determine applicability and feasibility and the ultimate configuration. However, curb extensions should not restrict the circulation of buses, trucks, emergency vehicles, and bicycles.

The improvements at Wilmington Avenue and I-105 eastbound ramp will add a crosswalk across Wilmington Avenue to facilitate access to the Willowbrook/Rosa Parks Station. If found to be warranted by DPW analyses, other specific improvements include:

- New traffic signals at Wilmington Avenue and 122nd Street, and at Mona Boulevard and 119th Street, to facilitate pedestrian crossings on long stretches of both streets currently without signalized crosswalks.
- A signalized pedestrian crosswalk on Mona Boulevard and 120th Street, to facilitate pedestrians crossing to Dr. Ralph Bunche Middle School.

4.5 Bicycle Circulation

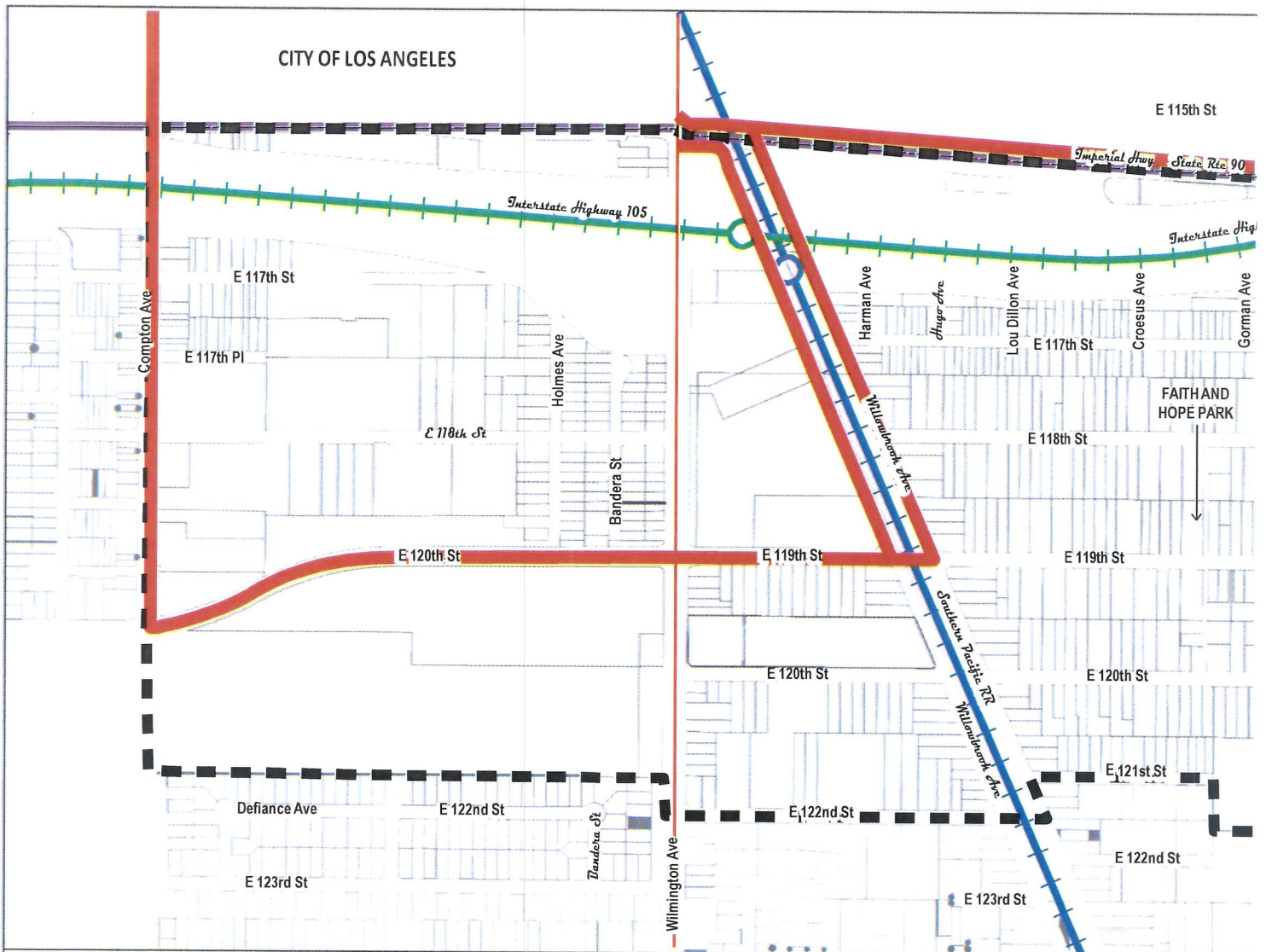
The Specific Plan Bicycle Network, shown in Figure 4-7, includes a combination of Class I, Class II, Class III and Class IV bicycle facilities that connect activity centers and neighborhoods to the Willowbrook/Rosa Parks Station, connect to adjacent communities, and provide a dedicated network for bicyclists to use safely and efficiently. The Bicycle Circulation System builds on the County’s Bicycle Plan. Additional recommendations are provided to complete and connect the bicycle network, as described below.

Bicycle Facilities

- A Class I Bicycle Path consists of an exclusive bicycle facility, separated from roadways and traffic.
- A Class II Bicycle Lane consists of a designated striped lane along the curb lane of a street. It provides for one-way travel and is generally delineated with special striping and signage.
- A Class III Bicycle Route is for shared use of bicycles with automobile traffic. Signs are posted, which indicate the road also serves as a bicycle route, but no special lane for bicycles is striped. There may, however, be painted bicycle symbols on the roadways surface (known as sharrows) to indicate to motorists that bicycles also use the street.
- A Class IV Bikeway, or cycle track, provides a right-of-way designated exclusively for bicycle travel within or adjacent to a roadway and which is protected from vehicular traffic. Types of separation include, but are not limited to, grade separation, flexible posts, inflexible physical barriers, or on-street parking.

Specific recommendations include:

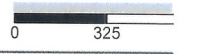
- Class I Bicycle Paths will be implemented on Willowbrook Avenue West between 119th Street and Imperial Highway — to provide access to the Willowbrook/Rosa Parks Station, and on Mona Boulevard (west side) between Imperial Highway and 124th Street. The facility on Willowbrook Avenue West could also be a Class IV cycle track.
- Class II Bicycle Lanes will be implemented on 120th Street between Compton Avenue and Wilmington Avenue, on Wilmington Avenue between 124th Street and 120th Street, and on Imperial Highway between Compton Avenue and Mona Boulevard.
- Not all streets can support bicycle lanes due to insufficient width or the presence of on-street parking, which is also an important asset to the function and economic well-being of the



-  County of Los Angeles
-  Willowbrook TOD Specific Plan Area
-  Metro Green Line
-  Metro Blue Line
-  Transit Streets (Multiple Routes)

Figure 4-8
Key Transi

WILLOWBROOK



adjacent commercial uses or neighborhoods. Where there is insufficient roadway width to stripe bicycle lanes and/or to retain on-street parking, a connected network is achieved through the designation of Class III Bicycle Routes. Class III Bicycle Routes will be implemented on Compton Avenue, Willowbrook Avenue West south of 119th Street, 119th Street between Wilmington Avenue and Mona Boulevard, and on 124th Street throughout the Plan Area.

Bicycle Amenities

Bicycle parking can include bicycle racks and lockers, bicycle corrals (larger areas dedicated to bicycle parking), and at bicycle stations. Bicycle stations can include repair facilities and a small bicycle shop, as well as secure bicycle parking. Bike share programs can provide bicycles for sharing by the community where people can rent a bicycle for a period of time and pick it up or drop it off at any bicycle share stop in an area.

Bicycle Parking and Stations

Bicycle parking should be provided according to Title 22 for all new developments. Additional bicycle parking should be provided at key public locations to also serve existing uses. This includes the Kenneth Hahn Plaza and the Willowbrook/Rosa Parks Station. Metro is planning to provide a bicycle station at the rail station called a Metro Bike Hub. Bicycle stations should also be provided on the MLK Medical Center Campus and the CDU Campus, and in the Northwest Subarea where offices are located. These additional bicycle stations should be provided by those facilities and uses.

Bike Share Program

A bike share program should be implemented in the Plan Area to encourage the use of bicycling – which would reduce vehicle trips and provide a healthy alternative method of transportation. It would also improve the first and last mile connections to the Willowbrook/Rosa Parks Station. While such a program could primarily be internal to the Plan Area, it should also be coordinated with any other existing or planned bike share programs in adjacent areas or the County.

4.6 Transit Circulation

Station Access

Metro is currently working on improvement plans for the Willowbrook/Rosa Parks Station. These are expected to include enhancements to the station facilities and platforms, the provision of a shuttle stop and bicycle station, and improvements to station access, including a crosswalk across the tracks south of the station to Willowbrook Avenue East.

Transit Service

Key transit streets (with multiple bus routes) in the Plan Area are shown in Figure 4-8. It is anticipated that current bus routes will continue to serve the Plan Area focusing on the Willowbrook/Rosa Parks Station. To encourage transit use, bus stops should be encouraged at quarter mile spacing to reduce walk time access to bus routes, and the provision of bus stop amenities should also be encouraged (such as benches, shade, landscaping, and transit service information).

Shuttle Routes

Existing shuttle routes that are operated by the County and CDU should be continued to facilitate alternative modes of transportation, and provide critical access for those without a car. Shuttle providers should consider extending service to accommodate second-shift hospital workers.

Additional shuttle routes should be added to serve new development in the Northwest Subarea and connect the land uses to the Willowbrook/Rosa Parks Station. These new shuttle services could be provided by the private sector as part of a comprehensive Transportation Demand Management Program (see Section 4.7 below).

The potential combination of the various shuttle routes could be considered, which improves the overall effectiveness of service and avoids duplication between routes, as long as the needs of individual land uses and institutions continue to be met.

4.7 Transportation Demand Management

A Transportation Demand Management Program will be developed by any development within the TOD to implement strategies, take advantage of the high level of transit service, and to reduce both vehicle trips and the number of parking spaces provided. Such programs include the encouragement of use of transit, bicycling, walking, and ride-sharing. These types of programs are generally most suitable and most effective for large employers, institutional uses, and office uses and could be attractive to employers in new office type land uses in the Northwest Subarea as they could reduce the capital costs needs of building parking. The Program should include the Northwest Subarea, CDU, and the MLK Medical Center.

Transportation demand management and trip reduction strategies could include, but are not be limited to:

- Encouraging use of transit, including subsidizing transit passes.
- Parking cash out programs.
- Encouraging ride-share.
- Providing preferential parking for carpools.
- Facilitating formation of carpools and vanpools.
- Site and building design to facilitate use of transit, bicycling, and walking.

A Transportation Management Organization (TMO) could also be established to facilitate these programs at an area-wide level and support individual employers and/or buildings in participating to the fullest extent possible.

Streetscape Improvements

5.1 Introduction

The purpose of this chapter is to describe recommended streetscape improvements in the Plan Area that serve to fulfill the Specific Plan goal of improving the quality of life for residents with improvements to the public realm. Some of these recommendations are conceptual and the County will need to further assess and supplement these recommendations, as noted, on a case-by-case basis.

Streetscape improvements are intended to enhance and unify the visual and spatial experience of the driver, pedestrian, and the bicyclist, and help provide key linkages between the activity centers and neighborhoods in the Plan Area. The streetscape gives the user a sense of direction and place. These enhancements also enhance connectivity and the ease of movement for pedestrians and bicyclists, supporting healthy transportation options.

5.2 Streetscape Improvements

Recommended streetscape improvements consist of an interrelated palette of street trees, street furniture, street lighting, signage and landscaping. Public art can also have a role in the streetscape palette. These elements are illustrated in Figure 5-1 and described in more detail in the following sections:

A. Willowbrook Area Access Improvements Project

DPW has streetscape improvements planned for Wilmington Avenue (Imperial to East 120th Street) and East 120th/East 119th Street (Compton to Willowbrook) as a part of the Willowbrook Area Access Improvements (WAAI) Project. The streetscape design includes new sidewalks, Class II bicycle lanes, solar pedestrian lights, street trees, street furnishings, bus shelters, and Low Impact Development features such as storm capture underground storage facilities and permeable pavers. The WAAI Project also includes signage and wayfinding services with the theme of “willow and brook.” The Specific Plan builds on this project by continuing the design themes of the WAAI Project to the rest of the Plan Area.

B. Green Alley Project

DPW is installing a green alley between East 118th Street and East 120th Street. The project consists of colored concrete pavement; curb and driveway reconstruction at the alley entrance; crosswalk enhancements; pedestrian lighting; monument sign; and decorative walls that hold potted plants.



**Figure 5-1
Streetscape Improvements**

- County of Los Angeles
- Willowbrook TOD Specific Plan Area
- Metro Green Line
- Metro Blue Line
- Pedestrian Connections
- New Street Lighting & Street Trees
- DFW Roadway Improvements Project
 - Roadway pavement improvements
 - Trees
 - Area signage
 - Landscaping
- Extend DFW Roadway Improvements to Specific Plan boundary
- Landscape Screening
- Metro Rosa Park Station Improvements
- Major Intersection Improvements
- Minor Intersection Improvements
- Major Pedestrian Hub
- Park/Open Space/Plaza

WILLOWBROOK TOD SPECIFIC PLAN



C. Streetscape Improvements at the Intersection of Wilmington Avenue and East 120th/East 119th Street

The two major pedestrian streets within the Plan Area are Wilmington Avenue in the north-south direction and East 120th/East 119th Street in the east-west direction. These two streets connect the major activity areas as well as connect across the rail tracks with the residential neighborhoods to the east. They also intersect to form the pedestrian core of the Plan Area. This intersection is the center of the Plan Area and should have special treatments to mark it as such. Building on the improvements planned per the WAAI Project, the Specific Plan proposes additional improvements at this intersection as illustrated in Figure 5-2. These include a bulb-out on the southeast corner of the

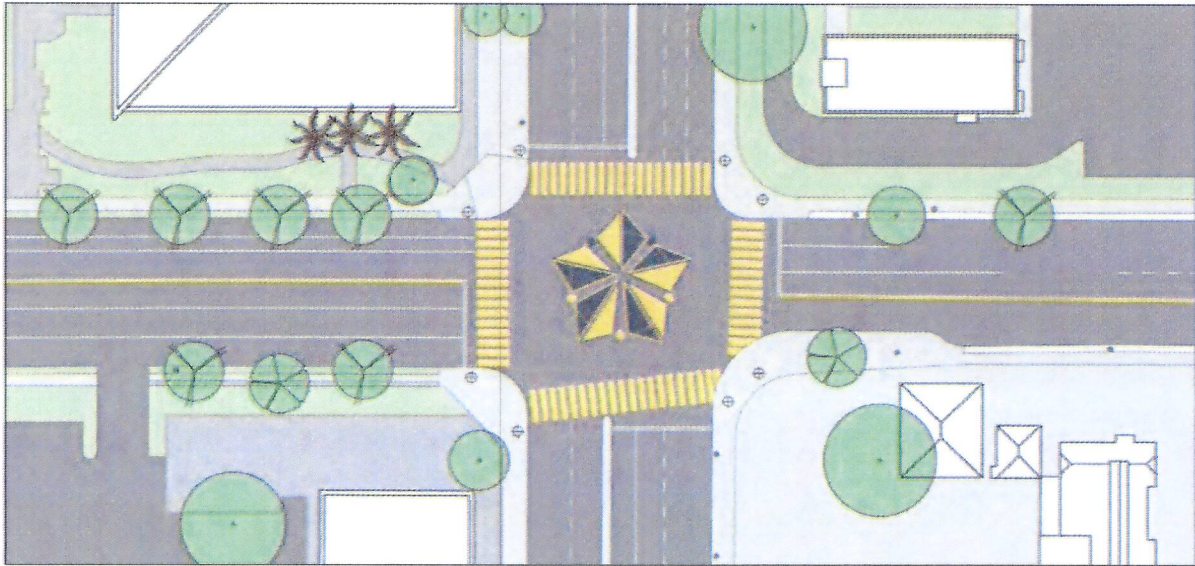
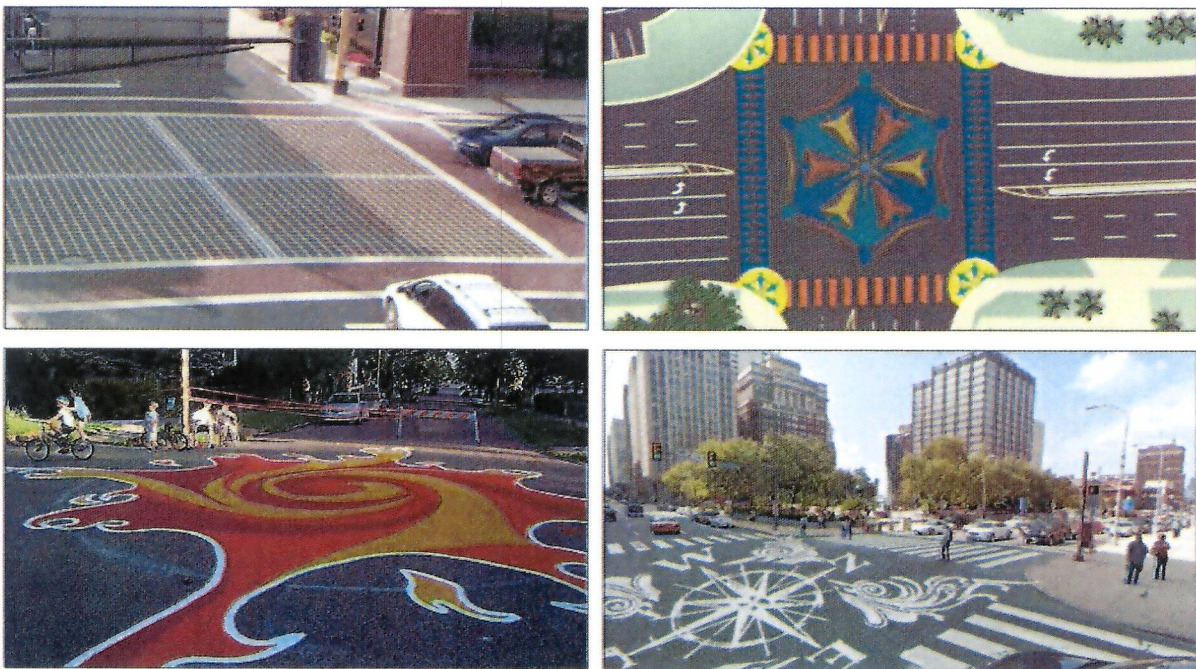


Figure 5-2: Wilmington Avenue and East 120th/East 119th Street intersection



Examples of paving designs that highlight special intersections.

intersection, and additional paving designs to mark this intersection as special. A gateway structure across Wilmington Avenue could also be explored. Examples of gateway structures from other communities are shown below.



Gateway structures that mark entries to special areas can also be used to celebrate the heart of a community.

D. Pedestrian Amenities

Elements of an engaging, pedestrian-oriented streetscape include: street trees (described in Section 5.3E), street lights, street furniture, wayfinding, and landscaped open space. In some instances, public art and water features may also be appropriate where it serves to interpret and draw attention to the history or culture of the area. In general, pedestrian amenities should be focused along the major streets and locations that have the highest amount of pedestrian activity.

1. **Street Furniture.** Various styles of street furniture can be found in the Plan Area. A consistent palette of street furniture will help unify the area, as well as provide essential seating, trash receptacles and bicycle racks. The WAAI Project is a first step in this process. As a part of this project, existing bus shelters, benches, trash receptacles and bicycle racks will be replaced or refinished to match the selected design theme. The WAAI street furniture palette is shown below. Trash receptacles similar to those proposed by the WAAI Project should be added at all intersections in the Plan Area. The feasibility of installation will depend on the site conditions at each intersection. ADA accessibility requirements must be followed to ensure that a clear path of travel is not impeded. Additional trash receptacles may be installed as pedestrian activity levels increases.

Site furnishings (benches, trash receptacles and bicycle racks, etc.) are also encouraged in setback areas, paseos, plazas, and courtyards. The selected site furnishings should be compatible in style with the buildings and selected to bring comfort, scale and design expression to the streetscape. It also must be highly durable and easy to maintain. All elements of the furniture palette should be uniform.



The selected street furniture palette in the Willowbrook Area Access Improvements project includes Landscape Forms' Presidio bench and trash receptacle in a silver metallic finish. (Source: County of Los Angeles Department of Public Works)

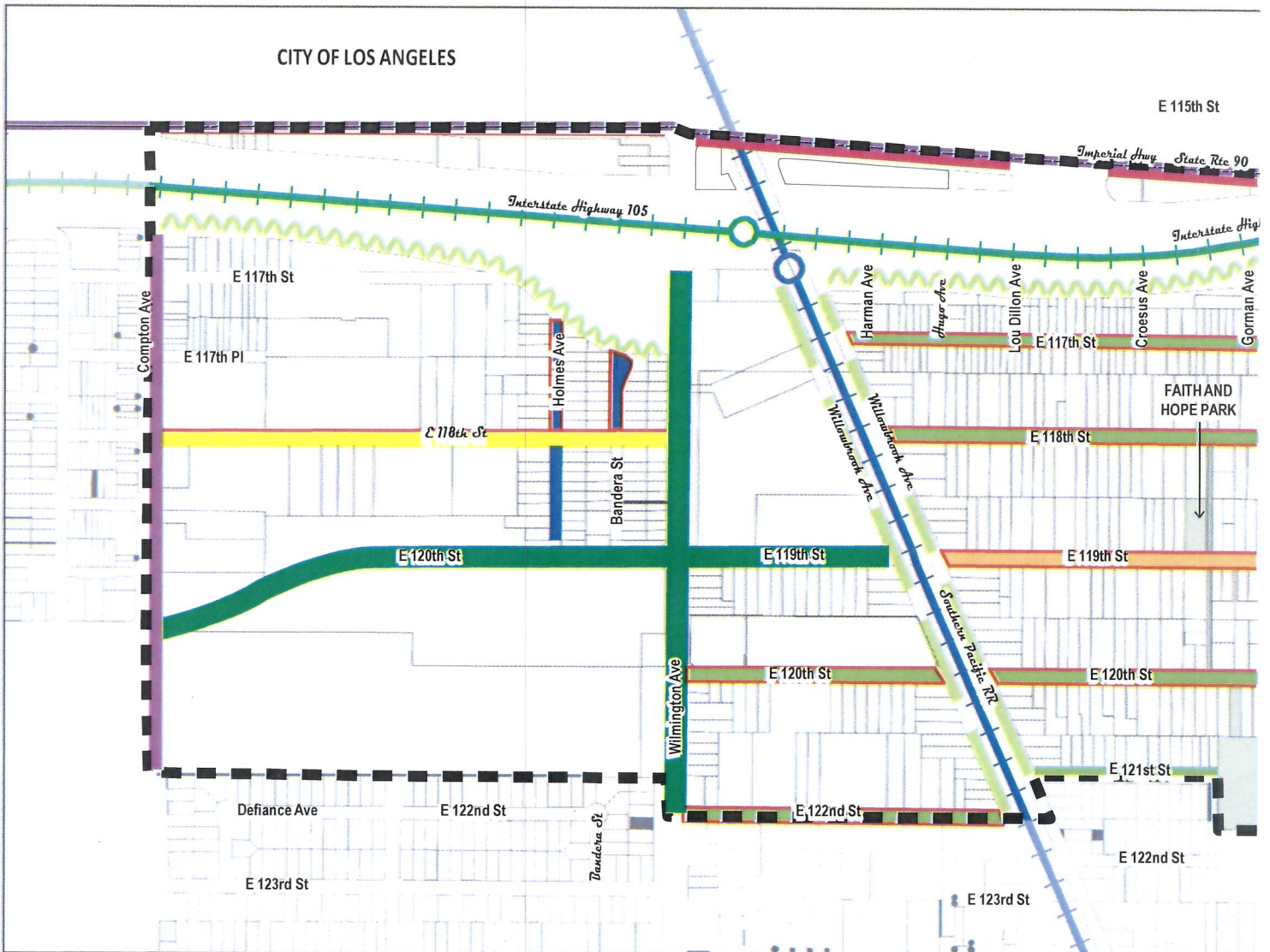
2. Wayfinding. Wayfinding helps people orient themselves in physical space and navigate from place to place through effective signage, markers, and/or monuments. It is how people choose a path within the built environment. A system of wayfinding signs and entry monument signs is a part of the WAAI Project. As shown below, these signs have a consistent design with a coordinated color palette that creates a unique theme (willow and brook) that is both recognizable and sensitive to its context. The Specific Plan recommends expanding this wayfinding system to the entire Plan Area. One of the key locations for this expansion is at the Willowbrook/Rosa Parks Station. Directional signage at this location should direct motorists and pedestrians to the rest of MLK Medical Center, CDU, Kenneth Hahn Plaza, as well as other public facilities and public parking.



Banners, directional signs and monument signs are a part of the Willowbrook Area Access Improvements Project. (Source: County of Los Angeles Department of Public Works)

3. Street Lighting. Lighting promotes safety and helps to create an appealing walkable environment. Lighting provides spatial definition to the sidewalk, adds ambiance to neighborhood settings, and affords a sense of security to users. The WAAI Project adds pedestrian-scaled lights to the key pedestrian streets. The Specific Plan recommends adding pedestrian-scaled lighting to the

CITY OF LOS ANGELES



- | | | |
|--|------------------------------------|---|
| County of Los Angeles | Existing Screening Vegetation | Callery Pear |
| Willowbrook TOD Specific Plan Area | Bronze Loquat or Pink Trumpet Tree | Chinese Pistache |
| Metro Green Line | Natchez Crape Myrtle | Pattern of
- Natchez Crape Myrtle
- Brisbane Box
- London Plane Tree |
| Metro Blue Line | Chinese Flame Tree | Turf Parkways (maintain existing) |
| Fraser's Photinia alternating with existing Canary Island Pine | Brisbane Box | |
| | Holly Oak | |

**Figure 5-3
Street Tree**

WILLOWBROOK

remainder of the streets in the Plan Area. The selected light fixtures should also adhere to guidelines set forth by the Dark Sky Association to protect the area's view of stars, as well as to avoid glare. Light fixtures in the public right-of-way shall also follow the Southern California Edison (SCE) standards for maintenance.

4. **Public Art.** Public art can complement and enhance the Plan Area, and make positive contributions to the public's experience and understanding of life in the area. Arts and culture help to enrich everyday life, stimulate the local economy, enhance the environment, celebrate the nature, engage citizens, and foster neighborhood pride. Art brings the community together, creating situations not merely for enjoyment and beauty, but also for dialogue, thought, and growth.

There are opportunities to encourage this evolving identity of the area by further integrating public art into the fabric and streetscape, such as at the Willowbrook/Rosa Parks Station, along major streets and activity centers.

E. Street Trees

The Plan Area has an inconsistent palette and pattern of street trees. Except for a few street segments, a mix of street tree species can be found in the Plan Area. This lack of uniformity contributes to the inconsistent and fragmented character of the Specific Plan area streets. A palette of carefully selected street trees will unify the variety of existing and future land uses in the Plan Area. In addition, street trees will provide shade, add seasonal color, define the street edge, and add to the urban forest. A uniform pattern of street trees will also increase connectivity and sense of cohesion on a pedestrian and auto-oriented scale, as well as offer a pleasant sidewalk experience.

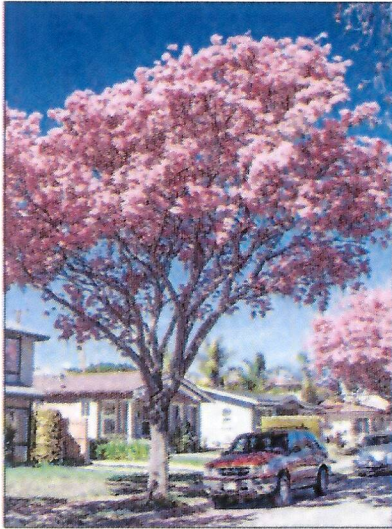
Table 5-1 lists and Figure 5-3 illustrates the street tree designations for the streets within the Specific Plan area. Figure 5-4 illustrates the Street Tree Palette. The street trees for Wilmington and East 120th/ East 119th Street (between Compton and Willowbrook Avenues), as planned in the WAAI Project, are also incorporated in this list. It also lists the existing street tree type found on each street. The designated street trees are also on the DPW Urban Forestry Unit's Maintenance District street tree lists.

Tree spacing recommendations for each street and tree type are also noted in Table 5-1. These are based on the canopy size of a mature tree of each species, the land uses along the streets, presence or absence of overhead power lines, and the available area for planting along the sidewalks and/or parkways. The final spacing will be determined by DPW and will need to consider field conditions including driveway aprons, street lights, and utilities on a block-by-block basis.

The following street tree and planting guidelines shall be adhered to for new street trees in addition to the standards established by DPW:

- All plant material shall meet the minimum standard of the American Nurserymen and Landscape Association and California Department of Agriculture Regulations, and be local container or field grown material.
- All trees shall be planted in accordance with established County planting standards.
- The minimum planting size for a tree in parkways and medians is a 24-inch boxed container.
- Median and parkway trees shall be trimmed to retain a trunk space clear of branches of at least 6 feet from grade at maturity.
- All street trees that are not placed in parkways shall be placed in a tree well per DPW specifications. Table 5-1 lists the minimum tree well dimensions.

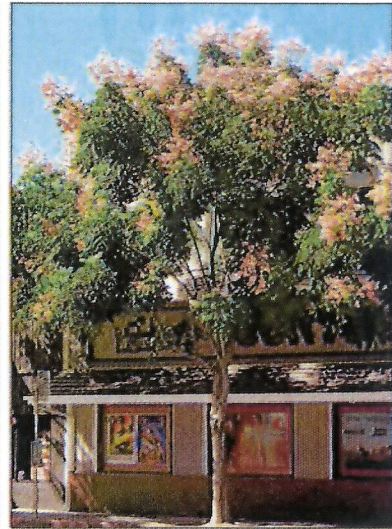
Figure 5-4: Street Tree Palette



Pink Trumpet Tree
Handroanthus impetiginosus
 Deciduous
 Height: 30-50 feet
 Canopy: 30 feet
 Bloom: Spring
 Water: low to moderate
 Powerline Friendly: Yes



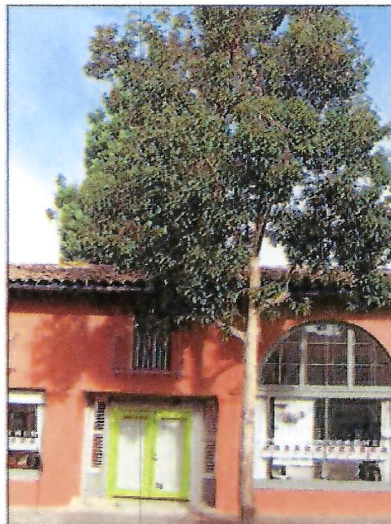
Bronze Loquat
Eriobotrya deflexa
 Evergreen
 Height: 25-30 feet
 Canopy: 20 feet
 Bloom: Winter
 Water: low to moderate
 Powerline Friendly: Yes



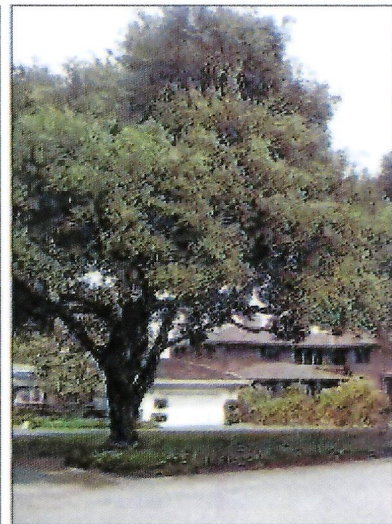
Chinese Flame Tree
Koelreuteria bipinnata
 Deciduous
 Height: 25-45 feet
 Canopy: 40 feet
 Bloom: Summer
 Water: moderate
 Powerline Friendly: Yes



'Natchez' Crape Myrtle
Lagerstroemia indica x fauriei
 'Natchez'
 Deciduous
 Height: 15-25 feet
 Canopy: 25 feet
 Bloom: Summer
 Water: low to moderate
 Powerline Friendly: Yes

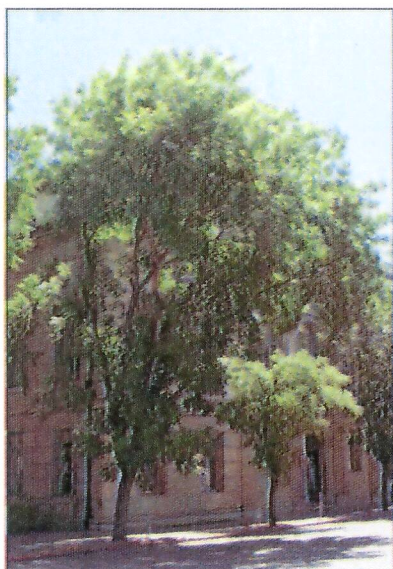


Brisbane Box
Lophostemon confertus
 Evergreen
 Height: 30-60 feet
 Canopy: 40 feet
 Bloom: Summer
 Water: low to moderate
 Powerline Friendly: No

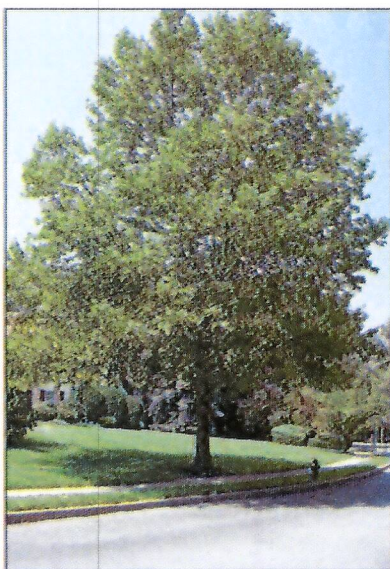


Holly Oak
Quercus ilex
 Evergreen
 Height: 30-65 feet
 Canopy: 50 feet
 Bloom: Spring, Summer
 Water: low to moderate
 Powerline Friendly: No

Figure 5-4 (continued)



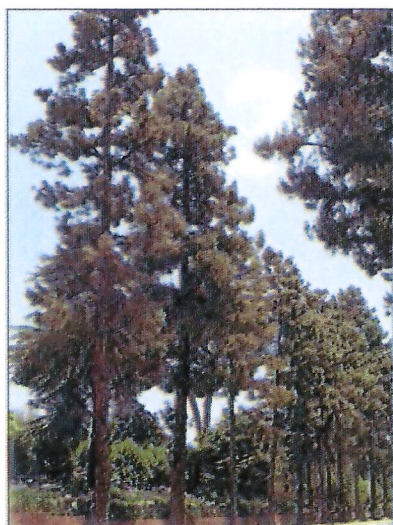
Chinese Pistache
Pistacia chinensis
Deciduous
Height: 60 feet
Canopy: 50 feet
Bloom: Winter
Water: low to moderate
Powerline Friendly: No



London Plane Tree
Platanus x acerifolia
'Bloodgood'
Deciduous
Height: 40-80 feet
Canopy: 70 feet
Bloom: Spring
Water: moderate
Powerline Friendly: No



Callery Pear 'Aristocrat'
Pyrus calleryana
Deciduous
Height: 35-45 feet
Canopy: 35 feet
Bloom: Spring
Water: moderate
Powerline Friendly: Yes



Canary Island Pine
Pinus canariensis
Evergreen
Height: 65-80 feet
Canopy: 40 feet
Bloom: None
Water: low to moderate
Powerline Friendly: No



Fraser Photinia
Photinia x fraseri
Evergreen
Height: 10-15 feet
Canopy: 20 feet
Bloom: Spring
Water: moderate
Powerline Friendly: Yes

**Table 5-1:
Street Tree Designations in the Specific Plan Area**

Street Names	Existing	Specific Plan (2015)	Recommended Spacing * (in feet)	Minimum Tree Well Dimensions ** (in feet)
North-South				
Compton Avenue (East side only)	Victorian Box <i>Pittosporum undulatum</i> Lemon Bottlebrush <i>Callistemon citrinus</i>	Brisbane Box <i>Lophostemon confertus</i>	30'-40'	5 sf
Holmes Avenue	Mixed	Chinese Flame Tree <i>Koelreuteri bipinnata</i>	30'-40'	5 sf/Turf Parkway
Wilmington Avenue	Mixed	Natchez Crape Myrtle <i>Lagerstroemia indica x fauriei 'Natchez'</i> Brisbane Box <i>Lophostemon confertus</i> London Plane Tree (Maintain Existing) <i>Platanus x acerifolia</i>	-- -- --	
Willowbrook Avenue	Canary Island Pine <i>Pinus canariensis</i>	Fraser Photinia <i>Photinia x fraseri</i>	Alternating between existing	Parkway
Mona Boulevard (West side only)	None	Holly Oak <i>Quercus ilex</i>	40'-50'	Turf Parkway
Bandera Street	None	Chinese Flame Tree <i>Koelreuteria bipinnata</i>	30'-40'	5 sf/Turf Parkway
East-West				
Imperial Highway (South side only)	Mixed	Chinese Pistache <i>Pistacia chinensis</i>	30'-40'	5 sf
117th Street	Mixed	Pink Trumpet Tree <i>Handroanthus impetiginosus</i> Alternative: Bronze Loquat <i>Eriobotrya deflexa</i>	25'-35'	5 sf/Turf Parkway
118th Street (West of Wilmington Avenue) (North side only)	Mixed	Callery Pear <i>Pyrus calleryana</i>	30'-40'	5 sf/Turf Parkway
118th Street (East of Wilmington Avenue)	London Plane Tree <i>Platanus x acerifolia</i>	Pink Trumpet Tree <i>Handroanthus impetiginosus</i> Alternative: Bronze Loquat <i>Eriobotrya deflexa</i>	25'-35'	5 sf/Turf Parkway
119th Street (East of Willowbrook Avenue)	Mixed	Natchez Crape Myrtle <i>Lagerstroemia indica x fauriei 'Natchez'</i> Brisbane Box <i>Lophostemon confertus</i>	20'-30'	4 sf/Turf Parkway
120th Street (East of Wilmington Avenue)	Mixed	Pink Trumpet Tree <i>Handroanthus impetiginosus</i> Alternative: Bronze Loquat <i>Eriobotrya deflexa</i>	25'-35'	5 sf/Turf Parkway
120th Street (West of Wilmington Avenue)	Canary Island Pine <i>Pinus canariensis</i>	Natchez Crape Myrtle <i>Lagerstroemia indica x fauriei 'Natchez'</i> Brisbane Box <i>Lophostemon confertus</i>	-- --	
121st Street (North side only)	None	Pink Trumpet Tree <i>Handroanthus impetiginosus</i> Alternative: Bronze Loquat <i>Eriobotrya deflexa</i>	25'-35'	5 sf/Turf Parkway
122nd Street (North side only)	Mixed	Pink Trumpet Tree <i>Handroanthus impetiginosus</i> Alternative: Bronze Loquat <i>Eriobotrya deflexa</i>	25'-35'	5 sf/Turf Parkway

* Recommended spacing is based on canopy size of mature trees, available planting areas along sidewalks/parkways and adjacent land uses.

** Minimum Tree Well Dimension: Optimal sizes provided. Actual size and planting shall be dependent on sidewalk width, adjacent land uses and tree type.

F. Parks and Open Space

Existing open space resources in the Plan Area include Mona Park, Faith and Hope Park, and the MLK Fitness Garden. Mona Park is a 8.4-acre recreational facility that includes a softball and multi- purpose fields, a gymnasium and a swimming pool and features year-round recreational programming including youth sports, the summer lunch program, after-school snacks, and after-school day camp programs. The Faith and Hope Park opened in 2016 on the corner of East 119th Street and the planned Green Alley of Family and Fellowship. Another important open space resource is the MLK Fitness Garden at the MLK Center for Public Health. Primarily located along the 120th Street frontage, the MLK Fitness Garden incorporates a walking path and par course fitness equipment for employees and the public.



Faith and Hope Park opened in 2016 along East 119th Street.



MLK Fitness Garden is located along 120th Street.



Mona Park connects to East 120th Street along a landscaped paseo (shown on the right).

The recommended strategies for parks and open space build upon the existing park space in the Plan Area. These strategies include:

- Explore joint use policy with the schools to better utilize existing and future open space resources.
- Encourage new development to provide public open space as a community benefit. Potential locations are shown in Figure 5-1.
- Consider building pocket parks and community gardens on County-owned vacant lots.
- Connect existing and future parks and open spaces with the street network to allow easy pedestrian and visual access.

G. Mona Boulevard Bicycle/Pedestrian Path

Mona Boulevard provides access from the residential areas to the key activity centers in the Plan Area. Mona Boulevard also provides north-south pedestrian access on the east side of the Plan Area including access to Mona Park, the MLK Elementary School and the Dr. Ralph Bunche Middle School.

Mona Boulevard from the I-105 Freeway to 124th Street will be converted from a four lane street to a three lane street, and a pedestrian/bicycle trail installed on the west side of the street. Parking will be retained on the west side of the street. The pedestrian/bicycle trail shall include a paved section to meet Class I design guidelines. The design of the trail will be subject to DPW requirements. Figure 4-3 in Chapter 4 of this Specific Plan illustrates a typical cross section of reconfigured Mona Boulevard.



The Mona Boulevard Bicycle/Pedestrian Path will provide for a Class I Bicycle Path connection, completing the bicycle network in the Plan Area. The pedestrian path will connect the residential neighborhoods with Mona Park, MLK Elementary School and Dr. Bunche Middle School.

6.1 Introduction

The purpose of this chapter is to provide an overview of existing and planned infrastructure facilities, and to outline recommended infrastructure upgrades for the Plan Area. The recommended upgrades are based on analyses of the capabilities and capacities of existing facilities and projected infrastructure needs based on build-out of the Willowbrook TOD Specific Plan. Figure 6-1 shows how the Plan Area has been divided into “groups” for the purposes of the analyses.

6.2 Water System

Three water purveyors serve the Plan Area: Liberty Utilities (formerly Park Water Company), Golden State Water Company and Los Angeles Department of Water and Power (LADWP). Park Water Company provides water service to the area west of Wilmington Avenue, and Golden State Water Company provides water service to the area east of Wilmington Avenue. There are potable water lines under almost all streets and alleys within the Plan Area. The sizes of the potable water lines vary from 4 to 12 inches. The existing pipes that distribute water services in the Plan Area include steel pipe (ST), asbestos cement pipes (AC), ductile iron pipes (DI), and cast iron pipes (CI). Figure 6-2 shows the layout of the existing water lines. This figure also illustrates the upgrades necessary to meet the projected demand.

As shown in Figure 6-2, except for Group 14 (which remains unchanged), almost all development capacity groups under total build-out of the Specific Plan have some increase in land use density, which in turn results in an increase in water demand. Most of the existing water lines in the Plan Area are 8 inches in diameter and above. The 8 inch pipes generally have the capacity to handle the increase in water demand/load under total build-out of the Plan Area. However, to meet the demand of water consumption, including water services for fire prevention purposes, such as demand of new fire hydrants and fire sprinkler systems in the new developments in the post total “build-out” condition, the following upgrades in water line pipe sizes are recommended:

Group 2A, 2B:

Replace existing water line of 4 inch CI on 118th Street north of this area with 8 inch DI (or PVC C-900).

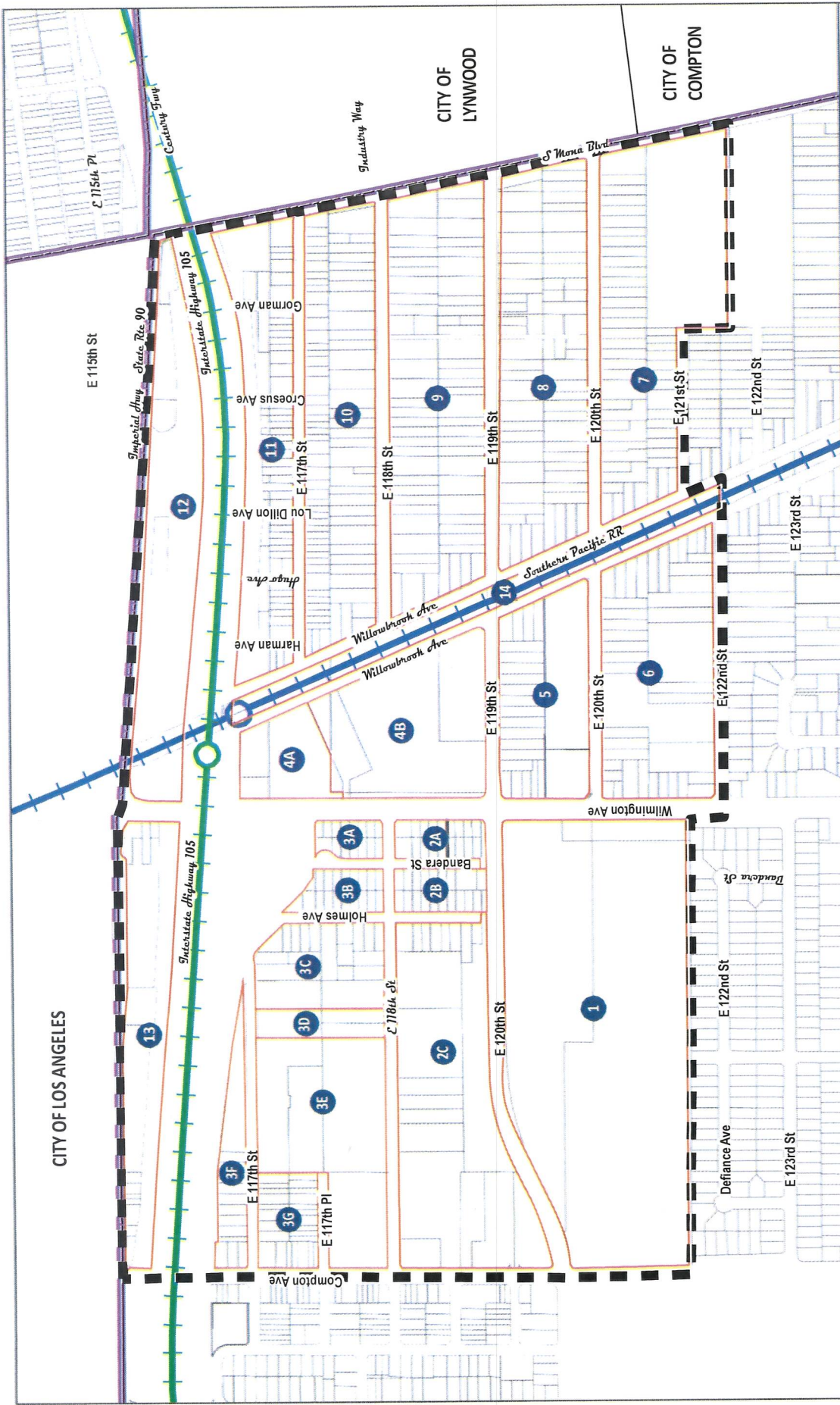


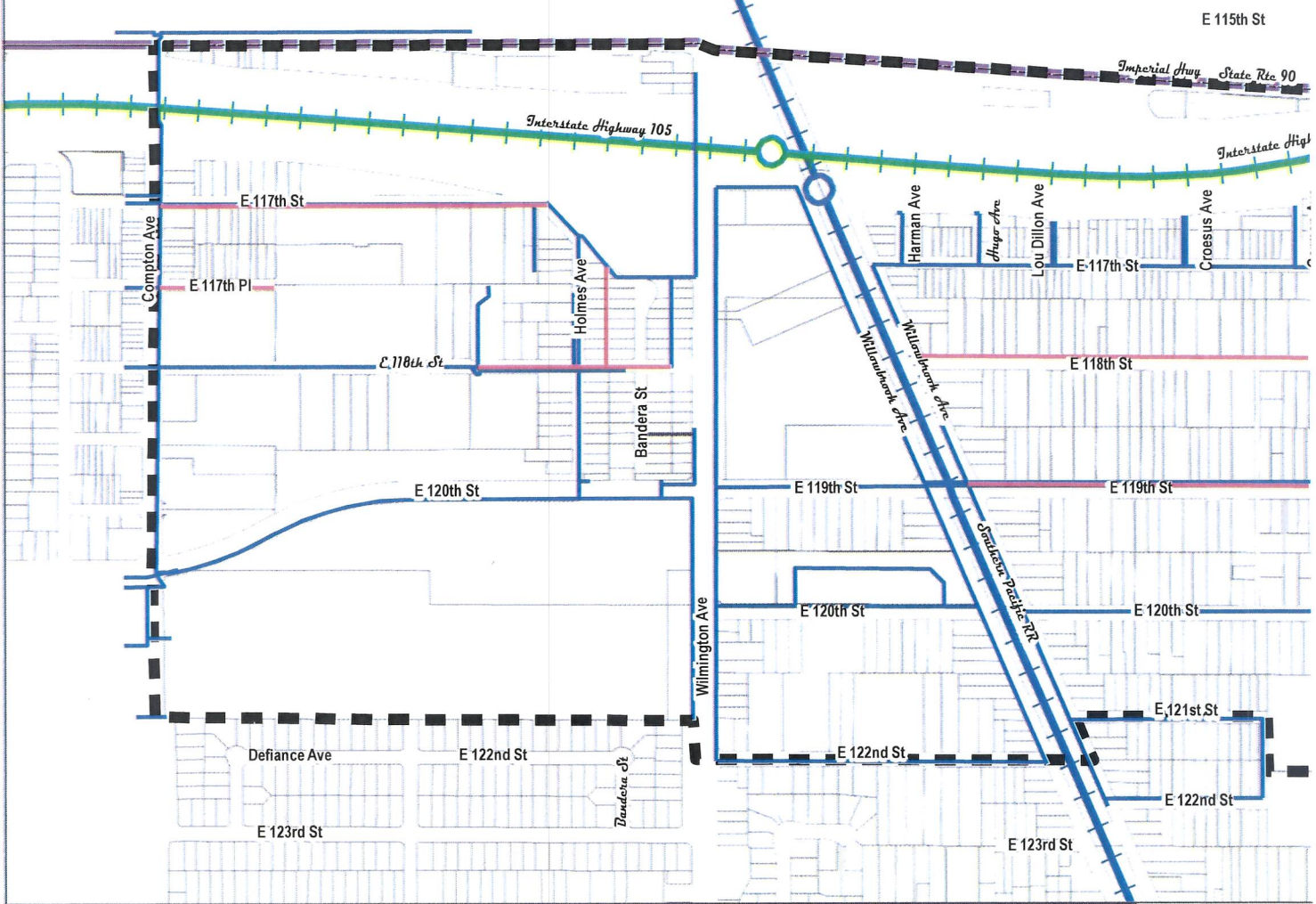
Figure 6-1
Key Map by Block for Calculating
Infrastructure Demand

WILLOWBROOK TOD SPECIFIC PLAN



- County of Los Angeles
- Willowbrook TOD Specific Plan Area
- Metro Green Line
- Metro Blue Line

CITY OF LOS ANGELES




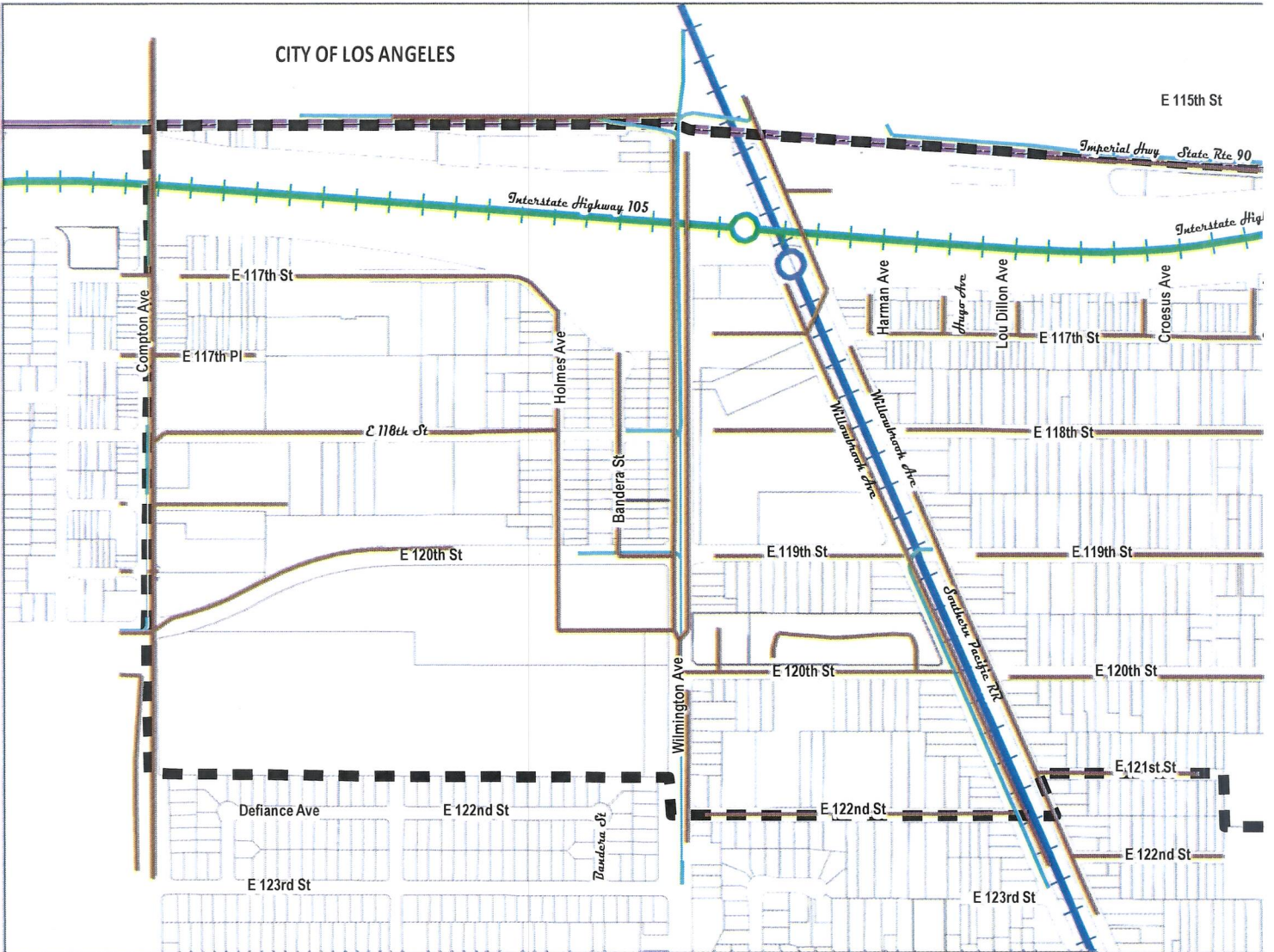
-  County of Los Angeles
-  Willowbrook TCD Specific Plan Area
-  Metro Green Line
-  Metro Blue Line
-  Existing Water System
-  Upgrade 4 or 6" DI Water to 8" DI Water

Figure 6-2
Existing and Proposed
Water System

WILLOWBROOK

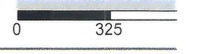




- County of Los Angeles
- Willowbrook TOD Specific Plan Area
- Metro Green Line
- Metro Blue Line
- Existing Sewer System
- Existing Storm Drain System

Figure 6-3
Existing Se
Storm Drai

WILLOWBROOK



Group 3B:

Replace existing water line of 6 inch CI in the alley between Holmes Avenue and Bandera Street with 8 inch DI (or PVC C-900).

Group 3C, 3D, 3E, 3F, 3G:

Upgrade existing water line of 4 inch CI on 117th Street from Compton Avenue to Holmes Avenue and the 4 inch CI in 117th Place with 8 inch DI (or PVC C-900).

Group 8, 9:

Upgrade existing water line of 6 inch CI and 4 inch CI in 119th Street to 8 inch DI (or PVC C-900) from Willowbrook Avenue to Mona Boulevard.

Group 9, 10:

Replace existing water line of 6 inch CI and 4 inch CI in 118th Street with 8 inch CI from Willowbrook Avenue to Mona Boulevard.

All three water purveyors will evaluate future development within their service areas and conduct their own detailed analysis, and will ultimately decide whether the water distribution facilities are required to be upgraded.

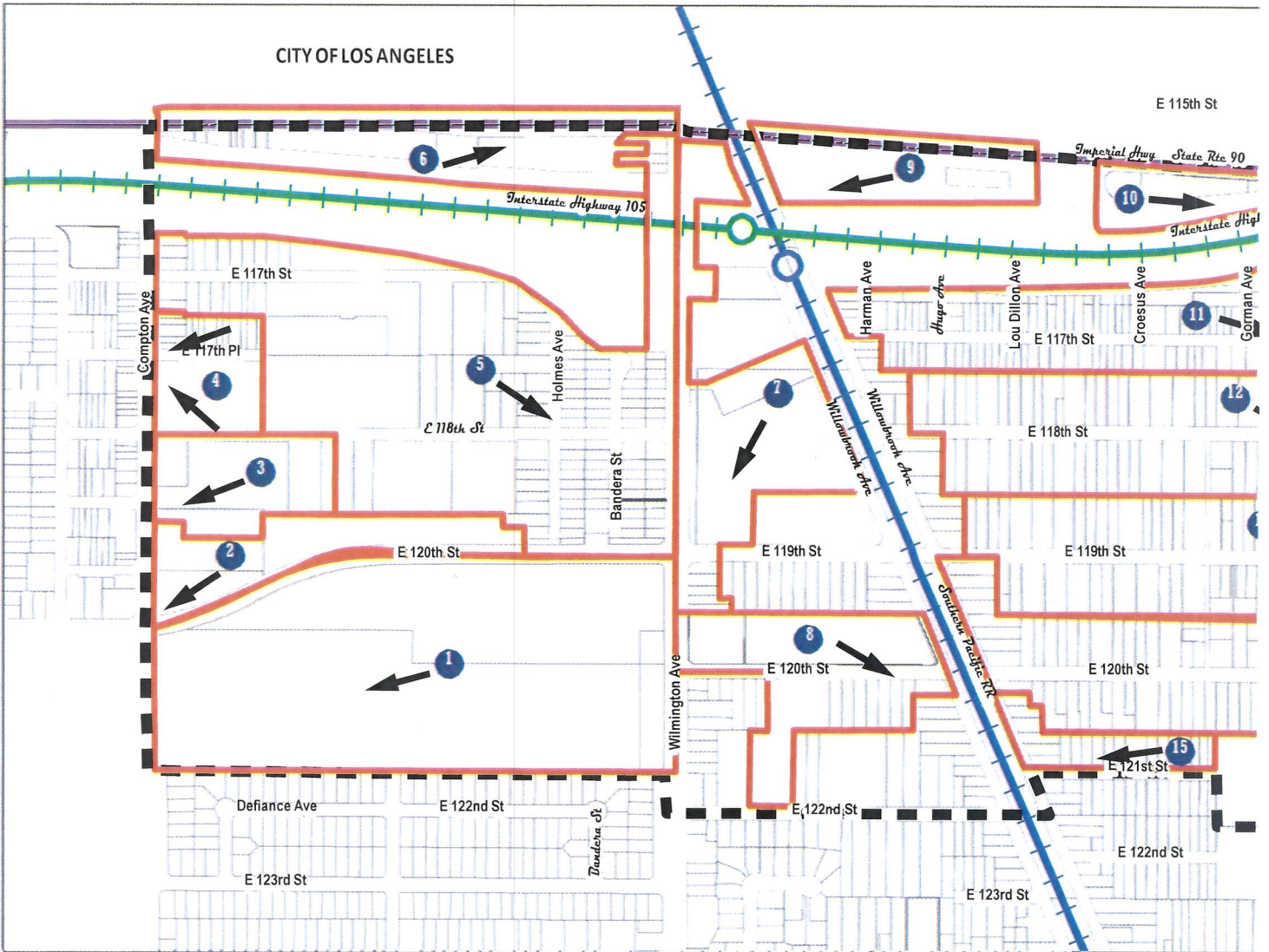
6.3 Sewer System

The sewer lines serving the Plan Area are owned and maintained by DPW and the Sanitation Districts of Los Angeles County (LACSD). The sewer lines within the area are mainly Vitrified Clay Pipes (VCP). The sewer mains are all under the major streets with smaller lateral lines connecting directly to residential buildings or businesses (Figure 6-3).

As mentioned earlier in the water system section, except for Group 14 (which remains unchanged), almost all development capacity groups under total build-out of the Specific Plan have some increase in land use density, which in turn results in an increase in sewage effluent throughout the Plan Area. The cumulative sewage effluent by each Tributary Area (shown in Figure 6-4 and Table 6-1) has been calculated using the Zoning Coefficient for runoff provided for a typical sewer area study by the DPW Land Development Division. All existing sewer mains in the Plan Area are 8" or above in diameter. Using the minimum allowable slope of 0.24 percent, the design capacity for the existing sewer mains are at least 0.27 cfs (some pipes are larger than 8 inch). There are five (5) existing sewer collection points that may not be adequate to meet the future cumulative sewage effluent.


One of the purposes of this study is to identify the deficiencies, if any, of the existing utilities when compared to the future developed condition. This study has identified that the developments in Tributary Areas 5, 7, 8, 11, and 16 have the potential to exceed the existing sewage capacity of their connecting sewer mains. The rest of the groups should remain below the sewage capacity.

Since the design capacity refers to future full build-out conditions there is no real justification to upgrade any of these sewer mains at this time. Regardless, DPW will require a detailed sewer area study of any new development within Tributary Area 5. The sewer area study will determine whether any upgrade will be required for sewer system network at the time of that specific development. The LACSD will also review all new sewer infrastructure upgrades as DPW will send improvement plans for the district to review.



-  County of Los Angeles
-  Willowbrook TOD Specific Plan Area
-  Metro Green Line
-  Metro Blue Line
-  Tributary Area

**Figure 6-4
Tributary Area
Calculating
WILLOWBROOK**



**Table 6-1:
Sewer Area Study Calculations by Tributary Area**

Street Name	Pipe		Area (Acres)	Calculated Flow (cfs)	Cumulative Calculated Flow* (cfs)	Cumulative Depth (inches)	Comment
	Size (inches)	Slope (%)					
Tributary Area 1 (Compton Ave)	8	0.24					<i>Not Part of Calculations</i>
Medical Buildings & Residential (R-3)			N/A				<i>Private Sewer Line</i>
Tributary Area 2 (Compton Ave)	8	0.24			0.1836	3.2	
Commercial			3.60	0.0540			
High School (1,675 people x 20 gal/day x 2.5)			N/A	0.1296			1,600 students & 75 staff
Tributary Area 3 (Compton Ave)	8	0.24			0.1350	2.8	
Residential (R-3)			4.50	0.1350			
Tributary Area 4 (Compton Ave)	8	0.24			0.1041	2.4	
Lincoln ES 2.5 x ((280 students x 15 gal/day) + (50 staff x 20 gal/day))			N/A	0.0201			280 students & 50 staff
Mixed Use 2 (60 du/acre)			1.40	0.0840			
Tributary Area 5 (Wilmington Ave)	8	0.24			1.9023	6.4	
Lincoln ES 2.5 x ((280 students x 15 gal/day) + (50 staff x 20 gal/day))			N/A	0.0201			280 students & 50 staff
Charter ES 2.5 x ((336 students x 15 gal/day) + (35 staff x 20 gal/day))			N/A	0.0222			336 students & 35 staff
Mixed Use 2 (60 du/acre)			31.00	1.8600			
Tributary Area 6 (Imperial Hwy)	8	0.24			0.1061	2.5	
Commercial			7.07	0.1061			
Tributary Area 7 (Wilmington Ave)	8	0.24			0.4437	5.4	
Mixed Use 1 (30 du/acre)			14.79	0.4437			
Tributary Area 8 (Willowbrook Ave)	15	0.12			2.7945	12.1	
Residential (R-3)			11.69	0.3507			
Residential (R-2)			3.60	0.0720			
Residential (R-1)			2.86	0.0257			
Tributary Area 9 (Willowbrook Ave)	8	0.24			0.0356	1.4	
Commercial			2.37	0.0356			
Tributary Area 10 (Mona Blvd)	8	0.24			0.0323	1.4	
Commercial			2.15	0.0323			
Tributary Area 11 (Mona Blvd)	8	0.24			0.2887	4.2	
Residential (R-3)			0.99	0.0297			
Residential (R-2)			12.95	0.2590			
Tributary Area 12 (Mona Blvd)	8	0.24			0.1654	3.1	
Residential (R-1)			18.38	0.1654			
Tributary Area 13 (Mona Blvd)	8	0.24			0.1516	3.0	
Residential (R-1)			16.84	0.1516			
Tributary Area 14 (Mona Blvd)	8	0.24			0.2006	3.4	
Residential (R-3)			2.17	0.0651			
Residential (R-1)			15.05	0.1355			
Tributary Area 15 (Willowbrook Ave)	8	0.24			0.1310	2.8	Manhole 48
Residential (R-3)			3.70	0.1110			Includes 117th to 119th
Residential (R-1)			2.22	0.0200			
Tributary Area 16	12	0.24			1.0468	7.1	Trunk Line
Residential (R-3)			19.00	0.5700			at Oris and Willowbrook
Residential (R-2)			16.64	0.3328			
Residential (R-1)			16.00	0.1440			

* Based on current land use and coefficients per DPW.

Sewer laterals are sewer pipes which connect with buildings with sewer mains that run in the street. In general, new or upgraded sewer laterals are required for new buildings, with the cost of the installation borne by the developers.

6.4 Storm Drain System

The storm drain system serving the Plan Area is maintained by the Los Angeles County Flood Control District (LACFCD). The storm drain system within the Plan Area consists of mostly underground Reinforced Concrete Pipes (RCP) ranging from 24 to 54 inches (Figure 6-3).

Build-out of the Plan Area will generate little or no increase in runoff to the existing drainage system, since the area is completely developed. Approximately 80-90 percent of the existing Plan Area is impervious. It is predicted that the new development will not directly trigger any need for upgrades to the County's existing storm drain major backbone facilities, mainly due to the Low Impact Development (LID) Ordinance requirements for percolation and on-site detention for new development, which will stabilize and/or even reduce runoff in the Plan Area.

There are no recommendations to upgrade the existing storm drain system within the Plan Area.

LID Practices and Project Requirements

In October 2008, the County adopted the LID Ordinance as part of Los Angeles County Code Title 12, Chapter 84 to require the use of LID principles in all development projects except road and flood infrastructure projects. Unlike traditional stormwater management, which collects and conveys stormwater runoff through storm drains, pipes, or other conveyances to a centralized stormwater facility, LID uses site design and stormwater management to maintain the site's pre-development runoff rates and volumes. The goal of LID is to mimic a site's pre-development hydrology by using design techniques that infiltrate, filter, store, evaporate, and detain runoff close to the source of rainfall.

LID practices or stormwater quality control measures can be categorized into the following types:

- *Retention-based stormwater quality control measures*
Examples: bio-retention, infiltration basin, dry well, permeable pavement.
- *Bio-filtration*
Example: bio-filtration area.
- *Vegetation-based stormwater quality control measures*
Example: stormwater planter (or planter box), vegetated swale, green roof.
- *Treatment-based stormwater quality control measures*
Examples: sand filter, constructed wetland, proprietary treatment control measures.

All new development that requires new grading in the Plan Area will require preparation of a hydrology study to demonstrate that building sites are free from flooding hazard. New development will be required to mimic the site's pre-development runoff by choosing the appropriate LID practice most suitable for the site. A proposed project must demonstrate that any proposed improvement, including filling, does not raise the flood level upstream or downstream of the project. As required by the County, developers shall prepare National Pollution Discharge Elimination System (NPDES) reports, such as the Water Quality Management Plan (WQMP) and Stormwater Pollution Prevention Plan (SWPPP), to ensure the quality of water is preserved and adverse environmental impacts are minimized.

6.5 Electrical System

Southern California Edison (SCE), which is a private utility company, provides electrical power service to the Plan Area. SCE sets its own service standards, with oversight from the California Public Utilities Commission (CPUC), and facility improvement strategies. Currently, there is a network of aerial electrical facilities creating a power grid that supplies sufficient electrical service to the Plan Area. There is no major deficiency or functional problem in the power supply facilities within the Plan Area. The specific locations of the existing underground and overhead electrical lines are illustrated in Figure 6-5.

The decision to upgrade the power supply facilities and the number of upgrades to meet the demand of future development will be determined by SCE in coordination with the County after developers have submitted their building plans. Demand for services and the ability to serve new developments are generally determined on a case-by-case basis.

Most of the electrical distribution facilities in the Plan Area are aerial facilities. The Specific Plan recommends that existing aerial electrical facilities be placed underground whenever funding is available. Underground electricity provides higher reliability, is safer in general, and is also less unsightly. One of the potential funding mechanisms is CPUC Rule 20. CPUC Rule 20 is a set of policies and procedures established by the CPUC to regulate the conversion of overhead electric equipment to underground facilities, a process often referred to as “undergrounding.” Rule 20 determines the level of ratepayer funding for different undergrounding arrangements.

CPUC Rule 20

Under Rule 20, undergrounding projects are financed by utility rate money, combined rate funds and local tax proceeds, or private funds, depending on whether Rule 20A, Rule 20B or Rule 20C provisions apply.

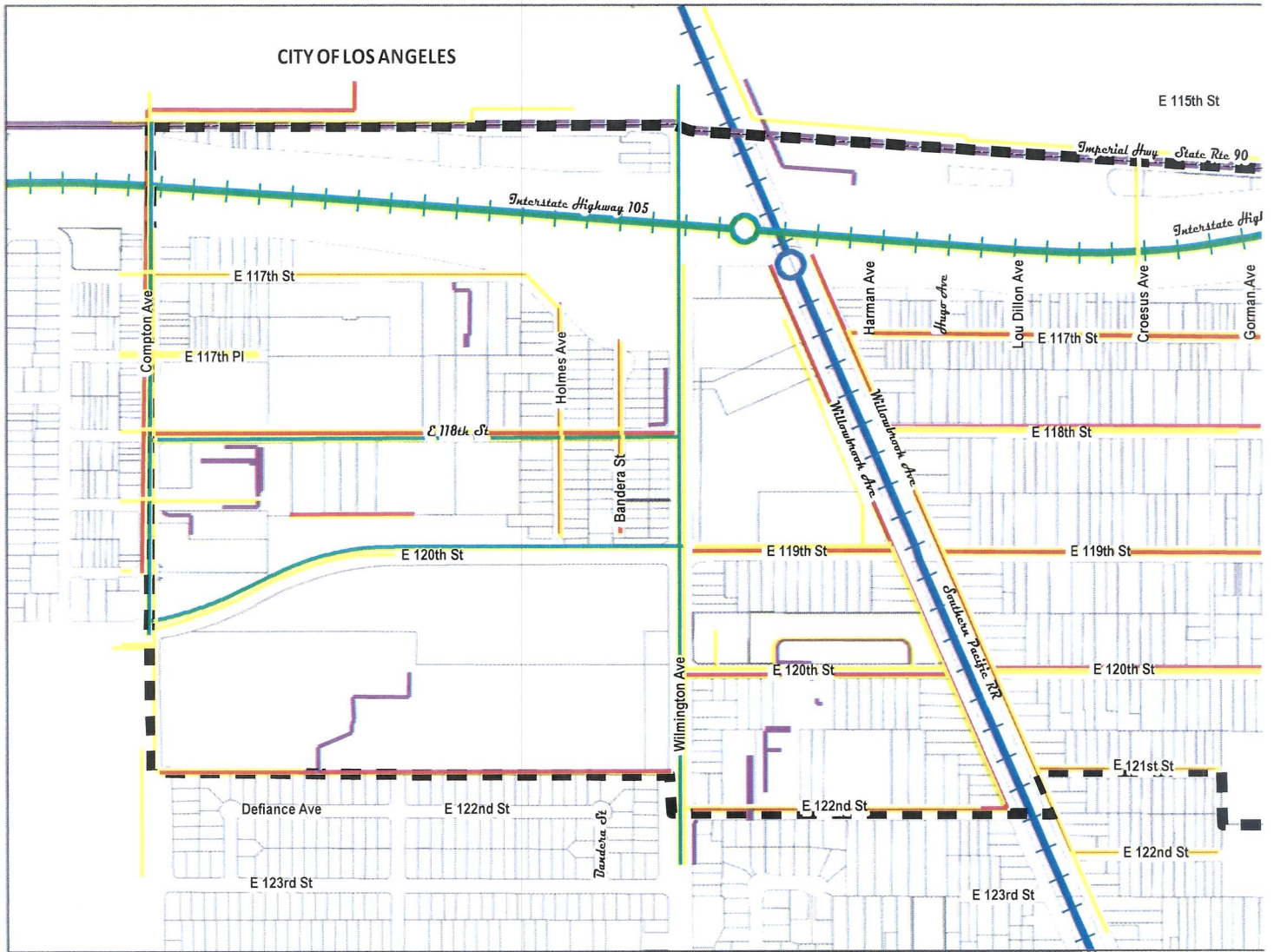
Rule 20A: *Rule 20A projects are paid for by all SCE customers and ratepayers, not just those who live in locations where facilities will be undergrounded. To qualify for full funding through utility rate proceeds, projects must produce a benefit to the general public, not just customers in the affected area, by satisfying one or more of these criteria:*

- *The location has an unusually heavy concentration of overhead facilities.*
- *The location is heavily traveled.*
- *The location qualifies as an arterial or major collector road in a local government’s general plan.*
- *The overhead equipment must be located within or pass through a civic, recreational or scenic area.*

Using CPUC formulas, SCE allocates rate funds to communities for undergrounding based on previous allocations, the ratio of customers served by overhead facilities to all the customers in the community, and the fraction that customers in the community represent of all SCE customers. Local governments use these formulas to project allocations, which allow them to prioritize projects and develop project schedules. Because funds are limited, local governments sometimes must wait and accumulate their allocations before starting an undergrounding project.

Rule 20B: *If an area is not eligible for Rule 20A or if local government cannot or chooses not to rely on the Rule 20A allocation process, Rule 20B allows rate funds to subsidize an undergrounding project. The subsidy includes an amount equal to about 5 to 20 percent of the total cost. The remaining cost is funded by local governments or through neighborhood special assessment districts.*

Rule 20C: *Rule 20C enables property owners to pay for undergrounding electric lines and equipment if neither Rule 20A nor 20B applies.*



- County of Los Angeles
- Willowbrook TOD Specific Plan Area
- Metro Green Line
- Metro Blue Line
- Electrical, Telecommunications, Cable TV Aerial Facilities
- Gas
- Underground Telecommunications Conduit
- Cable Television

**Figure 6-5
Existing Di**

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It is also worth noting that, prior to any undergrounding process, SCE will take the lead in contacting the other low voltage utilities that might be sharing the power poles with SCE aerial facilities, such as the telecommunication provider (AT&T) and cable TV provider (Spectrum) to coordinate and execute a joint trench. During this process, SCE will also reach out to DPW, Public Utility Division, for their assistance in contacting and coordinating with the aforementioned utility providers.

It is recommended that prior to approving new development, the County should contact the SCE regional manager in the Willowbrook area and set up an inspection at the new development site to determine whether the existing aerial SCE facilities in the area qualifies for Rule 20A funding.

6.6 Natural Gas System

The Southern California Gas Company/Sempra Utilities (The Gas Company), which is a private utility company, is the gas service provider for the Plan Area. Currently, gas pipelines are in all major streets in the Plan Area, specific locations of gas pipelines are illustrated in Figure 6-5.

The analysis on the capacity and capability to meet future demand will be conducted by The Gas Company in coordination with the County upon submittal of building plans by developers.

6.7 Telecommunications System

The Plan Area is within the service area of AT&T, which is a private utility company. AT&T is the provider of both local and long distance telecommunications in the Plan Area (Figure 6-5). The current local telecommunications network lacks the high speed internet service that will be in demand by future businesses and residents in the area.

AT&T will assess the demand for services and the ability to serve new developments on a case-by-case basis. The capacity and capability analysis for meeting future demands within the Plan Area will be conducted by AT&T after building plans are submitted by developers.

If there is “undergrounding” of existing aerial telecommunication facilities, SCE will reach out to AT&T and coordinate a joint trench. To save on costs, AT&T is likely to participate in the joint trench operation. SCE will dictate the layout of the trench, and the telecommunication lines would follow. According to the representative from AT&T, there are no major facility upgrades planned in the Plan Area in the near future. However, if there are facilities upgrades in the future, in most cases, AT&T will be responsible for the construction cost up front, and will recoup the cost later with the additional revenue from added customers.

6.8 Cable Television System

The cable service provider for this area is Spectrum, which is a private utility company. Most of the existing cable TV systems within the Plan Area are aerial facilities that share power poles with the electrical system, with a few existing underground conduits (Figure 6-5).

New land uses resulting from the Specific Plan will consist of both commercial and residential development; therefore, high speed internet and cable television services will be in demand. Similar

to a telecommunications system, Spectrum will assess the demand for services on a case-by-case basis and ultimately make the decisions concerning upgrades for the existing cable TV systems to meet the demand of the future developments.

If there is undergrounding of existing aerial telecommunication facilities, SCE will reach out to Spectrum and coordinate a joint trench. Similar to a telecommunications provider, Spectrum is likely to participate in the joint trench operation to save on construction costs.

In most cases, if there are facilities upgrades required, Spectrum will be responsible for the construction costs up front, and recouping the cost later on with the additional revenue from the added customers.

Economic Development Strategy

7.1 Introduction

The Plan Area is expected to undergo sizable growth in employment to support uses related to the hospital, medical offices, education and research and development, and retail and commercial offices. The purpose of this chapter is to provide the Economic Development Strategy for the Willowbrook TOD Specific Plan. The strategy provides economic analysis, a financing strategy, and policy recommendations to realize the full potential of Metro's investments in the redesigned Willowbrook/Rosa Parks Station and the County's investments in the MLK Medical Center Campus. The economic development strategy includes:

- Strategies that leverage public investments within the Specific Plan framework and build upon local assets to revitalize the Willowbrook community;
- Identification of potential community benefits from the implementation of the Specific Plan;
- Projections of employment and wage growth from implementation of the Specific Plan;
- Capital Improvement Program (CIP) that identifies and prioritizes a range of infrastructure and amenity improvements that benefit the community; and
- Financing approaches for the implementation of infrastructure and services.

The SCAG employment and demographic forecasts for the study area and the MLK Medical Campus Master Plan, as well as supporting documents and studies for the MLK Community Hospital and CDU informed the strategies.

7.2 Opportunities for Economic Development

The rehabilitation and expansion of the MLK Medical Campus, the redesign of the Willowbrook/Rosa Parks Station and the potential demand for ancillary medical services that may locate nearby, are the main economic opportunity drivers for the area. It is important to leverage these economic investments to provide employment and housing development opportunities to the Specific Plan area and the greater Willowbrook community.

7.3 Vision for a Community of Health and Wellness

The revitalized MLK Medical Campus is a catalyst for broader economic development and community service benefits to the Willowbrook and neighboring communities. By facilitating bicycle and pedestrian improvements and increasing transit access and housing choices, this Specific Plan helps realize the community's vision for health and wellness. This vision was inspired by an ULI Technical Assistance Panel study in 2009. The ULI panel identified the entire master-planned area as having the potential to become a health-focused, transit-oriented district.

The Plan Area has several interconnected subareas located to the west of the rail tracks and south of the freeway: 1) MLK Medical Center Subarea encompassing the MLK Medical Center campus; 2) CDU Campus Subarea encompassing CDU and King Drew Magnet High School; 3) Northwest Subarea including Lincoln-Drew Elementary School, Barack Obama Charter Elementary School, a proposed community college workforce training center, and an expanded County library; 4) the Kenneth Hahn Plaza Subarea; 5) single and multi-family homes and sites with retail and mixed use development potential on Wilmington Avenue; and 6) the Willowbrook/Rosa Parks Station Subarea.

7.4 Economic Development Strategy

The Economic Development Strategy recommends implementation strategies. Key components of the strategy are discussed in this section and summarized in Table 7-1. The steps and actions within the recommended implementation strategy are contingent upon funding and allocation of resources. The timeline identified is a relative estimate in comparison to other steps and actions within the strategy. Other key components are public financing and economic incentive methods for implementing the conceptual CIP in the Plan Area, as presented in Chapter 8.

The Economic Development Strategy presents a range of public and private financing techniques and related programs that could be used as part of the implementation strategy for the CIP, including land use incentive strategies, financing program initiatives, and public-private partnerships that build upon the substantial public and private investment into the MLK Community Hospital, CDU and the redesign of the Willowbrook/Rosa Parks Station.

Additionally, the CIP compiles the required public infrastructure and related costs to implement the Specific Plan. The CIP also identifies the projects that are essential to attract private investment within the Plan Area, and presents short-, mid- and long-range priorities over the anticipated build-out of the Plan Area.

Furthermore, the Economic Development Strategy presents five categories of actions to facilitate local economic development objectives. These categories highlight actions available to local County government to promote the vision of the Specific Plan. These actions include: a) institutional arrangements; b) targeted development opportunities; c) land use planning; d) infrastructure and services planning; and e) implementation and financing strategies.

**Table 7-1:
Implementation Strategy**

Implementation Step ²	Key Action ²	Coordinating Agencies	Timeframe ¹
Develop Institutional Arrangements	<ul style="list-style-type: none"> Work with the MLK Health and Wellness Community Development Corporation (MLK-CDC). 	County of Los Angeles Community Based Stakeholders	Short-Term
Foster Public-Private Partnerships	<ul style="list-style-type: none"> Coordinate with MLK Community Hospital and Charles Drew University to integrate local workforce training and capabilities with ultimate job growth, direct spin-off activities in healthcare services and allied office uses. 	County of Los Angeles MLK Community Hospital Charles Drew University MLK-CDC	Ongoing
Improve Kenneth Hahn Plaza	<ul style="list-style-type: none"> Work with Kimco Realty Corporation, the operator of Kenneth Hahn Plaza, to enhance the retail and dining experience and coordinate pedestrian, safety, and landscape improvements at the Plaza. Assist in tenant recruitment. Work with Kimco Realty Corporation to explore mixed use development. 	County of Los Angeles Kimco MLK-CDC	Ongoing
Continue Partnership with Los Angeles County Workforce Collaborative	<ul style="list-style-type: none"> Continue to collaborate with the Workforce Collaborative's initiative to coordinate workforce training programs for residents with public and private employers. 	County of Los Angeles LAC Workforce Collaborative LACDC	Ongoing
Create Opportunities for Infill Mixed Use Development	<ul style="list-style-type: none"> Coordinate with existing property owners on economic development opportunities of under-utilized and vacant sites. Coordinate with Compton Unified School District(CUSD) regarding the redevelopment of the school sites. Coordinate with the LACDC regarding the use of the Successor Agency-owned parcels. 	County of Los Angeles LACDC	Mid- to Long- Term
Pursue Local, State and Federal Infrastructure Funding Sources	<ul style="list-style-type: none"> Identify, monitor and apply for other government funding sources, for infrastructure and transportation improvements (also see Tables 7-2, 8-2 and 8-3). 	County of Los Angeles LACDC Metro	Ongoing
Establish an Enhanced Infrastructure Financing District (EIFD)	<ul style="list-style-type: none"> Conduct feasibility analysis and necessary elections for establishing an EIFD for financing public improvements and amenities. 	County of Los Angeles LACDC	Short- to Mid- Term
Establish a Development Impact Fee (DIF) Program	<ul style="list-style-type: none"> Conduct feasibility analysis for establishing a DIF Program for equitably spreading infrastructure costs equitably over new development. 	County of Los Angeles LACDC DPW/DRP	Short- to Mid- Term
Establish a Parking Management District in the Northwest Subarea	<ul style="list-style-type: none"> Consider a parking management plan in the Northwest Subarea to improve the availability of parking supply and pricing. 	Partnership between County of Los Angeles and Major Property Owners/Developers	Mid- to Long- Term
Establish a Business Improvement District (BID)	<ul style="list-style-type: none"> Marketing/promotional events/security/maintenance above existing public services; small scale capital improvements. Assessments must be benefit based. 	Property Owners/Business Owners	Mid- to Long- Term

1. The timeline identified is a relative estimate in comparison to the other actions included in the strategy.

Short-Term: 0-2 Years; Mid-Term: 2-5 years; and Long-Term: > 5 years;

Ongoing: occurring annually over development period.

2. The recommended steps and actions are contingent upon funding and allocation of resources.

Source: Stanley R. Hoffman Associates, Inc.

A. Institutional Arrangements

Community Development Corporation. Established in 2015, the MLK Health and Wellness Community Development Corporation (MLK-CDC) aims to develop an “urban family-health campus” adjacent to the new MLK Community Hospital, starting with re-purposing the former King-Drew hospital building into the MLK Community Wellness Center that will house a variety of service providers, including mental health, family services, job training and creation, outpatient clinical, bio- science, physical wellness, and neighborhood place-making.

The Specific Plan recommends the expansion of the MLK-CDC to provide a broader coverage of the Plan Area. The expanded MLK-CDC would direct the visioning, formulation and implementation of economic development strategies/plans and offer an effective means to achieve Specific Plan goals. The expanded MLK-CDC would operate under the auspices of the County with stakeholder roles at the county and community levels through adequate representation in executive roles. The MLK-CDC could take on the critical function of directing and coordinating various actors and available resources in pursuing targeted economic development opportunities for real estate development and identifying infrastructure and services in coordination with other partners. The MLK-CDC could also play a central role in workforce development through skills gap assessments and through leveraging existing programs at the national, state and county levels and instituting new ones, as necessary.

Public-Private Partnerships. Public-private partnerships provide effective means in the current constrained economy to leverage limited resources and achieve mutually desired goals. Potential partnerships between local and regional public agencies include those with specialized institutions in education, health care and research and development such as the MLK Community Hospital and CDU. Partnerships with the private and non-profit sectors enable the integration of local workforce capabilities with the ultimate job growth. Expanding these partnerships will help achieve the Specific Plan goals.

Los Angeles County Workforce Collaborative. Initiative for a Competitive Inner City (ICIC) is underway to help coordinate national, state and county wide workforce training programs, and for targeted collaboration with key stakeholders, including the private sector. Continuing to build on the work of the Workforce Collaborative helps the Specific Plan goals of improved employment opportunities for Willowbrook residents.

B. Targeted Development Opportunities

Several specific and targeted development opportunities have been identified for Willowbrook under the Willowbrook TOD Specific Plan, including:

Leveraging Health Care and Education Opportunities. Ongoing collaboration with the MLK Community Hospital should be leveraged for spin-off activities in health-care services and allied office uses. Similarly, ongoing partnership with CDU has the potential to spur research and development activities in healthcare and science programs for employers who want or need to be part of the medical community around MLK Medical Center.

Kenneth Hahn Plaza. The Plaza offers an opportunity for enhancing the retail and dining experience in the community through store location and enhanced design and capital improvements; the shopping center also has the potential for mixed use, transit oriented development.

C. Land Use Planning

Land Assembly. Land assembly incentives become important, particularly in a post-redevelopment environment, where encouraging the consolidation of smaller parcels into a larger, developable area results in more efficient development. While land assembly incentives are limited in the Willowbrook area, one incentive is the expedited processing of development that furthers the goals and objectives of the Specific Plan vision.

Use of Successor Agency Parcels. The economic development strategy recommends the catalytic use of parcels inherited by the successor agency to the County redevelopment agency to further incentivize the Specific Plan goals and objectives.

Reconfiguring Publicly Owned Parcels. Land resources already owned by agencies should be assessed for reconfiguration and consolidation for new development opportunities. Local public schools in the Northwest Subarea of the Plan Area have the potential to be reconfigured to make additional land resources available for private sector development under either a lease or sale arrangement.

D. Implementation and Financing Strategy

Various financing and implementation methods are viewed as important in the long-term context of the Specific Plan vision for a health and wellness community that builds on MLK Medical Center and CDU investments and attracts both residential and non-residential development, preferably with a medical employment related focus. These actions, their primary uses, responsible parties for implementation and phasing are summarized in Table 7-2.

Some of the techniques need to start early to build resources for the future, such as an Enhanced Infrastructure Financing District (EIFD), and others may be implemented after some development has occurred, such as a Parking Management District or a Business Improvement District (BID), but are still considered important as part of the overall strategy. Some of the techniques are exclusively for capital financing or operations and maintenance, and some of these techniques can provide both funding approaches. The strategy also includes the early establishment of public-private partnerships that can foster confidence in the attractiveness and vision of the Plan Area.

Core Strategies. The core, early strategies include maximizing infrastructure financing through an EIFD and establishing a development impact fee program for essential infrastructure, such as parking or street improvements, that spreads the costs equitably over new development. Also considered important in the short-term is the expansion of the non-profit MLK-CDC to direct the visioning, formulation and implementation of Specific Plan goals and objectives. Additionally, part of the mid- to long-term strategy would be to create a parking management plan that could address both the long-term supply and operations side of the parking system, including unbundling the cost of parking from the underlying rent or lease payments.

Along with maximizing these funding sources, the CIP for the Specific Plan, as discussed conceptually in this Chapter 7, identifies top community priorities to facilitate timely provision of public infrastructure and services.

Grant Funding Opportunities. Ongoing grant funding opportunities at the local level include Cap-and-Trade under the Low Carbon Transit Operations Program and the Metro and Transportation Development Act (TDA) funding. The Metro and TDA grant opportunities are generally funded through

**Table 7-2:
Summary of Potential Financing Techniques**

Financing Methods	Primary Uses	Coordinating Agencies
Development Impact Fee (DIF)	<ul style="list-style-type: none"> Capital improvements necessitated by new development. AB 1600 requires that a reasonable relationship or "nexus" must exist between the amount of the impact fee and the project on which it is imposed. 	County of Los Angeles
Special Benefit Assessment	<ul style="list-style-type: none"> Capital improvements necessitated by new development. Assessments must be benefit based. 	County of Los Angeles
Landscape Maintenance District (LMD)	<ul style="list-style-type: none"> Landscaping improvements. Landscaping operations and maintenance. LMD assessments must be benefit based. 	County of Los Angeles
Mello-Roos Special Tax	<ul style="list-style-type: none"> Capital improvements. Operations and maintenance. Can finance projects and services with both local and general benefits. 	County of Los Angeles
Business Improvement District (BID)	<ul style="list-style-type: none"> Marketing/promotional events/security/maintenance above existing public services; small scale capital improvements. Assessments must be benefit based. 	Property Owners/Business Owners
Community Development Block Grant (CDBG)	<ul style="list-style-type: none"> Federal program that provides direct annual grants to cities, counties, and states across the country. Grants are intended to revitalize disadvantaged areas, expand affordable housing and economic opportunities. 	County of Los Angeles Department of Regional Planning (DRP)
Enhanced Infrastructure Financing District (EIFD)	<ul style="list-style-type: none"> Use of County's future ad valorem property tax increment for financing public improvements and amenities. Requires a 55 percent vote of the electorate to incur bonded indebtedness; no increase in local property taxes. 	County of Los Angeles
Community Revitalization and Investment Authorities (AB 2)	<ul style="list-style-type: none"> Similar to an EIFD, AB2 allows use of the County's property tax increment for capital financing. Subject to a majority protest vote and subject to certain conditions related to serving disadvantaged communities. 	County of Los Angeles
Community Development Corporation (CDC)	<ul style="list-style-type: none"> Community development corporations (CDCs) are 501(c)(3) non-profit organizations for local areas. Community-based and provides economic development, education, community and affordable housing services. 	County of Los Angeles/Community Based Stakeholders

Table 7-2 (Continued)

Financing Methods	Primary Uses	Coordinating Agencies	Timeframe ¹
<p>California Infrastructure and Economic Development Bank</p>	<ul style="list-style-type: none"> Infrastructure State Revolving Fund (ISRF) Program provides economic development financing to agencies and non-profit corporations. Bank has broad authority to issue tax-exempt and taxable revenue bonds. 	<p>Governor's Office of Business and Economic Development</p>	<p>Mid- to Long- Term</p>
<p>Parking Management District</p>	<ul style="list-style-type: none"> Local districts that regulate parking supply and rates to meet the parking needs of the area. They can promote transit use, ridesharing, and alternative modes of transportation. 	<p>Partnership between County of Los Angeles and Major Property Owners/Developers</p>	<p>Mid- to Long- Term</p>
<p>Cap-and-Trade (Pursuant to AB 32)</p>	<ul style="list-style-type: none"> AB 32 authorizes the use of market mechanisms through an annual auction of surplus emissions reduction credits to generate funds, a portion of which can be targeted for clean transportation and sustainable community plans. The Low Carbon Transit Operations Program provides funding assistance for transit agencies to reduce GHG gas emissions and serve disadvantaged communities. 	<p>Caltrans in coordination with the California Air Resources Board (CARB)</p>	<p>Ongoing applications for grant funding</p>
<p>Metro and Transportation Development Act (TDA)</p>	<ul style="list-style-type: none"> Two percent of total TDA funds are allocated for bicycle and pedestrian projects. 	<p>Metro</p>	<p>Bi-Annual Metro "Call for Projects" application process</p>
<p>Metro - Various Grant Programs</p>	<ul style="list-style-type: none"> Congestion Mitigation and Air Quality (CMAQ) funds - bicycle, pedestrian and wayfinding projects. Metro Express Lanes Net Toll Revenue Re-Investment Grant Program - transit uses and roadway improvements. Metro Wayfinding Signage Grant Pilot Program. Metro Open Streets Program - regional car-free events. Surface Transportation Program - bicycle and pedestrian improvements and on-system roadway improvements. 	<p>Metro</p>	<p>Ongoing applications for grant funding</p>
<p>California Capital Investment Incentive Program (CIIP)</p>	<ul style="list-style-type: none"> Existing State program that provides property tax abatement for up to 15 consecutive years for large capital investment by qualified manufacturing facilities (must exceed \$150 million). 	<p>County and Governor's Office of Business and Economic Development</p>	
<p>Other Outside Grants and Loans</p>	<ul style="list-style-type: none"> Potential TOD grants from State's Strategic Growth Council (SSGC). SCAG Sustainable Communities Planning grants. Business façade improvement program grants. Affordable housing grants. 	<p>DRP; SSGC; SCAG</p>	<p>Ongoing applications for grant funding</p>

1. The timeline identified is a relative estimate in comparison to other actions included in the strategy.
 Short-Term: 0-2 Years; Mid-Term: 2-5 years; and Long-Term: > 5 years;
 Ongoing: occurring annually over development period.

local sales tax revenue measures on an annual or biannual cycle and have their own competitive application process. Other grant funding opportunities are also available at the federal, State and regional levels.

Additionally, programs such as the State of California's Infrastructure and Economic Development Bank (I-Bank) provides low-cost loans for qualified local programs; and Community Development Block Grant (CDBG) funds – while CDBG funds have been significantly reduced in recent years – still provide an ongoing source of funding to meet local priorities as part of the overall strategy.

Traditional Financing Techniques. Some of the more traditional financing techniques include: special benefit assessments, Mello-Roos special taxes and landscape lighting districts and are seen as mid- to long-term techniques as significant development begins to occur. The special tax and landscape lighting districts can fund both capital and operations and maintenance costs. Benefit assessments are subject to majority protest of the property owners, and special taxes are subject to 2/3 approval of the electorate.

A BID is a public/private partnership created to perform a variety of services to revitalize and improve commercial neighborhoods above existing local governmental services. Currently, there may not be enough business interest to form a BID, but over the mid- to long-term, interest may increase, particularly if businesses see this as a joint effort to efficiently use their collective funding to increase the quality and overall competitiveness of the area.

Emerging Techniques. Community Revitalization and Investment Authorities (CRIA), which was recently enacted into law, authorizes the revitalization of disadvantaged communities through planning and financing infrastructure improvements and upgrades; economic development activities; and affordable housing via tax increment financing based, in part, on the former community redevelopment law. A CRIA has some similarities to an EIFD, so both would not be implemented. The CRIA does have the power of eminent domain while the EIFD does not; also the CRIA has a number of restrictions that relate to serving disadvantaged communities and providing affordable housing. Finally, the County also has a California Capital Incentive Program (CCIP) that provides property tax abatement for up to 15 years for large capital investments by qualified manufacturing facilities (must exceed \$150 million); however, under an EIFD, this program would probably not be applicable as sufficient property tax increment revenues would likely not be available for both purposes.

7.5 Community Benefits

Community benefits from the implementation of the Economic Development Strategy are summarized below:

Identification and Coordination of Economic Opportunities. One of the pivotal benefits of public-private partnerships is the ongoing leveraging of opportunities that benefit firms/enterprises and the local workforce. A mechanism to implement workforce programs in response to private sector skill demands establishes an approach for realizing the potential for jobs growth and payroll increases within the community.

Capturing Benefits of Large Public Investments. Significant investments within Willowbrook in institutional and public transit development are expected to catalyze future growth in residential and non-residential uses over time. However, capturing and actually realizing these projected benefits depends upon programs in place, including those outlined above, and the establishment of the

EIFD for capital improvements for community recapture of fiscal benefits from increased property valuation. Appropriate land use zoning, such as mixed use, could further help incentivize the potential around large public investments.

Expanded Business Activities and Local Job Opportunities. A combination of the various initiatives outlined above could result in expanded business activities in spin-off industries associated with health care services and research and development, and an expanded spillover demand for local services, which would also benefit the local community. Land use planning serves a critical role in providing locational opportunities for such activities. The community would benefit from the expansion of better paying jobs in coordination with appropriate skills development initiatives, as discussed previously.

Better Transit Access and Jobs-Housing Balance. Public transit investments additionally provide the benefit of improving two-way access to jobs and housing opportunities. The ability of workers to access job opportunities in other parts of the County has the benefit of increasing community prosperity. On the flip side, local firms' ability to access a qualified workforce from a larger commute shed is an important factor in locational choice in addition to availability of land. Expanded housing opportunities over time could have the stabilizing benefit of improving the match of jobs and housing within the sub-regional economy. Capital improvements connecting and improving local community access to transportation infrastructure is an important community benefit in this regard to realize the above economic outcomes.

Affordable Housing. With the loss of redevelopment agency affordable housing set-aside funds, the decline in available affordable housing funds has been substantial. The County, through its Housing Element, will continue to address existing and new sources of funding for affordable housing programs, such as new market housing tax credit programs and programs that are currently being considered by the California State legislature, but have not yet been adopted.

Enhancing Overall Quality of Life. An overarching community benefit of expanded jobs, access to transit, and improved local services, local public infrastructure, and public amenities will enhance local quality of life.

Increasing Public Safety. The proposed street and access improvements under this Specific Plan would also have the benefit of improving public safety. A neighborhood with expanded business activities, work opportunities, transit access, and housing opportunities would likely reinforce community identity and citizen participation in local events, resulting in greater social and economic cohesion.

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Capital Improvement Program

8.1 Introduction

The conceptual CIP is summarized in Table 8-1 and outlines the general categories of improvements, estimated costs, responsible agencies and phasing. The CIP sets forth an ideal scenario and establishes priority projects contingent on the availability of funding. The total estimated costs in this conceptual CIP scenario are about \$73.3 million. The bulk of these costs are for the Willowbrook/Rosa Parks Station relocation and improvements in the Kenneth Hahn Plaza by Metro. The phasing for this work is identified as short-to-mid range, depending upon when the funding resources become available.

The major local infrastructure and accessibility costs are estimated for Streetscape Improvements at about \$16.8 million, followed by about \$3.5 million for Transportation-related Improvements. These improvements will be overseen primarily by DPW and funded by a variety of other sources that are presented in this chapter. The phasing for this work is estimated for short-to-mid to long- range. Again, this work depends on the availability of funding, particularly if the newly adopted EIFD legislation is used; this is because the EIFD's bonding capacity will increase over time as property tax increment increases with new development.

Finally, Public Parking Facility Improvements are identified as possible incentives for cost reductions for private sector development within the northwest quadrant. However, no specific plans or garage cost estimates are available at this time. An option for full construction of a public garage might be cost sharing garage subsidies with the private sector. The phasing of these costs is seen as mid-to-long- range because it is assumed that it will take five years or longer to create public infrastructure and amenities to begin to draw private sector investment.

8.2 Potential CIP Improvements

A detailed listing of potential transportation and related improvements is presented in Table 8-2 along with an identification of responsible lead and partner agencies, potential funding sources and phasing. Table 8-3 presents the potential utility improvements by private or enterprise utility companies also with their responsible lead companies and partner agencies, potential funding sources and phasing.

**Table 8-1:
Summary of Capital Improvement Plan Priorities**

Improvement	Estimated Cost	Coordinating Agencies	Timeframe ¹
Land Assembly ²	N/A	County and School Districts	Short- to Mid-Term
Transit-Related/Transit-Station Improvements	\$ 53,000,000	Metro ³	Short- to Mid-Term
Transportation Improvements	\$ 3,456,500	County of Los Angeles Dept. of Public Works (DPW)	Short- to Mid-Term
Streetscape Improvements	\$ 16,835,315	DPW	Short- to Mid-Term
Public Parking Garage Improvements/Subsidies	N/A	Potential Parking District ⁴	Mid- to Long-Term
TOTAL	\$ 73,291,815		

1. The timeline identified is a relative estimate in comparison to other improvements in the CIP.

Short-Term: 0-2 Years

Mid-Term: 2-5 Years

Long-Term: >5 Years

2. Potential land resources are available from the successor agency to redevelopment and from public school districts possibly reconfiguring their local campuses.

3. About \$10.25 million from the U.S. Department of Transportation (DOT) TIGER competitive grant funds is currently available; TIGER is an acronym for Transportation Investment Generating Economic Recovery.

4. A parking management district will incentivize private development through reduction in parking costs and management of parking supply.

Source: Stanley R Hoffman Associates, Inc.

Table 8-2: Streetscape and Transportation: Improvements, Responsibility, Potential Funding Sources

Improvements		Coordinating Agencies	Potential Funding Sources	Timeframe ¹
Transit-related Improvements		Metro	Metro	Short-Term
Upgraded Metro Rosa Parks Transit Station				
Vehicular/Pedestrian Improvements				
Road diet on Mona Boulevard		County of Los Angeles Department of Public Works (DPW)	<ul style="list-style-type: none"> Metro Call for Projects Cap and Trade County of Los Angeles Transportation Funds Federal and State grants and loans Transportation Development Act Funds 	Mid-Term
Road diet on Willowbrook Avenue West (between the Metro Station and 119th Street)		DPW		Mid-Term
Traffic signal - intersection of Mona Blvd. and 119th Street - <i>Traffic signal analysis and subsequent implementation</i>		DPW		Short-Term
Traffic signal - intersection of Wilmington and 122nd Street - <i>Traffic signal analysis and subsequent implementation</i>		DPW		Mid-Term
Pedestrian signal - intersection of Mona Boulevard and 120th Street - <i>Pedestrian signal analysis and subsequent implementation</i>		DPW		Short-Term
Pedestrian sidewalk paving on the west side of Willowbrook Avenue West between Metro Station & 119th Street		DPW / Metro		Mid-Term
Pedestrian sidewalk improvements on Wilmington Avenue between Imperial Highway and 117th Street (both sides)		DPW		Mid-Term
Pedestrian sidewalk improvements along 119th Street between Wilmington Avenue and Willowbrook West (north side only)		Private development		Mid-Term
Pedestrian Oriented Improvements Package (Standard Improvement package) – 12 locations <i>Standard Improvement package includes:</i> - <i>add zebra-style crosswalks to all crossings</i> - <i>add pedestrian countdown signals and audio signals to all crossings</i> - <i>add passive pedestrian detection to all crossings</i> - <i>add advanced stop bars to all approaches</i> - <i>add bulb-outs or reduce curb returns on corners where feasible</i>		DPW		Mid-Term

Table 8-2 (Continued)

Bicycle Improvements			
Class I bicycle path and pedestrian trail on Mona Boulevard	DPW	<ul style="list-style-type: none"> • Metro • DPW 	Mid-Term
Class I bicycle path/Class IV cycle track on Willowbrook Avenue West	Metro/DPW	<ul style="list-style-type: none"> • Cap and Trade • Federal and State Grants 	Short-Term
Class II bicycle lane on Imperial Highway between Compton Avenue and Mona Boulevard	DPW		Mid-Term
Class II bicycle lane on Wilmington Avenue between 120th and 124th Streets	DPW		Mid-Term
Class III bicycle route signage/stripping on Compton Avenue, 119th Street (between Wilmington & Mona), and Willowbrook Avenue West (south of 119th Street)	DPW		Mid-Term
Implement Bicycle Stations	MLK, CDU, Private Development		Short- to Mid- Term
Streetscape Enhancements			
Wilmington/119th/120th Streetscape Project (DPW-funded)	DPW	<ul style="list-style-type: none"> • Metro • DPW 	Short- to Mid-Term
Street trees on Specific Plan streets not included above	DPW	<ul style="list-style-type: none"> • Business Improvement District 	Long-Term
Pedestrian-scaled lighting on Specific Plan streets not included above	DPW	<ul style="list-style-type: none"> • Landscape and Lighting District 	Long-Term
Street furniture	DPW		Mid-Term
Other Mobility Related Actions			
Community contest to rename 120th Street between Compton and Wilmington Avenues	DPW	<ul style="list-style-type: none"> • Metro • County of Los Angeles • CDC 	Short Term
Setup Transportation Demand Management Program	DPW		Short Term
Parking			
Parking Management District	DPW	<ul style="list-style-type: none"> • County of Los Angeles, Enhanced Infrastructure Financing District • Future parking revenues 	Mid-Term
	Property and Business Owners		

1. The timeline identified is a relative estimate in comparison to other identified improvements.

Timeline: Short Term: 0-2 years; Mid-Term: 2-5 years; Long Term: >5 years.

Source: Stanley R. Hoffman Associates, Inc., The Arroyo Group, The Mobility Group

Table 8-3: Utilities: Improvements, Responsibility, Potential Funding Sources

Improvements	Oversight Responsibility - Lead and Partner	Potential Funding Sources	Phasing ¹
Evaluation of the need for water distribution lines upgrades in the Specific Plan area	Liberty Utilities, Golden State Water Company, and City of Los Angeles Dept. of Water and Power	Water Companies	Short- to Mid-Term
Detailed sewer study for Group 1	DPW and MLK Community Hospital	DPW through impact fees and connection charges	At time of development in Group 1
Other sewer related review by required by Sanitation Districts of Los Angeles County (LACSD)	Developers and LACSD	Development Impact Fees	Ongoing
Evaluation of demand for electrical services	<i>Primary responsibility:</i> Southern California Edison (SCE)	SCE through its utilities charges, as well as state and federal grants and	Ongoing
Evaluation of demand for telecommunication services	<i>Primary responsibility:</i> AT&T	AT&T funded through monthly phone charges	Ongoing
Evaluation of demand for cable services	<i>Primary responsibility:</i> Spectrum	Spectrum through its utilities charges, as well as state and federal grants and loans	Ongoing
Joint trenching with all utility providers	<i>Primary responsibility:</i> SCE <i>Secondary responsibility:</i> County of Los Angeles oversight and other private dry utilities companies	SCE in coordination with other utility providers	Ongoing

1. *Short term:* 0-2 years.
Mid-term: 2-5 years.
Long term: >5 years.

Source: Stanley R. Hoffman Associates, Inc., JMC² Civil Engineering

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SECTION 13 This ordinance shall be published in The Daily Commerce a newspaper printed and published in the County of Los Angeles.



Hilda F. Solis
Chair

ATTEST:

Celia Zavala

Celia Zavala
Executive Officer -
Clerk of the Board of Supervisors
County of Los Angeles

I hereby certify that at its meeting of March 9, 2021 the foregoing ordinance was adopted by the Board of Supervisors of said County of Los Angeles by the following vote, to wit:

Ayes

Noes

Supervisors Hilda L. Solis
Holly J. Mitchell
Sheila Kuehl
Janice Hahn
Kathryn Barger

Supervisors None

Effective Date: April 8, 2021

Operative Date:

Celia Zavala
Celia Zavala
Executive Officer -
Clerk of the Board of Supervisors
County of Los Angeles

I hereby certify that pursuant to Section 25103 of the Government Code, delivery of this document has been made

CELIA ZAVALA
Executive Officer
Clerk of the Board of Supervisors

By [Signature]



APPROVED AS TO FORM:
RODRIGO A. CASTRO-SILVA
County Counsel

By [Signature]
Lester J. Tolnai
Chief Deputy County Counsel